

**VILLAGE OF DOWNERS GROVE**  
**Report for the Village**  
**10/5/2021**

<b>SUBJECT:</b>	<b>SUBMITTED BY:</b>
Planned Unit Development, Zoning Map Amendment, Special Use and Final Plat of Subdivision – 2751-2761 Ogden Avenue	Stan Popovich, AICP Director of Community Development

**SYNOPSIS**

The petitioner is requesting approval of the following items to construct a hotel and restaurant at the southeast corner of Ogden Avenue and Walnut Avenue:

- Planned Unit Development
- Zoning Map Amendment from existing O-R-M, Office-Research-Manufacturing, to B-3/PUD, General Services and Highway Business/Planned Unit Development
- Special Use for the drive-through restaurant facility
- Final Plat of Subdivision

**STRATEGIC PLAN ALIGNMENT**

The goals for 2021-2023 include *Strong and Diverse Local Economy*.

**FISCAL IMPACT**

N/A

**RECOMMENDATION**

Approval on the October 12, 2021 active agenda per the Plan Commission’s 5-1 positive recommendation. The public hearing for 21-PLC-0015 was held on September 13, 2021.

The Plan Commission found that the proposal complies with the Subdivision lot dimensions in Section 20.301, is an appropriate use in the district, is compatible with the Comprehensive Plan and meets all standards for approval of a Planned Unit Development, associated Zoning Map Amendment and Special Use found, respectively in Section 28.12.030, Section 28.12.040 and Section 28.12.050.

**BACKGROUND**

Property Information & Zoning Request

The petitioner’s proposal is to construct a hotel, restaurant, and a stormwater detention basin on three lots at the southeast corner of Ogden Avenue and Walnut Avenue. The eastern lot (“Lot 1”) includes a 4-story, 48,660 square-foot, 122 room Woodspring Suites Hotel. The petitioner is proposing two options for the western lot (“Lot 2”). Option 1 includes a 4,900 square-foot, fast-casual, dine-in restaurant. Option 2 includes a 2,350 square-foot fast-food restaurant with two drive-through lanes. The drive-through will queue along the south side of the building and curve around to the east side to pick-up orders. The southern lot (“Lot 3”) includes 0.46 acres of stormwater detention.

The proposal provides pedestrian and vehicular connections from both Ogden and Walnut Avenues. The internal circulation patterns allow pedestrian and vehicles to move around the entire site. Additionally, pedestrian and vehicular connections are provided through Lot 1 to the Park District property to the south. The petitioner coordinated with the Park District to align their proposal with the Park District's future development. Cross access pedestrian and vehicular easements will be provided throughout the site to ensure the pedestrian and vehicular access is maintained.

#### Compliance with the Comprehensive Plan

The Comprehensive Plan identifies the following key features of Catalyst Site #D1:

- A striking, high-quality building on this site to effectively anchor the west end of the corridor and serve as a landmark for drivers on I-355

The proposed development also meets the Comprehensive Plan's key concepts for this subarea:

- Maximizes exposure and access to I-355 without compromising Village character or identity
- Parking lot screening and interior landscaped islands are provided
- Dumpster enclosures and screening are provided
- The use of shared access agreements to improve circulation on Ogden Avenue
- Beautification of Ogden Avenue achieved through installation of streetscape elements
- Zoning used to protect character by regulating uses that may be incompatible

The proposed development also meets other goals of the Comprehensive Plan. The Comprehensive Plan states that Corridor Commercial Areas include a blend of neighborhood-oriented commercial retail, offices, smaller regional retail and service uses. These uses serve a dual role by providing for the daily needs of the local residents with the restaurant use, while also providing a service to the larger region with the hotel use.

Lastly, the Comprehensive Plan notes that new public sites be developed and designed as focal points of the community and for the Village to collaborate with other Downers Grove agencies such as the Park District. The connectivity between the subject site and the future Park District development, and the on-site circulation will help to serve the community through providing pedestrian connections and decreased on-street traffic.

#### Compliance with the Zoning Ordinance

The subject property is currently zoned O-R-M, Office-Research-Manufacturing. The proposal includes a request for a Map Amendment to rezone the property to B-3/PUD, General Services and Highway Business/Planned Unit Development. The request for the map amendment is twofold. A Special Use to operate a drive through restaurant is available in the B-3 Zoning District (and not the O-R-M Zoning District). Additionally, the map amendment would allow the subject property to be more in line with recommendations of the Comprehensive Plan, as described above.

The proposed PUD deviates from one zoning requirement. The petitioner is requesting a deviation from the parking requirement for Lot 2, Option 1. The site will have 184 spaces, when 187 spaces are required. Per the petitioner, the deviation is necessary to avoid additional impacts on the adjacent wetland. The traffic study supports that the 184 spaces will provide sufficient parking for the site as the deviation is small and other methods of transportation are available, such as ride-sharing, taxis, and pedestrian options. The proposed development meets all other zoning ordinance bulk requirements.

The zoning ordinance notes that development types that are appropriate for PUD approval, including:

- Developments that enhance protection of natural elements, including wetlands.
- Developments that provide a mix of land use types.

- Developments that are consistent with the goals and policies of the Comprehensive Plan.

The PUD will also achieve a variety of planning goals as outlined in Section 28.4.030.A.2 of the Zoning Ordinance:

- Implementation of and consistency with the comprehensive plan and other relevant plans and policies.
- Compact, mixed-use development patterns where residential, commercial, civic and open spaces are located in close proximity to one another.
- The protection and enhancement of open space amenities and natural resource features.

#### Compliance with the Subdivision Ordinance

The petitioner is proposing to subdivide this lot into three lots of record. The Final Plat of Subdivision includes the necessary public utility and drainage easements, which are provided as required by the Village Ordinance. As a condition of approval a perpetual cross access easement will be provided between Lots 1 and 2, in addition to a cross access easement for the Downers Grove Park District property to the south. A stormwater management easement will also be provided.

#### Engineering\Public Improvements

The project will meet all provisions of the Stormwater and Floodplain Ordinance, and new stormwater detention is provided. Public improvements include a new curb cut on Ogden Avenue and a new curb cut on Walnut Avenue. The Walnut Avenue northbound right and left turn lanes will be striped to provide sufficient space for northbound traffic.

A detailed site planning analysis was completed in order to avoid and minimize wetland and buffer impacts as much as possible. The north end of the existing wetland that is within the subject parcel will incur minimal impacts with the proposed project plan. Only a drive aisle around the proposed hotel building, which is needed for safety purposes to allow emergency vehicles to be able to completely circulate around the building, will be impacting the wetland. Since there is no wetland mitigation proposed as part of the improvements, participation in the DuPage County Wetland Banking Program is required.

#### Traffic and Parking

A traffic and parking impact study for the proposed development was prepared by KLOA. The traffic study also took the Park District's future development into consideration when projecting traffic volumes. Capacity analyses were performed for the key intersections included in the study area to determine the ability of the existing roadway system to accommodate existing and future traffic demands. KLOA found that each of these intersections will be adequate in accommodating the traffic projected to be generated by the proposed development and will provide efficient and flexible access for both options one and two.

#### Public Comment

At the public hearing, one resident asked if there would be truck parking allowed, to which the petitioner shared that trucks would not be allowed to park in the lot overnight. The resident was in support of the project. The Plan Commission voted 5-1 to recommend approval of the petition.

The dissenting Plan Commissioner felt that more information on the market feasibility was necessary, but the primary concern was that the development should be a large retail use per the Comprehensive Plan. However, it was identified that the Plan Commissioner was referencing the 2011 Comprehensive Plan when this site was part of an 18-acre property. The most recently adopted 2017 Comprehensive Plan recommends that "a striking, high-quality building on this site to effectively anchor the west end of the corridor and serve as a landmark for drivers on I-355".

**ATTACHMENTS**

Ordinances

Resolution

Aerial Map

Staff Report with attachments dated September 13, 2021

Draft Minutes of the Plan Commission Hearing, dated September 13, 2021

**ORDINANCE NO. \_\_\_\_\_****AN ORDINANCE REZONING CERTAIN PROPERTY  
LOCATED AT 2751-2761 OGDEN AVENUE**

WHEREAS, the real estate located at 2751-2761 Ogden Avenue, located at the intersections of Ogden and Walnut Avenues, hereinafter described has been classified as "O-R-M, Office-Research-Manufacturing District" under the Zoning Ordinance of the Village of Downers Grove; and

WHEREAS, the owner or owners of said real estate have requested that such property be rezoned as hereinafter provided; and

WHEREAS, such petition was referred to the Plan Commission of the Village of Downers Grove, and said Plan Commission has given the required public notice, has conducted a public hearing respecting said petition on September 13, 2021 and has made its findings and recommendations all in accordance with the statutes of the State of Illinois and the ordinances of the Village of Downers Grove; and

WHEREAS, making due allowance for existing conditions, the conservation of property values, the development of the property in conformance to the official Comprehensive Plan of the Village of Downers Grove, and the current uses of the property affected, the Council has determined that the proposed rezoning is for the public good.

NOW, THEREFORE, BE IT ORDAINED by the Council of the Village of Downers Grove, in DuPage County, Illinois, as follows:

**SECTION 1.** The Zoning Map of the Village, pursuant to Section 28.12.030 of the Downers Grove Municipal Code, is hereby further amended by rezoning to "B-3/PUD, General Services and Highway Business/Planned Unit Development" the zoning classification of the following described real estate, to wit:

LOT 1 IN OGDEN AND WALNUT RESUBDIVISION OF PARTS OF LOTS 1, 2, 3, 4 AND 5 IN ARTHUR T. MCINTOSH AND COMPANY'S FIRST ADDITION TO BELMONT, BEING A SUBDIVISION OF PART OF SECTIONS 1, 2, 11 AND 12, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 20, 2015 AS DOCUMENT R2015-116130, IN DUPAGE COUNTY, ILLINOIS.

Commonly Known As: 2751-2761 Ogden Avenue, Downers Grove, IL 60515

PIN: 08-01-304-031

**SECTION 2.** The official zoning map shall be amended to reflect the change in zoning classification effected by Section 1 of this ordinance, subject to the following conditions:

1. Any changes to the conditions represented by the Petitioner as the basis for this petition, whether those changes occur prior to or after Village approval, shall be promptly reported to the Village. The Village reserves the right to re-open its review process upon receipt of such information; and
2. It is the Petitioner's obligation to maintain compliance with all applicable Federal, State, County and Village laws, ordinances, regulations, and policies.

**SECTION 3.** That the following factors were considered in this rezoning as shown in the Zoning Ordinance:

1. The existing use and zoning of nearby property;
2. The extent to which the particular zoning restrictions affect property values;
3. The extent to which any diminution in property value is offset by an increase in the public health, safety and welfare;
4. The suitability of the subject property for the zoned purposes;
5. The length of time that the subject property has been vacant as zoned, considering the context of land development in the vicinity;
6. The value to the community of the proposed use; and
7. The comprehensive plan.

SECTION 4. That all ordinances or parts of ordinances in conflict with the provisions of this ordinance are hereby repealed.

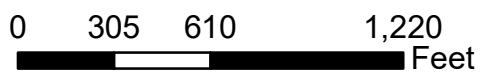
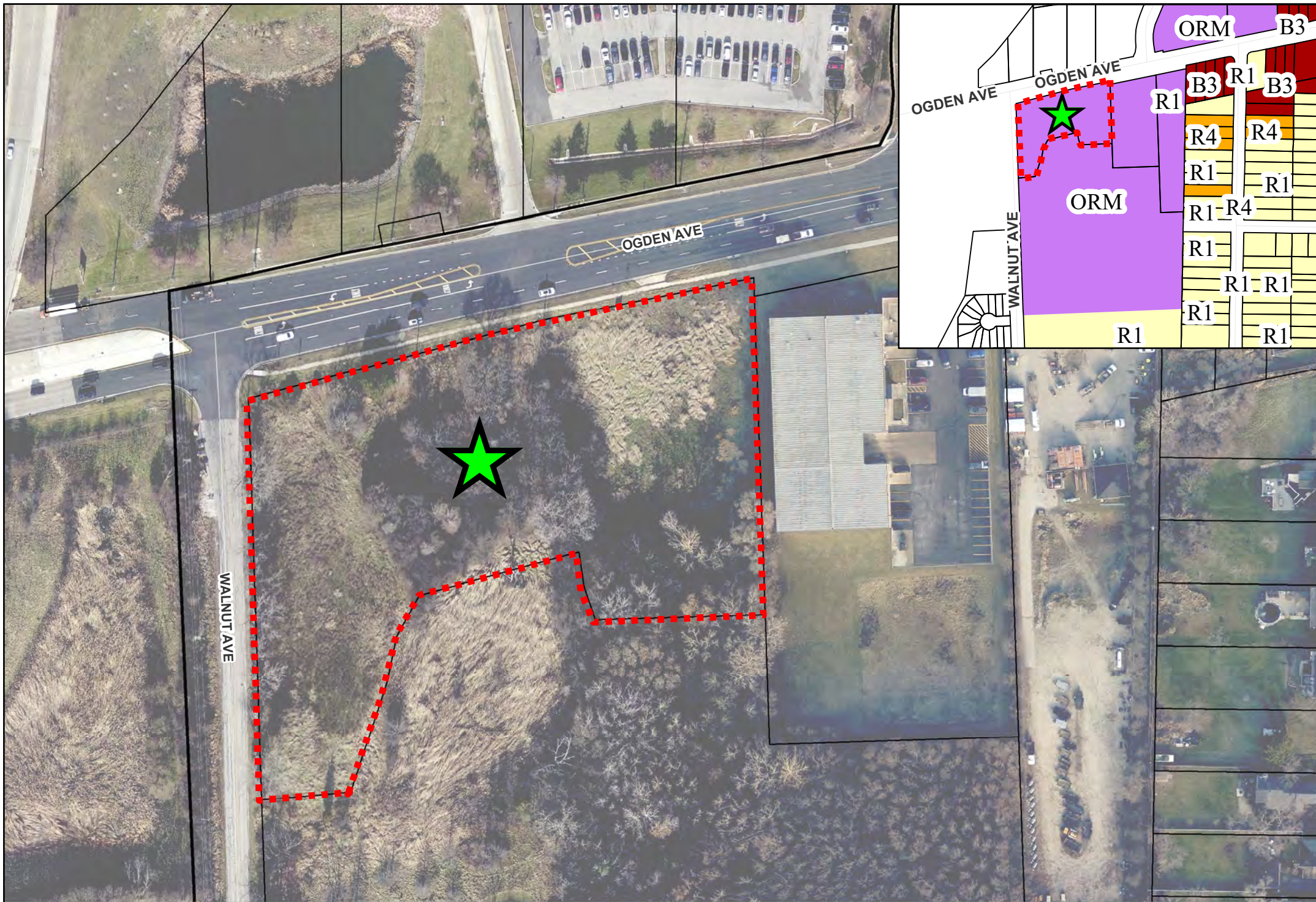
SECTION 5. This ordinance shall be in full force and effect from and after its passage and publication in pamphlet form as provided by law.

\_\_\_\_\_  
Mayor



Passed:


Published:

Attest: \_\_\_\_\_  
Village Clerk



### Location Map: 2751 and 2761 Ogden Avenue

Project Location   
Subject Property 





**VILLAGE OF DOWNERS GROVE  
REPORT FOR THE PLAN COMMISSION  
SEPTEMBER 13, 2021, AGENDA**

<b>SUBJECT:</b>	<b>TYPE:</b>	<b>SUBMITTED BY:</b>
21-PLC-0015 2751-2761 Ogden Avenue	Planned Unit Development, Map Amendment, Special Use, and Final Plat of Subdivision	Gabriella Baldassari, AICP Development Planner

### REQUEST

The petitioner is requesting approval of the following items to construct a hotel, restaurant, and a stormwater detention basin on three lots located at the southeast corner of Ogden Avenue and Walnut Avenue, commonly known as 2751 and 2761 Ogden Avenue:

1. A Planned Unit Development
2. A Map Amendment from existing O-R-M, Office-Research-Manufacturing, to B-3/PUD, General Services and Highway Business/Planned Unit Development
3. A Special Use for the drive-through restaurant facility
4. A Final Plat of Subdivision

### NOTICE

The application has been filed in conformance with applicable procedural and public notice requirements.

### GENERAL INFORMATION

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**OWNERS:** Tag Land DGC LLC  
3201 Old Glenview Road, Suite 302  
Wilmette, IL 60091

**PETITIONER:** Chicago WSS Developer II  
Duffey Phelps  
324W. Burlington Avenue  
LaGrange, IL 60525

### PROPERTY INFORMATION

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**EXISTING ZONING:** O-R-M, Office-Research-Manufacturing  
**EXISTING LAND USE:** Vacant  
**PROPERTY SIZE:** 145,528.84 sq. ft. (3.34 acres)  
**PIN:** 08-01-304-031

#### SURROUNDING ZONING AND LAND USES

**ZONING**

**NORTH:** DuPage County R-1, Single Family  
**SOUTH:** O-R-M, Office-Research-Manufacturing  
**EAST:** O-R-M, Office-Research-Manufacturing  
**WEST:** Tollway Property (Unincorporated)

**FUTURE LAND USE**

Office Corporate Campus  
Park Open Space  
Corridor Commercial  
Tollway Property  
(Unincorporated)

## ANALYSIS

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### SUBMITTALS

This report is based on the following documents, which are on file with the Department of Community Development:

1. Project Narrative
2. Standards of Approval
3. Location Map
4. Plat of Survey
5. Architectural Plans
6. Engineering Plans
7. Landscape Plan
8. Preliminary Stormwater Report
9. Sign Plans
10. Plat of Subdivision
11. Summary of Neighborhood Meeting
12. Traffic Impact Study

### PROJECT DESCRIPTION

The petitioner is proposing to develop the vacant site at the southeast corner of Ogden Avenue and Walnut Avenue. The proposal is to construct a hotel, restaurant, and a stormwater detention basin on three lots. The eastern lot ("Lot 1") includes a 4-story, 48,660 square-foot, 122 room hotel. The petitioner is proposing two options for the western lot ("Lot 2"). Option 1 includes a 4,900 square-foot, fast-casual, dine-in restaurant. Option 2 includes a 2,350 square-foot fast-food restaurant with two drive-through lanes. The southern lot ("Lot 3") includes 0.46 acres of detention.

The petitioner is requesting approval of the following items:

1. A Planned Unit Development designation to complete a comprehensive development of the site with the listed uses.
2. A Zoning Map Amendment to rezone from O-R-M, Office-Research-Manufacturing, to B-3/PUD, General Services and Highway Business/Planned Unit Development. Planned Unit Developments are permitted in the B-3 zoning district.
3. A Special Use to permit a drive-through restaurant in the B-3, General Services and Highway Business district for Lot 2 - Option 2. A drive-through use is listed in Section 28.5.010 of the Zoning Ordinance as an allowed Special Use in the B-3 Zoning District.
4. A Final Plat of Subdivision to subdivide one lot of record into three lots of record.

### *Existing Conditions*

The subject property consists of one lot of record. The property is zoned O-R-M, Office-Research-Manufacturing. The vacant lot currently has no curb cuts. The subject property was previously part of a larger lot that extended further south, but was subdivided in 2015 to better market this lot for future commercial development. The lot to the immediate south is owned by the Downers Grove Park District and is anticipated to be used for recreational purposes in the future, further discussed below. A wetland is located on the southern portion of the subject property that covers approximately 2,502 square-feet of the site.

*Proposed Development*

The petitioner is proposing to develop the site as follows:

Table 1- Development Summary

<b>Lot</b>	<b>Location</b>	<b>Building Description</b>	<b>Building Size</b>
1	Eastern Lot	Hotel	48,660 sq. ft.
2	Western Lot	Restaurant Option 1 – Dine In	4,900 sq. ft.
		Restaurant Option 2 – Drive Through	2,350 sq. ft.
3	Southern Lot	Detention Basin	N/A

The petitioner does not have a restaurant tenant confirmed for Lot 2 and is requesting flexibility for tenant options that may desire use of a drive through. Because a tenant has not been identified, elevations are also not provided. A condition of approval has been added which specifies that the proposed elevations shall be consistent and compatible with the hotel. Access to the development will be provided from a proposed access point on Ogden Avenue and a proposed access point on Walnut Avenue. As a condition of proposal, a cross access easement will be provided between both lots.

*Lot 1 (Eastern Lot)*

The Woodspring Suites Hotel is proposed for Lot 1. The building is proposed as a 4-story, 48,660 square-foot, 122-room hotel. A drive aisle between the hotel and restaurant will be provided, with a pedestrian access connection between the two buildings. The front of the hotel will have a canopy covering a drop-off/ pick-up lane. A sidewalk is proposed around the hotel and along the eastern border of the lot to provide pedestrian access to the Downers Grove Park District property located immediately south of the development. A pedestrian connection, as required by Village Ordinance is provided to Ogden Avenue. The trash enclosure that will serve the hotel will be in the southern parking lot behind the hotel.

The following elements are incorporated in the design of the building:

- Each side shows a variation in colors that will add visual interest to the building.
- The exterior is constructed with stone veneer as well as cement board siding.
- Each side utilizes plane variations to add design interest.
- Tower features on the front two corners to add to the overall design of the building.
- Cedar tresses under the front canopy will break up the visual plane.

*Lot 2 (Western Lot) Option 1*

Option 1 for Lot 2 will consist of an approximately 4,900 square-foot, fast-casual, and dine-in restaurant. Vehicles can circulate around the restaurant in either direction, including along the east side of the lot between the restaurant and the hotel. This option includes a pedestrian connection between the restaurant and the hotel. There will also be two pedestrian connections, as required by Village Ordinance, to Ogden Avenue and Walnut Avenue. The trash enclosure serving the restaurant will be located in the southeast corner of the parking lot.

*Lot 2 (Western Lot) Option 2*

Option 2 for Lot 2 will be an approximately 2,350 square-foot restaurant with a two lane drive-through. The drive-through will have space for 12 stacked vehicles. The drive-through will queue along the south side of the building and curve around to the east side to pick-up their orders. From there, they can continue onto Lot 1 or circulate around the restaurant counter-clockwise to leave from the Walnut Avenue access point. There will also be two pedestrian connections, as required by Village Ordinance, to Ogden Avenue and Walnut Avenue. The trash enclosure serving the restaurant will be located in the southeast corner of the parking lot.

*Parking*

Parking is provided on Lots 1 and 2 and will serve both lots. For Option 1, the petitioner is proposing a

total of 184 spaces including 8 accessible spaces, when 187 total spaces are required utilizing the Village's shared parking calculation; therefore a deviation from the Village's Zoning Ordinance is required. For Option 2, with the drive-through restaurant, the petitioner is proposing a total of 182 spaces including 8 accessible spaces, when 165 total spaces are required utilizing the Village's shared parking calculation.

### *Signage*

Signage is not part of this petition, and any signage proposed for the development shall comply with the Zoning Ordinance requirements through a separate sign permit application. For reference, the three proposed monument signs and four proposed wall signs are compliant with Zoning Ordinance. Future signage for Lot 2 will need to comply with the Zoning Ordinance requirements.

### *Downers Grove Park District Property*

As noted above, the Downers Grove Park District owns the property directly to the south. The Downers Grove Park District anticipates future recreational opportunities on their property. The petitioner coordinated with the Downers Grove Park District in order to align their proposal with the future district development. Both development Options 1 and 2 will continue the sidewalk along Walnut Avenue for pedestrian access to the park. An internal sidewalk along the eastern border of Lot 1 is also provided to allow additional pedestrian access to the district property. As a condition of approval, a cross access easement and pedestrian access easement will be provided allow vehicular and pedestrian access between the two sites.

### **COMPLIANCE WITH THE COMPREHENSIVE PLAN**

The Comprehensive Plan identifies the subject property as a Catalyst Site #D1 in the *Ogden Avenue - Key Focus Areas*.

The Comprehensive Plan identifies the following key features of Catalyst Site #D1:

- A striking, high-quality building on this site to effectively anchor the west end of the corridor and serve as a landmark for drivers on I-355

The proposed development also meets the Comprehensive Plan's key concepts for this subarea:

- Maximizes exposure and access to I-355 without compromising Village character or identity
- Parking lot screening and interior landscaped islands are provided
- Dumpster enclosures and screening are provided
- The use of shared access agreements to improve circulation on Ogden Avenue
- Beautification of Ogden Avenue achieved through installation of streetscape elements
- Zoning used to protect character by regulating uses that may be incompatible

The proposed development also meets other goals of the Comprehensive Plan. The Comprehensive Plan states that Corridor Commercial Areas include a blend of neighborhood-oriented commercial retail, offices, smaller regional retail and service uses. These uses serve a dual role by providing for the daily needs of the local residents with the restaurant use, while also providing a service to the larger region with the hotel use.

Lastly, the Comprehensive Plan notes that new public sites be developed and designed as focal points of the community and for the Village to collaborate with other Downers Grove agencies such as the Park District. The connectivity between the subject site and the future district development, and the on-site circulation will help to serve the community through providing pedestrian connections and decreased on-street traffic.

The proposed development meets the goals of the Comprehensive Plan.

**COMPLIANCE WITH THE SUBDIVISION ORDINANCE**

The subject property is made up of one lot of record. The petitioner is proposing to subdivide this lot into three lots of record. All new business and employment lots must be at least 100' wide by 140' deep, and have a total area of 10,500 square-feet.

All three proposed lots meet the requirements as shown in the table below:

Table 4 - Subdivision Overview

2751 and 2761 Ogden Avenue	Lot Width		Lot Depth		Lot Area (s.f.)	
	Required	Proposed	Required	Proposed	Required	Proposed
<b>Lot 1</b>	100'	342'	140'	325'	10,500	87,466
<b>Lot 2</b>	100'	150'	140'	217'	10,500	38,304
<b>Lot 3</b>	100'	165'	140'	158'	10,500	19,759

The Final Plat of Subdivision includes the necessary public utility and drainage easements, which are provided as required by the Village Ordinance. As a condition of approval a perpetual cross access easement will be provided between Lots 1 and 2, in addition to a cross access easement for the Downers Grove Park District property to the south. A stormwater management easement will also be provided as required for the proposed development.

**COMPLIANCE WITH ZONING ORDINANCE**

The subject property is currently zoned O-R-M, Office-Research-Manufacturing. The proposal includes a request for a Map Amendment to rezone the property to B-3/PUD, General Services and Highway Business/Planned Unit Development. The request for the map amendment is twofold. A Special Use to operate a drive through restaurant is available in the B-3 Zoning District (and not the O-R-M Zoning District). Additionally, the map amendment would allow the subject property to be more in line with recommendations of the Comprehensive Plan, as described above.

The bulk requirements of the proposed development in the B-3 Zoning District are summarized in the following table:

Table 5 – Subject Property Zoning Requirements

<b>Lot 1- Hotel</b>	<b>Required</b>	<b>Proposed</b>
Max. Building Height	60 feet	47 feet
Max. F.A.R.	0.75	0.56
Building Street Setback (N)	75 feet from centerline	130.66 feet
Building Rear Setback (S)	10 feet	77.99 feet
Parking Setback (N)	50 feet from centerline	54.5 feet
Parking Setback (E)	5 feet	10 feet
Parking Setback (S) Min.	5 feet	5 feet
Open Space Min.	8,747 s.f.	19,165 s.f.
Street Yard Open Space 50% Min.	4,373 s.f.	4,973 s.f.

<b>Lot 2- Option 1 (Dine-in Restaurant)</b>	<b>Required</b>	<b>Proposed</b>
Max Building Height	60 feet	25 feet
Max F.A.R.	0.75	0.06
Building Street Setback (N)	75 feet from centerline	126.92 feet
Building Interior Setback (E)	10 feet	49.53 feet
Building Street Setback (W)	35 feet	80.53 feet

Building Rear Setback (S)	10 feet	80.23 feet
Parking Setback (N)	50 feet from centerline	74.55 feet
Parking Setback (W)	8 feet	8.5 feet
Parking Setback (S)	5 feet	5 feet
Open Space	3,830 s.f.	6,766 s.f.
Street Yard Open Space 50% Min.	1,915 s.f.	3,878 s.f.

<b>Lot 2- Option 2 (Drive-through Restaurant)</b>	<b>Required</b>	<b>Proposed</b>
Max Building Height	60 feet	25 feet
Max F.A.R.	0.75	0.06
Building Street Setback (N)	75 feet from centerline	126.92 feet
Building Rear Setback (S)	10 feet	80.23 feet
Parking Setback (N)	50 feet from centerline	74.88 feet
Parking Setback (W)	8 feet	8.5 feet
Drive-Through Setback (N)	50 feet from centerline	122.24 feet
Drive-Through Setback (W)	8 feet	50.5 feet
Drive-Through Setback (S)	5 feet	54 feet
Stacking Spaces	12 spaces	12 spaces
Open Space	3,830 s.f.	7,783 s.f.
Street Yard Open Space 50% Min.	1,915 s.f.	3,878 s.f.

The proposed PUD deviates from one zoning requirement. As noted above, the petitioner is requesting a deviation from the parking requirement for Lot 2, Option 1. The site will have 184 spaces, when 187 spaces are required. Per the petitioner, the deviation is necessary to avoid additional impact on the adjacent wetland. Additional parking spaces would further impact the wetland, and a reduction of the drive aisles would inhibit emergency vehicle access. The traffic study supports that the 184 spaces will provide sufficient parking for the site as the deviation is small and other methods of transportation are available, such as ride-sharing, taxis, and pedestrian options.

#### *Planned Unit Development Request*

A Planned Unit Development is intended to accommodate development that may be difficult to carry out under applicable zoning standards and results in public benefits that are at least commensurate with the degree of flexibility provided. Examples of development types that are appropriate for PUD approval, per Section 28.4.030.A.1 of the Zoning Ordinance, include:

- Developments that enhance protection of natural elements, including wetlands.
- Developments that provide a mix of land use types.
- Developments that are consistent with the goals and policies of the Comprehensive Plan.

The proposal includes a 19,759 square-foot lot dedicated to stormwater detention and wetland preservation, which includes stormwater management easements on the two developed lots. Additionally, the mix of land uses offered will provide a variety of options to the area, including connectivity with the Downers Grove Park District property to the south. The hotel and restaurant development helps advance the goals of the Comprehensive Plan by developing Catalyst Site # D1 and other goals as described above.

The PUD will also achieve a variety of planning goals as outlined in Section 28.4.030.A.2 of the Zoning Ordinance:

- Implementation of and consistency with the comprehensive plan and other relevant plans and policies.
- Compact, mixed-use development patterns where residential, commercial, civic and open spaces are located in close proximity to one another.
- The protection and enhancement of open space amenities and natural resource features.

The proposed development meets the provisions of a Planned Unit Development. As noted above, the proposed development provides improvements to Catalyst Site #D1 that are identified in the Comprehensive Plan. The site's proximity to the future development of the Downers Grove Park District property will mix commercial uses with open spaces. The requested parking deviation allows for protection of the wetland area. The development provides a high-quality building and improvements that are compatible with the surrounding area.

## **ENGINEERING/PUBLIC IMPROVEMENTS**

### *Stormwater/Wetland Mitigation*

Based on the project scope, stormwater detention is required for the proposed development. The site grading and drainage systems, erosion and sedimentation control measures and devices, and the best management practices for the project has been designed to collect, convey and manage accumulated runoff from the project in accordance with the Village of Downers Grove Stormwater Requirements.

As noted previously a wetland is located on the southern portion of the subject property. The existing wetland is approximately 0.97 acres in total size and the majority of it is off-site to the south, but the north end (approximately 2,502 sf) is within the subject parcel property lines. Of the approximately 2,502 sf of the wetland on the subject property, 1,402 sf are proposed to be disturbed by the improvements. The rest of the off-site wetland will remain undisturbed in its natural condition.

A detailed site planning analysis was completed in order to avoid and minimize wetland and buffer impacts as much as possible. The current site plan is a product of this detailed analysis. The north end of the existing wetland that is within the subject parcel will incur minimal impacts with the proposed project plan. Only a drive aisle around the proposed hotel building, which is needed for safety purposes to allow emergency vehicles to be able to completely circulate around the building, will be impacting the wetland. Since there is no wetland mitigation proposed as part of the improvements, participation in the DuPage County Wetland Banking Program is being proposed. A condition of approval requiring participation in the County Wetland Banking Program has been added to the project.

### *Traffic Analysis*

A traffic and parking impact study for the proposed development was prepared by KLOA. Capacity analyses were performed for the key intersections included in the study area to determine the ability of the existing roadway system to accommodate existing and future traffic demands. Concept plans of the future park were provided for KLOA for this study. The traffic study takes the future Park District development into consideration when projecting traffic volumes.

A right-in/right-out access drive will be provided on Ogden Avenue approximately 315 feet east of Walnut Avenue. IDOT has reviewed the proposed curb-cut locations and is supportive of the proposed access point as designed. The access drive will provide one inbound lane and one outbound lane signed and physically restricted to prevent left-turn movements. Outbound movements will be under stop sign control.

As proposed, a full movement access drive will be provided on Walnut Avenue approximately 250 feet south of Ogden Avenue. The access drive will provide one inbound lane and one outbound lane with outbound movements under stop sign control. Walnut Avenue will be restriped to provide two northbound lanes striped for an exclusive left-turn lane and an exclusive right-turn lane. This intersection has sufficient reserve capacity and no additional geometric or traffic control improvements are recommended. KLOA reports that each of these intersections will be adequate in accommodating the traffic projected to be generated by the proposed development and will provide efficient and flexible access.

The traffic and parking impact study also analyzes the potential drive-through restaurant under Option 2. Access to the development will be the same in both options. About half of drive-through restaurant trips are diverted from the existing traffic which reduces the count for new pass-by traffic generated by the

restaurant use. The proposed drive-through design provides stacking for a minimum of twelve vehicles which will be adequate to accommodate the peak demand of the drive-through operation. KLOA has determined that intersections will be adequate in accommodating the traffic projected to be generated by the proposed development and will provide efficient and flexible access for Option 2.

#### *Public Improvements*

Public improvements include a new curb cut on Ogden Avenue and a new curb cut on Walnut Avenue. Curb, gutter, and sidewalks will be maintained or provided when necessary along both right-of-ways. The Walnut Avenue northbound right and left turn lanes will be striped to provide sufficient space for northbound traffic. The Sanitary District has provided conceptual approval for the proposed development.

#### **PUBLIC SAFETY REQUIREMENTS**

The Fire Prevention Division has reviewed the proposed plans and determined that the development provides sufficient access for emergency vehicles. As shown in the truck turning plan, the Village's largest emergency vehicle can maneuver through the site and access both buildings. The two proposed buildings will also include a fire alarm system and sprinkler system that meet the Village's code requirements. A fire department connection is provided on the front of each building façade.

#### **NEIGHBORHOOD COMMENT**

Notice was provided to all property owners 250 feet or less from the property in addition to posting public hearing notice signs and publishing the legal notice in *The Bugle*. Staff did not receive any inquiries.

As required by the Zoning Ordinance, the petitioner held a neighborhood meeting on August 23, 2021. The petitioner received one call in advance of the meeting asking for details about the hotel company. There were no attendees at the meeting, and no other communication was received.

#### **STANDARDS OF APPROVAL**

The petitioner is requesting a Planned Unit Development, Map Amendment, Special Use and Final Plat of Subdivision approval for the development of a 122-room hotel and a restaurant that will either be a dine-in or a drive-through restaurant, in the B-3 zoning district. The review and approval criterion for each request is listed below.

The petitioner has submitted a narrative that attempts to address all the standards of approval. The Plan Commission should consider the petitioner's documentation, the staff report, and the discussion.

#### ***Planned Unit Development***

##### ***Section 28.12.040.C.6 Review and Approval Criteria***

The decision to amend the zoning map to approve a PUD development plan and to establish a PUD overlay district are matters of legislative discretion that are not controlled by any single standard. In making recommendations and decisions regarding approval of planned unit developments, review and decision-making bodies must consider at least the following factors:

- a. *The zoning map amendment review and approval criteria of Sec. 28.12.030.I.*
- b. *Whether the proposed PUD development plan and map amendment would be consistent with the comprehensive plan and any other adopted plans for the subject area.*
- c. *Whether PUD development plan complies with the PUD overlay district provisions of Sec. 28.4.030.*
- d. *Whether the proposed development will result in public benefits that are greater than or at least equal to those that would have resulted from development under conventional zoning regulations.*
- e. *Whether appropriate terms and conditions have been imposed on the approval to protect the interests of surrounding property owners and residents, existing and future residents of the PUD and the general public.*

#### ***Zoning Map Amendment***

***Section 28.12.030.I. Zoning Map Amendment Review and Approval Criteria***

The decision to amend the zoning map is a matter of legislative discretion that is not controlled by any single standard. In making recommendations and decisions about zoning map amendments, review and decision-making bodies must consider at least the following factors:

1. *The existing use and zoning of nearby property.*
2. *The extent to which the particular zoning restrictions affect property values.*
3. *The extent to which any diminution in property value is offset by an increase in the public health, safety and welfare.*
4. *The suitability of the subject property for the zoned purposes.*
5. *The length of time that the subject property has been vacant as zoned, considering the context of land development in the vicinity.*
6. *The value to the community of the proposed use.*
7. *The comprehensive plan.*

***Special Use******Section 28.12.050.H Approval Criteria – Special Uses***

No special use may be recommended for approval or approved unless the respective review or decision-making body determines that the proposed special use is constituent with and in substantial compliance with all Village Council policies and plans and that the petitioner has presented evidence to support each of the following conclusions:

1. *That the proposed use is expressly authorized as a Special Use in the district in which it is to be located;*
2. *That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.*
3. *That the proposed use will not, in the particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.*

**DRAFT MOTION**

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Staff will provide a recommendation at the September 13, 2021 meeting. Should the Plan Commission find that the request meets the standards of approval for a Planned Unit Development, accompanying Map Amendment, Special Use and Plat of Subdivision, staff has prepared a draft motion that the Plan Commission may make for the recommended approval of 21-PLC-0015:

Based on the petitioner's submittal, the staff report, and the testimony presented, I find that the petitioner has met the standards of approval for a Planned Unit Development, accompanying Map Amendment, Special Use and Plat of Subdivision as required by the Village of Downers Grove Zoning Ordinance and is in the public interest and therefore, I move that the Plan Commission recommend to the Village Council approval of 21-PLC-0015, subject to the following conditions:

1. The Special Use, Planned Unit Development and Map Amendment shall substantially conform to the staff report, renderings, architecture plans prepared by Holladay Properties, dated December 14, 2021, engineering plans prepared by CivWorks Consulting LLC, dated August 30, 2021, landscape plans prepared by LG Workshop LLC, dated July 2, 2021, and traffic plans prepared by KLOA dated August 30, 2021 except as such plans may be modified to conform to the Village codes and ordinances.
2. The petitioner shall subdivide the lot into the three lots of record pursuant to Section 20.5 of the Subdivision Ordinance prior to the issuance of any site development or building permits.
3. The building elevations for Lot 2 shall be consistent and compatible with the hotel building design.

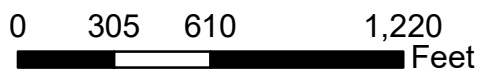
4. A perpetual cross access and parking easement is provided between Lots 1 and 2.
5. A perpetual cross access easement is provided to the property immediately south of the planned unit development.
6. A pedestrian easement is provided on Lot 1 for the benefit of public access to the property immediately south of the planned unit development.
7. Approval of participation in the County Wetland Banking Program is required prior to the issuance of any site development or building permits.
8. Parking shall be utilized across both Lots 1 and 2, for the planned unit development. If at any point the properties are under separate ownership, an off-site parking agreement must be completed in a form approved by the Village Attorney. The agreement must provide for the perpetual use of parking across both lots.
9. Payment of a fee-in-lieu of placement of the sidewalk as required by Village Ordinance. The fee will be determined based on the square footage area of the sidewalk, which will be located immediately south of the Walnut Avenue access drive to the southern property line. Payment must be made prior to the issuance of any site development or building permits.
10. All signage shall conform to the Village's Sign Ordinance.

Staff Report Approved By:



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Stan Popovich, AICP  
Director of Community Development



### Location Map: 2751 and 2761 Ogden Avenue



August 30th, 2021

To: Jason Zawila

Re: SEC Corner of Ogden and Walnut – PUD, Rezoning, Special Use, Final Plat of Subdivision

Dear Jason,

On behalf of Holladay Properties I am happy to submit an application to request approval for a proposed comprehensive redevelopment at the SEC Corner of Ogden and Walnuts, commonly known as 2751 and 2761 Ogden Avenue.

1. A Planned Unit Development
2. Rezoning – to B-3/PUD General Services and Highway Business/Planned Unit Development
3. Special Use – for potential drive thru restaurant
4. Final Plat of Subdivision

## **A. PROJECT NARRATIVE**

Per the Downers Grove Comprehensive Plan the 2751 and 2761 Ogden site has been labeled a catalytic site in the Ogden Avenue West end corridor. As such Holladay understands the importance of cultivating high-quality business tax generating businesses on this site. We also realize that being on the West End of Ogden this site serves as a gateway to the Village and as such it is important to build high quality and visually appealing buildings.

This site is also unique in that it backs up the future “Walnut Sports Park” and the hotel and future restaurant will be a vital amenity to the sport park. As such we have worked with the Park District from the very beginning to ensure that the uses align with their potential needs and that our site plan considers their future expansion. This includes an access point to their facilities on the SW corner of the property.

Below and attached is an overview of our proposed project along with information that we hope qualifies Holladay Properties. We are incredibly excited about the prospects of working with Downers Grove and thank you in advance for your consideration.

Holladay Properties is planning the development of an approximate 3.341 acre site on the Southeast corner of Ogden and Walnut. The property will be subdivided into three lots of 2.008 acres, 0.878 acres, and .4536 acres. The first phase of the development will include the construction of a 122 WoodSpring Suites branded hotel on the 2 acre site as well as the mass grading of the 0.8793 acre site and construction of all stormwater improvements including a wetland bottom detention basin on the .455 acre site and a below ground vault under the Southern parking lot of the hotel. Additionally, improvements to the curb and gutter system and an installation of a gravel shoulder road along Walnut will be made during the first phase.

The second phase of the project will include the development of a restaurants on the 0.878 acre site. There has been interest in the site from multiple groups but due to COVID-19 there are no final plans. The plans that are submitted show both an option for a sit down fast casual restaurant along with a



restaurant with a double drive thru lane. We are planning to submit both options to maintain flexibility for what is developed on that space.

With the development of a hotel and the emerging Sports Complex to the North of the site the site will be attractive to many concepts.

The zoning to the North is County R-1 and to the West is tollway property.

Holladay Properties, has reviewed the comprehensive plan and that the hotel and restaurant meets the intent and purpose of the business corridor and will be a great addition to Downers Grove. As the Ogden Avenue corridor is the principal access point to the village Holladay understands the importance of developing a well-designed, high quality, easily accessible, sustainable, visually appealing, and well landscaped development.

In addition to the design of the building Holladay Properties is committed to running and maintaining a thriving business that will bring jobs and tax revenue to the community.

This submittal is supplemented with several exhibits that visually show what the proposed development will look like along with background information on the WoodSpring Suites Hotel and of the developer Holladay Properties. If at any time additional information is needed please do not hesitate to contact Duffey Phelps of Holladay Properties at (202)365-8604.

## **B. DEVELOPER BACKGROUND**

### ***About Holladay Properties and Holladay Hospitality:***

Founded in 1952 by noted architect Wallace F. Holladay, Holladay Properties has grown and diversified from the construction of single-family tract housing to become a full-scale land development, design/build, and fully integrated commercial real estate company, as well as one of the largest medical office management firms in the country. Holladay has more than 200 full-time associates in a range of specialties working from five main development offices and more than 30 management offices in major metropolitan areas throughout the United States with the knowledge and experience to complete any type of project for any kind of client.

Holladay Properties is probably best known in Downers Grove for its multi-family project “Burlington Station” located at 5100 Forest Avenue. The 89-unit amenity filled apartment project has been a gigantic success that was the culmination of a fantastic developer/municipal partnership.

Holladay Properties is a privately held company owned by eleven active partners. Our company has completed an array of projects ranging in size from \$750,000 to \$75 million with a total valuation of over \$2 billion. Holladay has managed construction for over five million square feet of new space in the past 10 years and currently has over 3,000 acres of land under development throughout the eastern half of the U.S.

Holladay Properties entered the hospitality business in 1991, when we planned and developed our first hotel, a joint venture with the Sisters of the Holy Cross, the Inn at Saint Mary’s Hotel & Suites in South Bend, IN. Located on the campus of St. Mary’s College within the Notre Dame community, this luxury hotel quickly became the one of the finest independent hotels in the market.

Since 1991, Holladay has developed multiple internationally branded hotels as well as conference and meeting spaces as opportunities have presented themselves. These developments have usually been incorporated in larger land developments or in conjunction with major partners. Holladay



currently owns and maintains an active role in the management of 20 hotels in eight different markets.

Holladay Hospitality has grown to be recognized as one of the premier hotel owners and developers in the Midwest. We are proven leaders in the development of a wide spectrum of hotel brands, and put pride in our ability to achieve success through strong, hands-on management. This success is apparent when looking at Holladay's long list of recognitions on both brand and customer levels.

Holladay also distinguishes itself by being on the cutting edge of innovation. From development to construction management and customer service, we aim to be on the cutting edge of technology and design, ensuring that our properties remain highly marketable and guest friendly.

### ***Asset Management***

Holladay Properties takes great pride in its properties. Holladay is traditionally a long-term holder of its assets and operates its portfolio of hotels and buildings with the philosophy that strong preventative maintenance leads to better, more attractive buildings. We mandate that a significant portion of revenues go to a reserve fund each year for regular maintenance as well as large capital upgrades. Additionally, we conduct monthly inspections of the property on an owner level to ensure proper upkeep. This is in addition to the WoodSpring Suites brand who has an entire branch of their business that is dedicated to quality control. They conduct both planned and random inspections of the property and track over 500 various quality control items. What this means is that Downers Grove will have an asset that will look and perform just as well year one as it does year twenty-five.

## **C. WOODSPRING SUITES BACKGROUND**

In 2014, Holladay Properties acquired the territory rights to develop fifteen (15) WoodSpring Suites extended stay hotels in the Chicago and Northwest Indiana markets. Since then Holladay has opened 7 WoodSpring hotels in the Chicago area and have two more currently under development.

### ***About WoodSpring Suites***

In 2003, WoodSpring Suites was launched by Jack DeBoer - founder of Residence Inn, Summerfield Suites, and Candlewood Suites - to compete in the extended stay hospitality industry. WoodSpring currently has 230 units open of which 80 are corporately owned, with the balance owned by franchisees. There are 22 units under construction with an additional 50+ in various stages of entitlement. In 2019 Choice Hotels acquired the brand from Lindsey Goldberg and today Choice handles all franchise business. Choice hotels is a public company and is one of the largest hotel companies in the world.

At its core, WoodSpring Suites delivers a clean and comfortable living environment to its guests. The hotel has amenities that cater to longer term guests but also all the comforts and conveniences that transient guests demand as well. For additional information on WoodSpring Suites please see Exhibit B.

The target WoodSpring Suites guest include:

- Corporate Travelers
- Small business owners
- Medical professionals / Traveling Nurses
- Construction workers



- Contract workers
- Traveling youth sports teams and their families
- College students and teachers
- Military personnel and government contractors
- Families of hospitalized friends or family members
- Employees with temporary work assignments
- Individuals conducting or participating in job training
- Seasonal residents such as retirees, holiday visitors, and family visitors
- Individuals relocating to an area who are waiting for home to close or apartments to open up

The average length of stay at our properties across the Chicago area is 13.1 days and at locations with strong commercial activity around them like the Ogden site the average is drops to closer to 10 days on average.

The WoodSpring brand has a strong national sales team devoted to driving corporate users to our hotels. Additionally, unlike many of our competitors our management company also has a dedicated sales and rate management team to ensure that we are driving corporate sales. While we cater to leisure and local travelers as needed our goal is to fill the hotel with higher rated corporate business groups, travel sports groups, and contractors coming to the area.

An example of corporate accounts both local and national that have stayed at our Tinley Park location over the past year include:

Amazon  
 Athens Paper  
 Ingalls  
 Ford Motor Company  
 Borg Warner Construction  
 Lydon Steel  
 Michael Pipeline  
 Solar Sites  
 Union Pacific  
 Midwest Solar and Wind  
 Avik Services  
 Keurig / Dr. Pepper  
 Leibold Irrigation  
 Greeding Construction  
 Culver Equipment  
 Intermex  
 Leisure Spaces LLC  
 Weitz Commercial

### **Staffing**

When open the hotel will be staffed with between 10 – 15 employees and is seasonal based on hotel room demand. Positions include General Manager, Guest Service Attendants, Front Desk Manager, Head of Housekeeping, Housekeepers, Maintenance Technicians, and Custodians.



### **Projected Nightly Rates**

	<b><u>Low Season Avg.</u></b>	<b><u>Peak Season Avg.</u></b>
<b><u>Queen Suite</u></b>	\$79 - \$89	\$99 - \$129
<b><u>Double Queen Suite</u></b>	\$89 - \$109	\$109 - \$159

Due to the location and corporate business surrounding this property we expect rates in this market to be at a premium to our other locations in the Chicago area.

### **Sales Strategy**

- Use of digital media
  - Corporate Website bookings/ads
  - 3<sup>rd</sup> Party Websites. Also known as “OTAs” (Expedia, Booking.com, Kayak.com, etc)
  - Social Media Advertisements
  - Website Banner Ads
- Use of National Sales Team
  - Choice hotels and WoodSpring have sales associates dedicated to drumming up national sales accounts
- Use of regional sales team
  - Our management company has a regional sales team who organize “sales blitzes” and well as phone and email drives to local companies on at least a quarterly basis.
- Use of onsite management for sales
  - Our onsite management team is constantly scouring the market and developing relationships with business leaders in the area to drum up business and book room nights.
- Use of brand call center to handle call volume that can’t be taken at a property level.
- Use of national construction/infrastructure project databases to track construction contracts coming to the market.

## **D. ECONOMIC IMPACT – HOTEL AND RESTAURANT**

The WoodSpring Suites hotel and neighboring restaurant will have a significant economic impact on the Downers Grove economy:

- The creation of 300+ jobs during construction
- \$1.3m+ in spending on equipment and materials suppliers in the area
- Creation of \$4m+ in sales and room revenue when the properties are stabilized.
  - This translates to at least \$90,000+ in local sales and occupancy taxes (1% sales and 4.5% room occupancy tax) per year.
- 20 - 30 permanent well paying full and part-time jobs upon opening
- An influx of spending at local businesses related to the 130+ guests that will call Downers Grove home while traveling.
- A new hotel and restaurant that will spark additional interest from businesses along the Ogden Ave corridor.



## **E. ARCHITECTURE AND BUILDING DESIGN**

WoodSpring Suites recently hired the award winning architectural company BBR to completely redesign their prototype hotel. The redesign enhances the aesthetics of the exterior of the hotel to meet the expectations of the communities they plan to serve in the future. The hotels are built to be good looking, and smart functioning. This means using high grade and where possible environmentally friendly construction materials on all aspects of the building and using windows, lighting, HVAC systems, water, heaters, and appliances that are energy efficient. Please see Exhibit A - Site Development Plans (Sheets A1.1, A1.2, A2.1, & A2.2) for elevations and floorplans.

LED lights are used throughout the building and in the parking lot. These lights provide significant illumination for guest safety in the lots.

It should be note that the redesign BBR and WoodSpring Suites completed did not stop with just the exterior as the entire interior from the ground up was also redesigned. BBR and WoodSpring spent thousands of hours interviewing guests and potential guests on what their expectations and demands are. Armed with this information the hotel was completely reimagined from the lobby experience all the way through to the guest suite. New furniture, fixtures, beds, sheets, paint colors, and much more were picked out during this process and the results are outstanding.

### **Parking**

The project provides parking on both Lots 1 and 2 that is shared amongst the development. For the First Option which includes a hotel and a traditional sit-down restaurant we propose a total of 184 spaces including 8 accessible spaces. For this option 187 total spaces are required when the shared parking calculations are applied. The proposal for the hotel and traditional sit-down restaurant is 3 spaces short.

The second option is for a hotel with a drive-through restaurant, we propose a total of 182 spaces including 8 accessible spaces. For this option 165 total spaces are required when the shared parking calculations are applied. The parking minimum is met for option 2 and we have a surplus of 12 spaces over the minimum required.

Based on our prior experience with hotel development we do not expect to have any issues with parking. The parking count for the hotel use at other similar properties is equal to the number of rooms (122) + one spot for full time employees onsite at any given time or a total of 125 spots which is the same as what we are currently have for the proposed Ogden Avenue hotel. Generally, on nights when the hotel is full there are multiple rooms that have arrived in a single automobile. Additionally, as this hotel does not have a banquet center or restaurant its demand numbers are lower. Finally, there are people who stay with us who only use other modes of transportation like shuttle services, rideshare services, public transportation, and Taxi Cabs. It should also be noted that a shared parking easement will be put in place to ensure that overflow parking for either the restaurant or hotel is accounted for.

It also should be noted that the deviation is necessary to avoid significantly impacted a wetland that protrudes onto the site.



## F. WHY DOWNERS GROVE?

### Why Downers Grove?

*Lack of supply of hotels in class, strong demand generators, visibility and accessibility, amenity for Park District*

- *Hotel Supply Shortage in Market*
  - There is currently a void of high-quality extended stay hotels in the Downers Grove market. This void is leading to a loss of travelers looking to in Downers Grove to surrounding markets, most notably Oak Brook, Naperville, Warrenville and Lisle. There are multiple organizations in the market who have noted that they send their employees to hotels outside of Downers Grove because of this void. We want to keep these guests and the significant direct and indirect revenues in the form of occupancy and sales tax from travelers who would be living, dining, and shopping in the area.
- *Strong Demand Generators and Central Location*
  - Downers Grove and the surrounding areas have many demand generators that are key to a WoodSpring Suites' success. The site on Ogden is centrally located along I-355 with easy access to I-88. This access means we will be able to pull business from corporations located in multiple communities along these corridors. These communities all have demands drivers including construction workers, healthcare facilities, and over 10,000 businesses who employ contractors that have the need for temporary accommodations.
- *Accessibility*
  - WoodSpring Suites brand and marketing teams have completed countless studies that show the importance of visibility and accessibility to attract guests. With over 128,000 cars passing by the site on I-355, I-88 and Ogden Avenue daily the site is ideally located to gain lots of traffic. Customers of extended stay hotels often look for a diverse group of restaurants and shops when picking their hotel; this location has both.
- *Park District Accommodations / Restaurant*
  - The Downers Grove Park District has a long term plan to develop a multi-use sports facility just to the South of our site. Our hotel and future restaurant will be a fantastic compliment to the sports complex. Additionally, the site plan was created with the project in mind so that in the future there will be seamless access between the two sites.

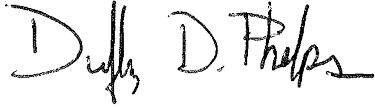
Holladay Properties and WoodSpring Suites work together to analyze and scrutinize sites, both analytically and qualitatively, and believe that a WoodSpring Suites hotel will perform well along the in the Ogden Ave corridor. Of note are the 140 businesses in the area who have 150+ employees along with the perfect score (10/10) for "WoodSpring Industry Concentration" which includes industries in the manufacturing, transportation, utilities, and construction sectors. These larger businesses typically bring a high volume of room nights to the area.



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Once again thank you for your consideration. If you have any questions, comments, and/or feedback please do not hesitate to contact me.

Sincerely,



Duffey Phelps  
Vice President – Development Manager  
Holladay Properties



## FINDING OF FACTS

### Review and Approval Criteria ZONING MAP AMENDMENTS

Plan Commission Number & Title: \_\_\_\_\_

A DETAILED RESPONSE TO ALL OF THE STANDARDS SHALL BE PROVIDED, SPECIFYING HOW EACH STANDARD IS OR IS NOT MET.

*Section 28.12.030.I. Review and Approval Criteria (Zoning Map Amendments - Rezonings)*  
*The decision to amend the zoning map is a matter of legislative discretion that is not controlled by any single standard. In making recommendations and decisions about zoning map amendments, review and decision-making bodies must consider at least the following factors.*

**1) *The existing uses and zoning of nearby property.***

The current use of the subject site is vacant land and it is zoned ORM. The surrounding properties to the north, east, south, and west are zoned B-3, R-1, and R-4. Immediately to the North and West is undeveloped land and immediately to the East is Granger Supply Company. Immediate to the South is the future Walnut Sport Park. The required building setbacks have been maintained with adjacent properties. The proposed use and development is appropriate as compared to the surrounding zoning and uses. This standard has been met.

**2) *The extent to which the particular zoning restrictions affect property values.***

The PUD overlay and the proposed development will enhance the value of adjacent properties by adding a significant amenity to the area. The subject property will be improved using thoughtful design and construction and will certainly not negatively impact property prices. This standard has been met.

**3) *The extent to which any diminution in property values is offset by an increase in the public health, safety, and welfare.***

The propose rezoning will not impact property values or the public health, safety, and welfare of the community or neighborhood in a negative way. The property will be developed to provide improvements that increase the public health, safety, and welfare. This standard has been met.

**4) *The suitability of the subject property for the zoned purposes.***

The property proposed PUD overlay will enhance the suitability of the proposed use for the subject property. The PUD overlay will allow



**5) *The length of time that the subject property has been vacant as zoned, considering the context of land development in the vicinity.***

The property has been vacant as zone for over 10 years. While there have been multiple attempts and concepts for the property none have come to fruition. The proposed use has been met with enthusiasm from the Park District and meets the expectations that are laid out in the comprehensive plan. This standard has been met.

**6) *The value to the community of the proposed use.***

The property is identified within the *Ogden Avenue Key Focus Areas* in the Comprehensive Plan. Our proposal will provide a resource that can be used by both residents and guests to the village. The proposal will improve the area through the enhancement of landscaping, pedestrian access, and connectivity, while limiting curb cuts and maintain good traffic circulation. Rezoning the property for the PUD overlay will allow options for the applicant to incorporate the key concepts and objectives identified in the Comprehensive Plan at a prime location within the Village. This standard has been met.

**7) *The Comprehensive Plan***

The proposed PUD overlay and the proposed project take into account that the property is a Catalytic Site in the Ogden Avenue Key Focus Area and the use is consistent with the Comprehensive Plan. The project will attract a high level of visitors and revenue to the area. This standard has been met.



## FINDING OF FACTS

### Review and Approval Criteria

#### PLANNED UNIT DEVELOPMENT

Plan Commission Number & Title:

A DETAILED RESPONSE TO ALL OF THE STANDARDS SHALL BE PROVIDED, SPECIFYING HOW EACH STANDARD IS OR IS NOT MET.

#### *Section 28.12.040.C.6 Review and Approval Criteria (Planned Unit Development)*

*The decision to amend the zoning map to approve a PUD development plan and to establish a PUD overlay district are matters of legislative discretion that are not controlled by any single standard. In making recommendations and decisions regarding approval of planned unit developments, review and decision-making bodies must consider at least the following factors:*

**1. *The zoning map amendment review and approval criteria of Sec. 12.030.I.***

See the analysis of zoning map amendment review and approval criteria in separate document.

**2. *Whether the proposed PUD development plan and map amendment would be consistent with the Comprehensive Plan and any other adopted plans for the subject area.***

The proposed project is consistent with the Comprehensive Plan. The Plan identifies this area a Catalyst Site within the *Ogden Avenue Key Focus Area*. A PUD overlay will provide the necessary tools to develop the property with a hotel and restaurant that will drive high levels a business to the site while addressing key concepts of improving circulation, access, screening, and safety per the current comprehensive plan.

**3. *Whether PUD development plan complies with the PUD overlay district provisions of Sec. 4.030.***

The proposed project meets many of the PUD overlay district provisions and objectives as found in Section 4.030 of the Zoning Ordinance. The project will be mixed use and include both a hotel and a restaurant which are consistent uses with what is suggested in the Comprehensive Plan. The PUD will allow for flexibility in developing a creative project that allows a hotel and restaurant to coexist on the site and by doing so will offer a substantial benefit to the Park District project to the South.

**4. *Whether the proposed development will result in public benefits that are greater than or at least equal to those that would have resulted from development under conventional zoning regulations.***

The project as proposed will result in the expansion of existing businesses that call the Village of Downers Grove home. It will also be a resource for businesses to use while visiting the area and could spark future growth and business demand in the area. The improved land will provide numerous benefits that would not be possible under the conventional zoning regulations. This standard has been met.



***5. Whether appropriate terms and conditions have been imposed on the approval to protect the interests of surrounding property owners and residents, existing and future residents of the PUD and the general public.***

The petitioner has worked with Village staff to develop a site plan that optimizes the potential of the site and brings our cohesive plan together. This includes increased pedestrian access and safety and promoting efficient circulation in and around the project. The petitioner also will install high quality landscaping and buffering to protect the interests of surrounding business owners, residents, and the general public. The project will be a model to future projects and advance the goals and objective laid out in the Comprehensive Plan. This standard has been met.



Special Use Criteria.

*No special use may be recommended for approval or approved unless the respective review or decision-making body determines that the proposed special use is consistent with and in substantial compliance with all Village Council policies and plans and that the applicant has presented evidence to support each of the following conclusions:*

- 1. That the proposed use is expressly authorized as a special use in the district in which it is to be located;***

This project requires a special use for a drive-thru at a restaurant. The district it is located in specifically states that this is an authorized use.

- 2. That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community;***

Offering drive-thru services have become increasingly popular at restaurants and will maximize the potential uses of the potential restaurant users in the future. The potential drive thru restaurant will add to the dining options of Downers Grove residents and visitors to the area and add a convenient way for them to pick up food if they decide not to dine in.

- 3. that the proposed use will not, in the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.***

The proposed use will not be detrimental to the health, safety, or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity. A traffic study has been prepared that shows that the traffic generated by the drive thru will no impact public convenience.





1 Walker Avenue  
Suite 300  
Clarendon, Hills IL 60514  
[www.holladayproperties.com](http://www.holladayproperties.com)

September 2, 2021

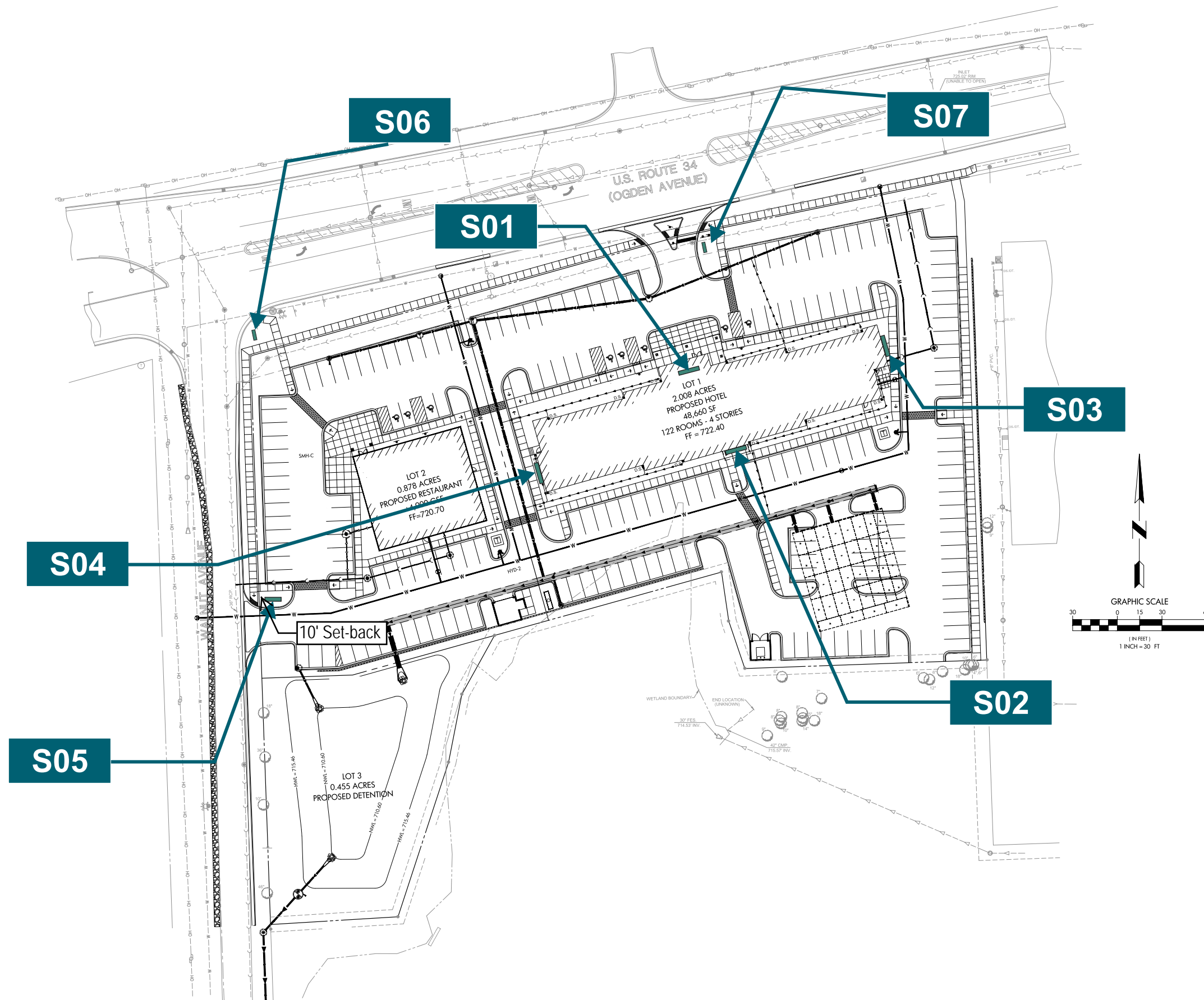
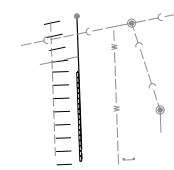
Notices for the neighborhood meeting were sent out on Monday August 9<sup>th</sup>, 2021 via Certified Mail to all neighbors within 250 ft of the subject site.

A neighborhood meeting was held in the multipurpose room of the Downers Grove Recreation Center on Monday August 23<sup>rd</sup>, 2021 from 6:00pm – 7:00pm CST. We had a presentation and handouts that detailed the project ready to go but nobody from the neighborhood attended.

Prior to the meeting an employee from Grainger which is located to the East of the property contacted us regarding the letter we sent to ask about the overall development and what restaurant and hotel would be part of the plans. They had no objections to the use.

Duffey Phelps  
Vice President – Holladay Properties.

# SITE MAP



ADDRESS: 2751 Ogden Ave | Downers Grove, IL

DRAWN BY: MRA

DATE: 07/09/2021

REVISION:

PAGE: 2

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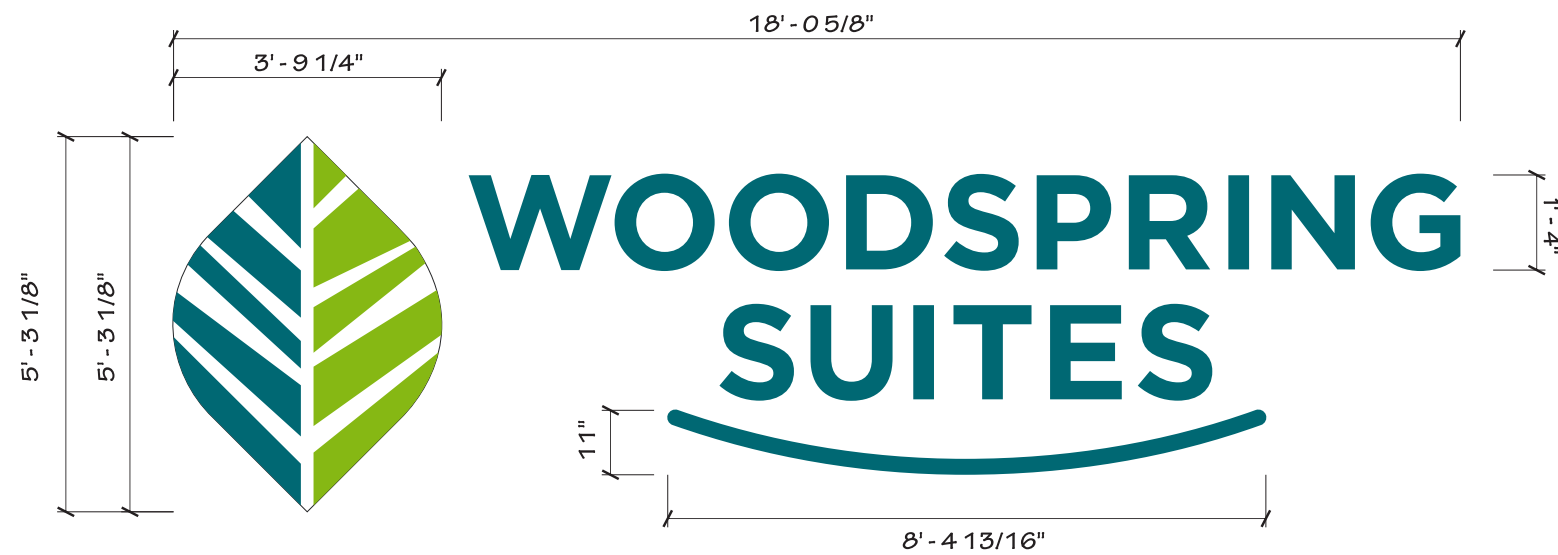
2655 International Parkway  
Virginia Beach, VA 23452

# PROPOSED: SIGN S01 - CLH-16

Code Allowance	Proposed
(1) wall sign per street frontage with the exception of buildings 4 stories tall are allowed (3) wall signs NTE 100 square feet.	(4) Wall signs at 95 square feet.



Front Elevation



WOODSPRING SUITES HORIZONTAL CHANNEL LETTERS

SIGN TYPE	"W" HEIGHT	LOGO HEIGHT	LOGO LENGTH	UNDERSCORE HEIGHT	UNDERSCORE LENGTH	OVERALL HEIGHT	OVERALL LENGTH	BOXED SQUARE FOOTAGE
	A	B	C	D	E	F	G	
CLH-16	16"	5' - 3 1/8"	3' - 9 1/4"	11"	8' - 4 13/16"	5' - 3 1/8"	18' - 0 5/8"	94.96

**SIGN DETAILS:**

Sign Type: CLH-16

Description: Channel Letters

Restoration SOW:

Electrical SOW: To be provided to sign location by others

Landscaping SOW: N/A

Comments: 5' - 3 1/8" OAH  
Proposed - 94.5+ sq. ft.



ADDRESS: 2751 Ogden Ave | Downers Grove, IL

DRAWN BY: MRA

DATE: 07/09/2021

REVISION:

PAGE: 3

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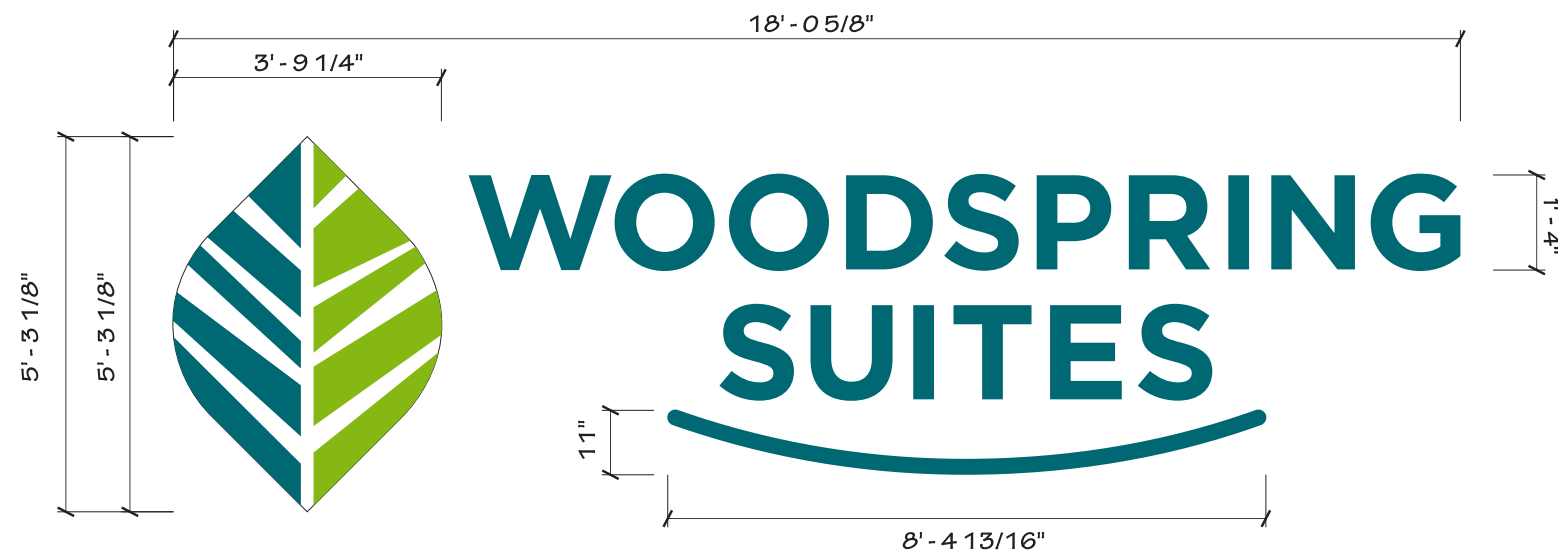
2655 International Parkway  
Virginia Beach, VA 23452

# PROPOSED: SIGN S02 - CLH-16

Code Allowance	Proposed
(1) wall sign per street frontage with the exception of buildings 4 stories tall are allowed (3) wall signs NTE 100 square feet.	(4) Wall signs at 95 square feet.



Back Elevation



**SIGN DETAILS:**

Sign Type: CLH-16

Description: Channel Letters

Restoration SOW:

Electrical SOW: To be provided to sign location by others

Landscaping SOW: N/A

Comments: 5' - 3 1/8" OAH  
Proposed - 94.5+ sq. ft.

WOODSPRING SUITES HORIZONTAL CHANNEL LETTERS

SIGN TYPE	"W" HEIGHT	LOGO HEIGHT	LOGO LENGTH	UNDERSCORE HEIGHT	UNDERSCORE LENGTH	OVERALL HEIGHT	OVERALL LENGTH	BOXED SQUARE FOOTAGE
	A	B	C	D	E	F	G	
CLH-16	16"	5' - 3 1/8"	3' - 9 1/4"	11"	8' - 4 13/16"	5' - 3 1/8"	18' - 0 5/8"	94.96



ADDRESS: 2751 Ogden Ave | Downers Grove, IL

DRAWN BY: MRA

DATE: 07/09/2021

REVISION:

PAGE: 4

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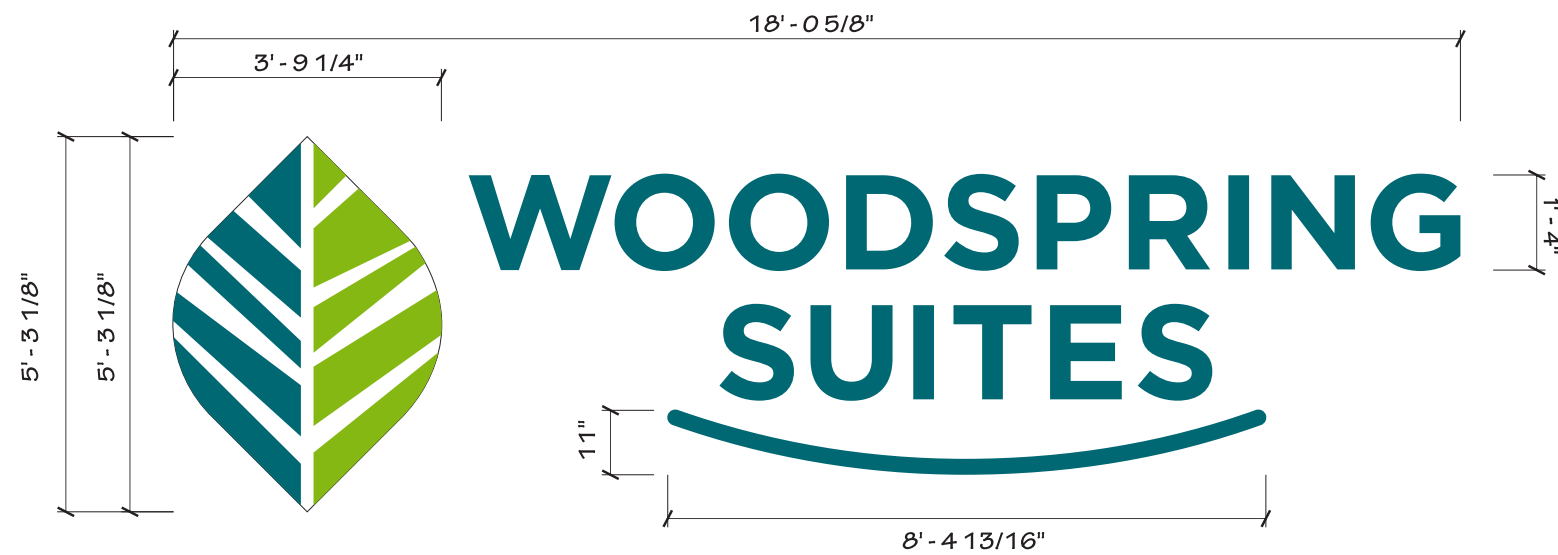
2655 International Parkway  
Virginia Beach, VA 23452

# PROPOSED: SIGN S03 - CLH-16

Code Allowance	Proposed
(1) wall sign per street frontage with the exception of buildings 4 stories tall are allowed (3) wall signs NTE 100 square feet.	(4) Wall signs at 95 square feet.



North Side Elevation



**SIGN DETAILS:**

Sign Type: CLH-16

Description: Channel Letters

Restoration SOW:

Electrical SOW: To be provided to sign location by others

Landscaping SOW: N/A

Comments: 5' - 3 1/8" OAH  
Proposed - 94.5+ sq. ft.

WOODSPRING SUITES HORIZONTAL CHANNEL LETTERS								
SIGN TYPE	"W" HEIGHT	LOGO HEIGHT	LOGO LENGTH	UNDERSCORE HEIGHT	UNDERSCORE LENGTH	OVERALL HEIGHT	OVERALL LENGTH	BOXED SQUARE FOOTAGE
	A	B	C	D	E	F	G	
CLH-16	16"	5' - 3 1/8"	3' - 9 1/4"	11"	8' - 4 13/16"	5' - 3 1/8"	18' - 0 5/8"	94.96



ADDRESS: 2751 Ogden Ave | Downers Grove, IL

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DRAWN BY: MRA

DATE: 07/09/2021

REVISION:

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PAGE: 5

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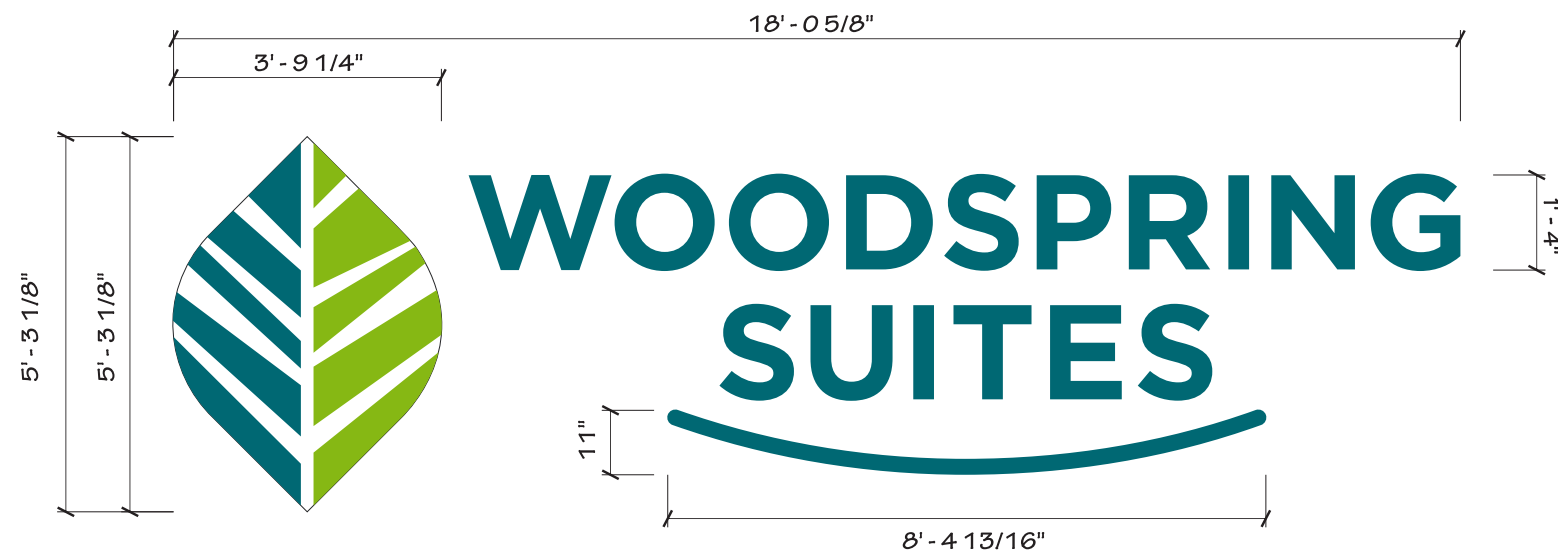


# PROPOSED: SIGN S04 - CLH-16

Code Allowance	Proposed
(1) wall sign per street frontage with the exception of buildings 4 stories tall are allowed (3) wall signs NTE 100 square feet.	(4) Wall signs at 95 square feet.



South Side Elevation



**SIGN DETAILS:**

Sign Type: CLH-16

Description: Channel Letters

Restoration SOW:

Electrical SOW: To be provided to sign location by others

Landscaping SOW: N/A

Comments: 5' - 3 1/8" OAH  
Proposed - 94.5+ sq. ft.

WOODSPRING SUITES HORIZONTAL CHANNEL LETTERS

SIGN TYPE	"W" HEIGHT	LOGO HEIGHT	LOGO LENGTH	UNDERSCORE HEIGHT	UNDERSCORE LENGTH	OVERALL HEIGHT	OVERALL LENGTH	BOXED SQUARE FOOTAGE
	A	B	C	D	E	F	G	
CLH-16	16"	5' - 3 1/8"	3' - 9 1/4"	11"	8' - 4 13/16"	5' - 3 1/8"	18' - 0 5/8"	94.96



ADDRESS: 2751 Ogden Ave | Downers Grove, IL

DRAWN BY: MRA

DATE: 07/09/2021

REVISION:

PAGE: 6

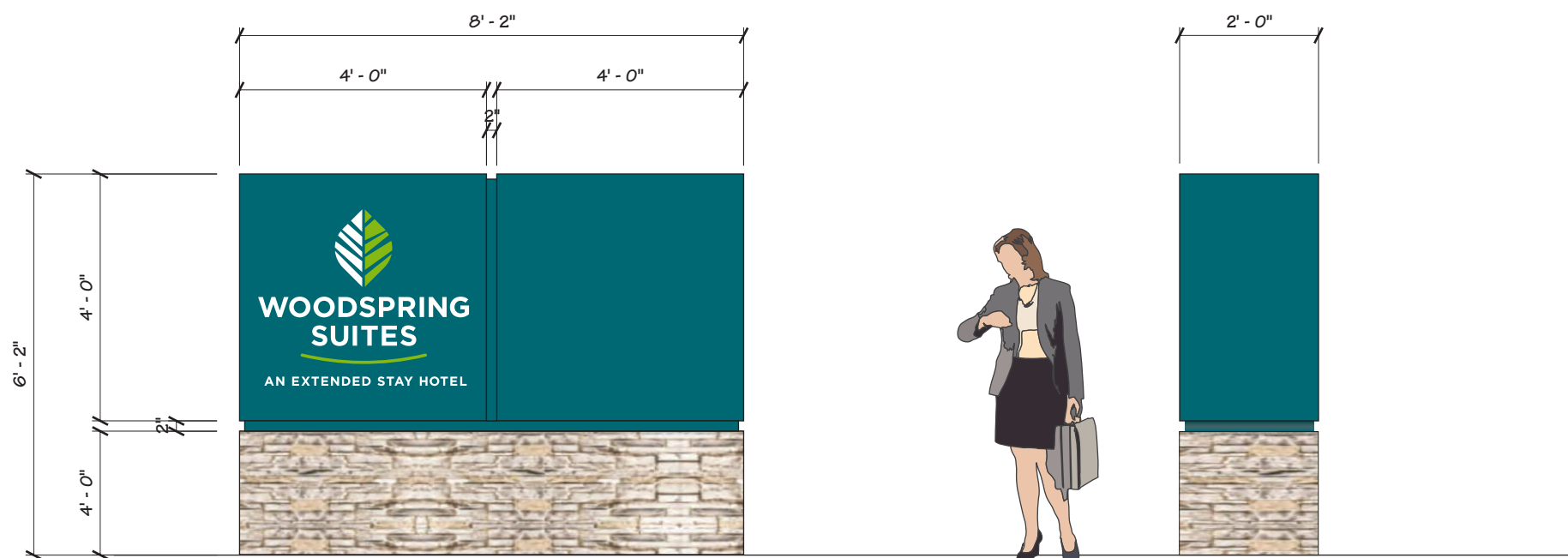
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2655 International Parkway  
Virginia Beach, VA 23452

# PROPOSED: SIGN S05, S06 and S07 - CUSTOM MONUMENT

Code Allowance	Proposed
15' OAH, 60 square feet	6' OAH, 32 square feet



**Custom Monument**  
 Main ID - 16 Sq.Ft.  
 Tenant Cabinet - 16 Sq.Ft.  
 Total - 32 Sq.Ft.  
 Scale: 3/8" = 1'-0"

**SIGN DETAILS:**

**Sign Type:** Custom Monument Sign

**Description:** Painted aluminum face with 3/4" push-thru acrylic letters with surface digitally printed green and white vinyl.

**Electrical SOW:** To be provided to sign location by others

**Landscaping SOW:** TBD

**Comments:** 6' - 0" OAH  
 Proposed - 32 sq. ft.



ADDRESS: 2751 Ogden Ave | Downers Grove, IL

DRAWN BY: MRA

DATE: 07/09/2021

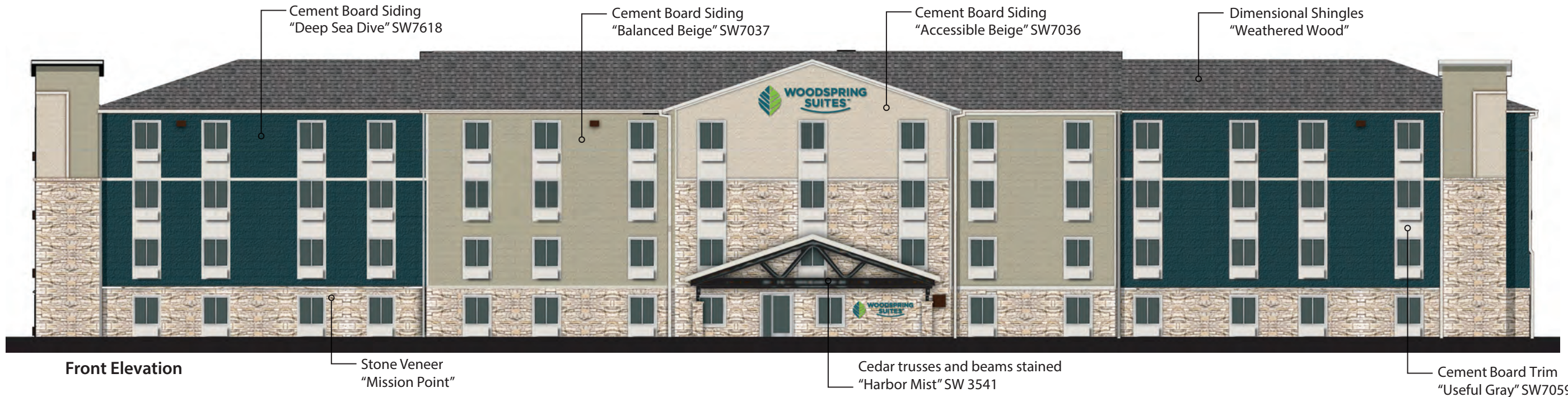
REVISION:

PAGE: 8

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2655 International Parkway  
 Virginia Beach, VA 23452



Front Elevation



Back Elevation



North Side Elevation



South Side Elevation

WoodSpring Suites  
Exterior Elevations  
Downers Grove, IL







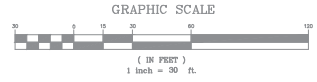
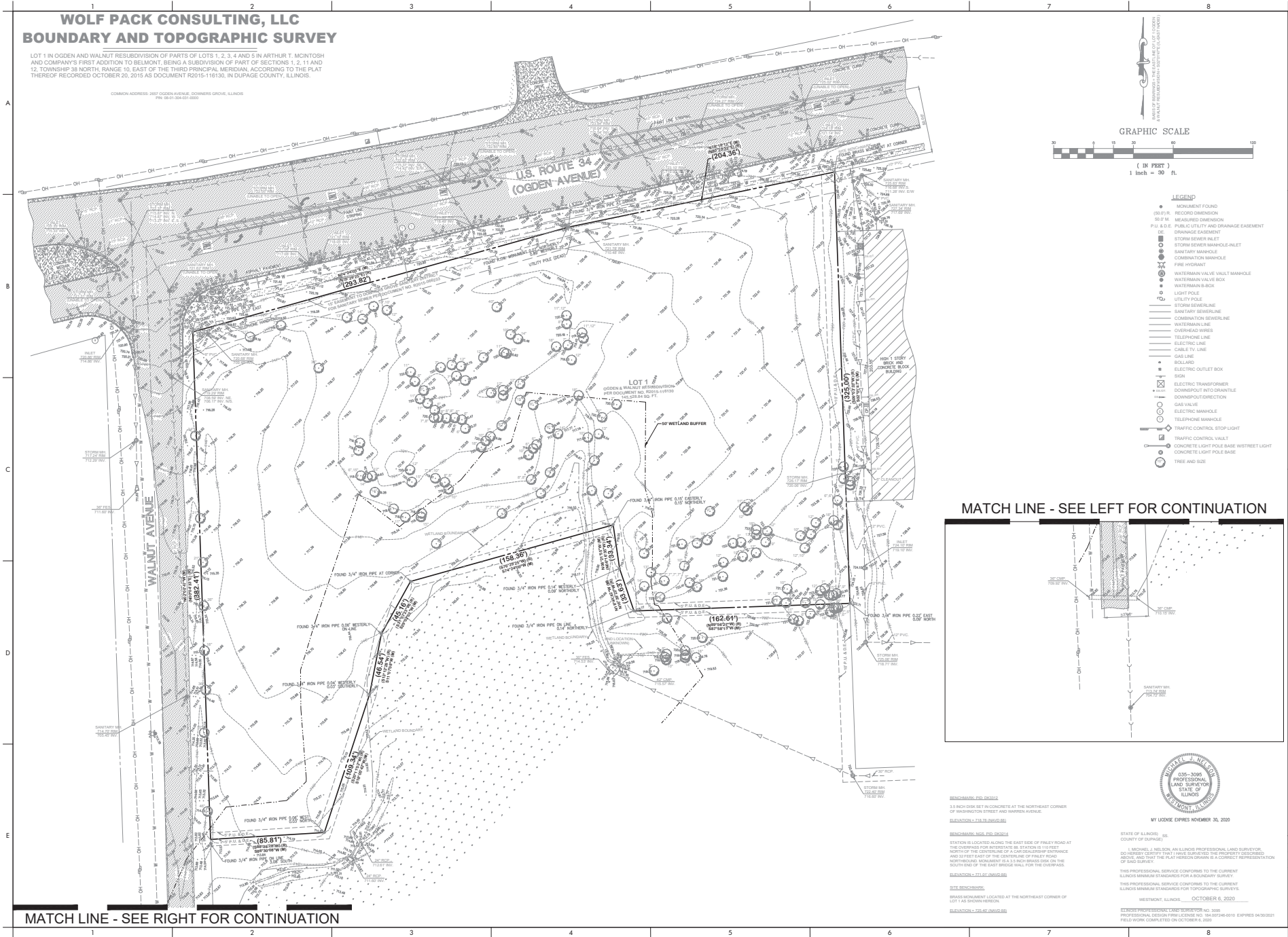




# WOLF PACK CONSULTING, LLC BOUNDARY AND TOPOGRAPHIC SURVEY

LOT 1 IN OGDEN AND WALNUT RESUBDIVISION OF PARTS OF LOTS 1, 2, 3, 4 AND 5 IN ARTHUR T. MCINTOSH AND COMPANY'S FIRST ADDITION TO BELMONT, BEING A SUBDIVISION OF PART OF SECTIONS 1, 2, 11 AND 12, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 20, 2015 AS DOCUMENT #0215-116130, IN DUPAGE COUNTY, ILLINOIS.

COMMON ADDRESS: 2877 OGDEN AVENUE, DOWNERS GROVE, ILLINOIS  
P.O. BOX 504030, CHICAGO, ILLINOIS 60650



- LEGEND**
- MONUMENT FOUND
  - RECORD DIMENSION
  - MEASURED DIMENSION
  - P.U. & D.E. PUBLIC UTILITY AND DRAINAGE EASEMENT
  - DRAINAGE EASEMENT
  - STORM SEWER INLET
  - STORM SEWER MANHOLE-INLET
  - SANITARY MANHOLE
  - COMBINATION MANHOLE
  - FIRE HYDRANT
  - WATERMAIN VALVE VALVE BOX
  - WATERMAIN VALVE BOX
  - WATERMAIN BOX
  - LIGHT POLE
  - UTILITY POLE
  - STORM SEWERLINE
  - SANITARY SEWERLINE
  - COMBINATION SEWERLINE
  - WATERMAIN LINE
  - OVERHEAD WIRES
  - TELEPHONE LINE
  - ELECTRIC LINE
  - CABLE TV LINE
  - GAS LINE
  - ELECTRIC OUTLET BOX
  - BOLLARD
  - SIGN
  - ELECTRIC TRANSFORMER
  - DOWNPOUT INTO GRANITILE
  - DOWNPOUT DIRECTION
  - GAS VALVE
  - ELECTRIC MANHOLE
  - TELEPHONE MANHOLE
  - TRAFFIC CONTROL STOP LIGHT
  - TRAFFIC CONTROL VALVE
  - CONCRETE LIGHT POLE BASE WITH RET LIGHT
  - CONCRETE LIGHT POLE BASE
  - TREE AND SIZE

REVISIONS

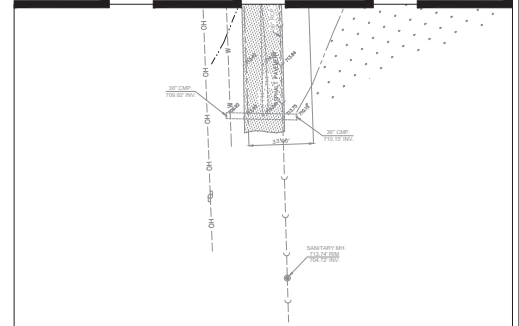
NO.	DATE	DESCRIPTION
1	08/03/21	REVISED PER VALUAGE REVIEW
2	07/02/21	REVISED PER VALUAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

**CivWorks Consulting, LLC**

3030 N. NEW AVE. SUITE 400  
CHICAGO, ILLINOIS 60654  
PH: 872.607.9770  
E-MAIL: info@civworks.com  
WWW.CIVWORKS.COM

MATCH LINE - SEE LEFT FOR CONTINUATION



MATCH LINE - SEE RIGHT FOR CONTINUATION

**PROFESSIONAL LAND SURVEYOR**  
STATE OF ILLINOIS  
BY LICENSE EXPIRES NOVEMBER 30, 2020

STATE OF ILLINOIS, ILLINOIS  
COUNTY OF DUPAGE  
I, MICHAEL J. NELSON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, DO HEREBY CERTIFY THAT I HAVE SURVEYED THE PROPERTY DESCRIBED ABOVE, AND THAT THE PLAT THEREON IS A CORRECT REPRESENTATION OF SAID SURVEY.

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR TOPOGRAPHIC SURVEYS.

WESTMONT, ILLINOIS, OCTOBER 6, 2020

EXTENDED PROFESSIONAL LAND SURVEYOR NO. 3036  
PROFESSIONAL DESIGN FIRM LICENSE NO. 04-00746-010 EXPIRES 03/31/2021  
FIELD WORK COMPLETED ON OCTOBER 6, 2020

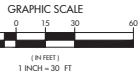
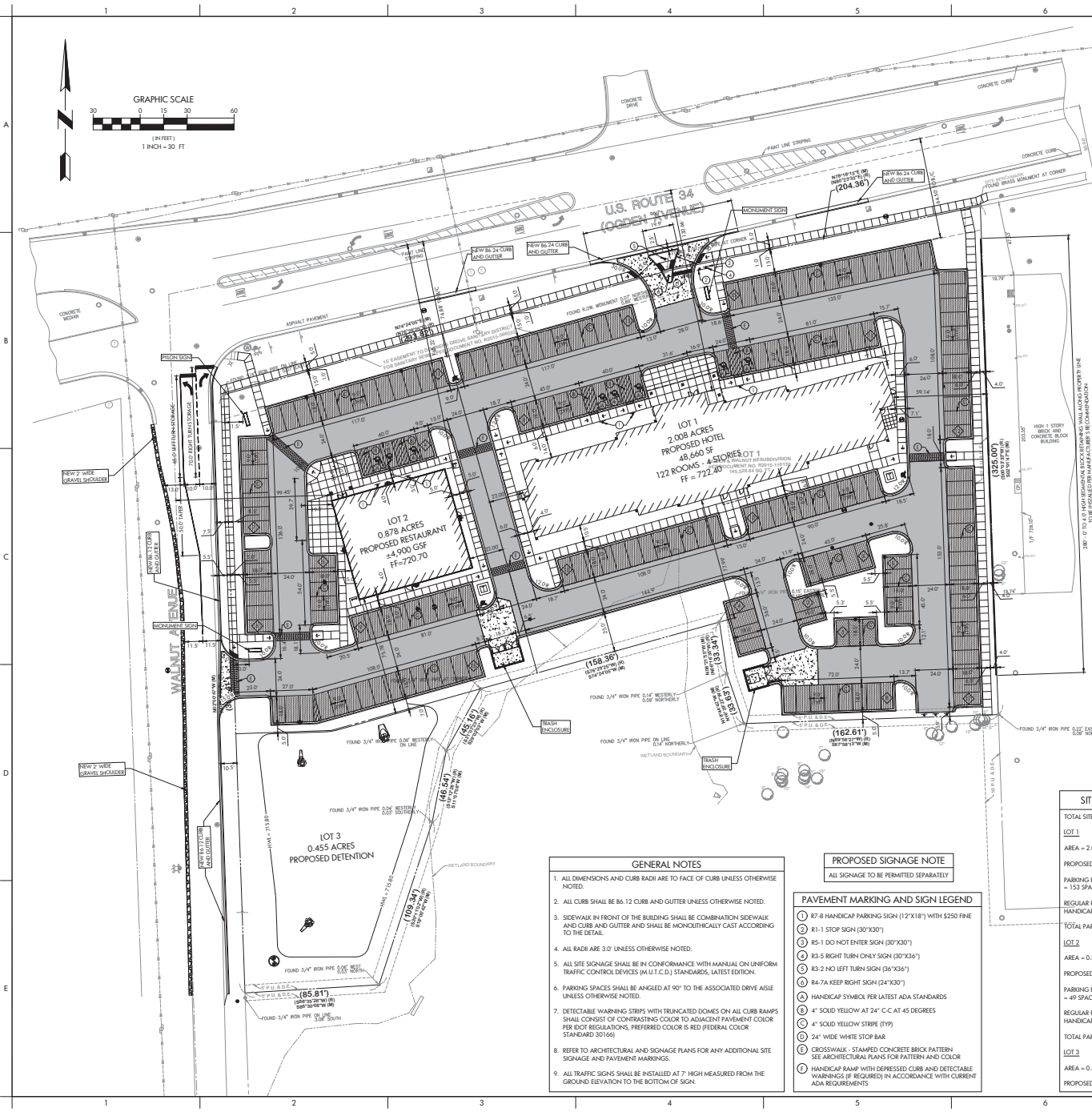
EXISTING CONDITIONS

PROPOSED WOODSPRING SUITES HOTEL

2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515

SHEET NO. **C2.0**

PROJ. NUMBER: 19006



LAND USE (REQUIRED)	WEEKDAY		WEEKEND	
	7:00 AM - 7:00 PM	6:00 PM - 6:00 PM	7:00 AM - 7:00 PM	6:00 PM - 6:00 PM
LODGING (153)	100% = 153	80% = 92	100% = 153	80% = 92
EATING AND DRINKING (49)	50% = 25	70% = 34	100% = 49	45% = 22
<b>TOTAL REQUIRED</b>	<b>178</b>	<b>126</b>	<b>187</b>	<b>133</b>

TOTAL PARKING PROVIDED = 184 SPACES. THEREFORE THE OVERALL SITE WILL BE 3 SPACES SHORT DURING THE PEAK PARKING TIME.

ZONING ANALYSIS	
PROJECT NAME	PROPOSED WOODSPRING SUITES HOTEL
ADDRESS	2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515
PIN(s)	08-01-304-031
ZONING DISTRICT	EXISTING: O-R-M; OFFICE - RESEARCH - MANUFACTURING PROPOSED: B-3; GENERAL SERVICES AND HIGHWAY BUSINESS
MINIMUM DISTRICT AREA	MIN. 4 ACRES; PARCEL AREA = 4.31 ACRES
EXISTING USE	VACANT LAND
PROPOSED USE	WOODSPRING SUITES HOTEL AND FUTURE SIT DOWN RESTAURANT
PETITION TYPE	P.U.D.
DEVIATIONS	LOT 1: 1. REDUCED NUMBER OF ON-SITE PARKING SPACES LOT 2: 1. NONE OVERALL DEVELOPMENT: 1. REDUCED NUMBER OF SHARED PARKING SPACES

LOT 1 - PROPOSED WOODSPRING SUITES HOTEL					
REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	87,465 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	47 FEET	YES	-13 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.56	YES	-0.19
BELG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	130.64 FEET	YES	+55.64 FEET
BELG. SIDE SETBACK (EAST)	MINIMUM	0.0 FEET	59.14 FEET	YES	+59.14 FEET
BELG. SIDE SETBACK (WEST - INTERIOR)	MINIMUM	0.0 FEET	22.00 FEET	YES	+22.00 FEET
BELG. SIDE SETBACK (SOUTH)	MINIMUM	10.0 FEET	77.99 FEET	YES	+67.99 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	64.50 FEET TO B/C	YES	+14.50 FEET
PARKING SETBACK (EAST)	MINIMUM	5.0 FEET	10.00 FEET	YES	+5.00 FEET
PARKING SETBACK (WEST - INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (SOUTH)	MINIMUM	5.0 FEET	5.0 FEET	YES	0.0 FEET
TOTAL PARKING PROVIDED	MINIMUM	125 (ROOM (153))	125	NO	-8 SPACES
TWO-WAY DRIVE AISLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
OPEN SPACE	MINIMUM	8,247 S.F.	19,165 S.F.	YES	+10,918 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	4,373 S.F.	4,978 S.F.	YES	+605 S.F.

LOT 2 - FUTURE SIT DOWN RESTAURANT (USER TO BE DETERMINED)					
REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	38,304 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	25 FEET (PARAPET)	YES	-35 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.06	YES	-0.69
BELG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	126.92 FEET	YES	+51.92 FEET
BELG. SIDE SETBACK (EAST - INTERIOR)	MINIMUM	10.0 FEET	49.53 FEET	YES	+39.53 FEET
BELG. SIDE SETBACK (WEST - WALNUT)	MINIMUM	35.0 FEET	80.53 FEET	YES	+45.53 FEET
BELG. SIDE SETBACK (SOUTH)	MINIMUM	10.0 FEET	80.23 FEET	YES	+70.23 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	74.88 FEET TO B/C	YES	+24.88 FEET
PARKING SETBACK (EAST - INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (WEST - WALNUT)	MINIMUM	8.0 FEET	8.5 FEET	YES	0.5 FEET
PARKING SETBACK (SOUTH)	MINIMUM	5.0 FEET	5.0 FEET	YES	0.0 FEET
TOTAL PARKING PROVIDED	MINIMUM	107 (K SF (49))	59	YES	-48 SPACES
TWO-WAY DRIVE AISLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
BICYCLE PARKING	MINIMUM	2 SPACES	4 SPACES	YES	+2 SPACES
OPEN SPACE	MINIMUM	3,830 S.F.	6,766 S.F.	YES	+2,936 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	1,915 S.F.	3,378 S.F.	YES	+1,463 S.F.

SITE DEVELOPMENT SUMMARY	
TOTAL SITE AREA = 3.341+ ACRES	
<b>LOT 1</b>	
AREA = 2,008 ACRES	
PROPOSED HOTEL = 48,640 GSF - 122 ROOMS	
PARKING PROVIDED = 125 SPACES / ROOM = 153 SPACES	
REGULAR PARKING PROVIDED = 120 SPACES	
HANDICAP PARKING PROVIDED = 5 SPACES	
TOTAL PARKING PROVIDED = 125 SPACES	
<b>LOT 2</b>	
AREA = 0.878 ACRES	
PROPOSED RESTAURANT = 4,900 GSF	
PARKING PROVIDED = 10 SPACES / 1,000 SF = 49 SPACES	
REGULAR PARKING PROVIDED = 56 SPACES	
HANDICAP PARKING PROVIDED = 3 SPACES	
TOTAL PARKING PROVIDED = 59 SPACES	
<b>LOT 3</b>	
AREA = 0.455 ACRES	
PROPOSED WETLAND BOTTOM DETENTION BASIN	

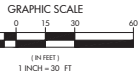
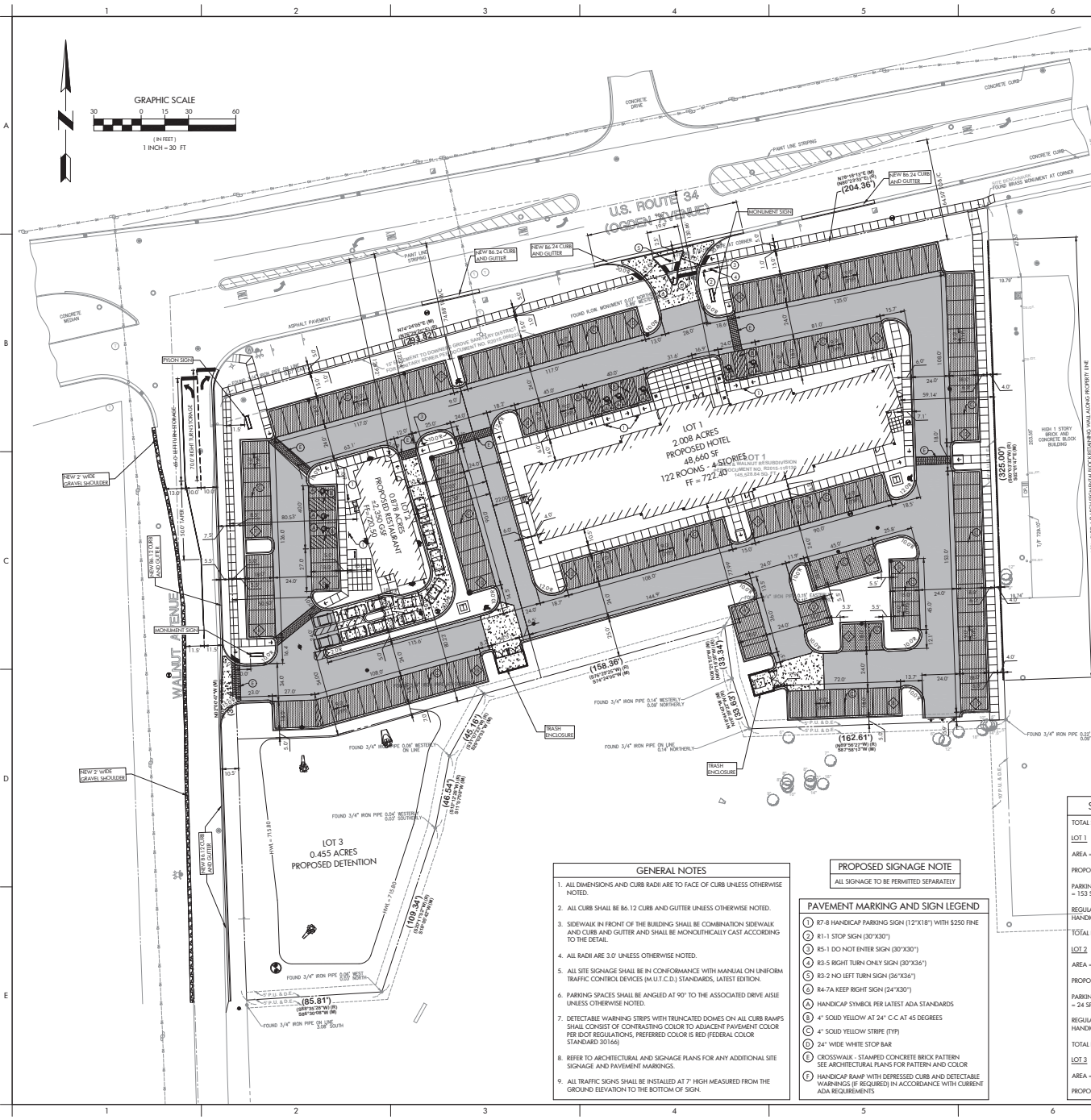
SITE PAVING LEGEND	
	HEAVY DUTY ASPHALT PAVEMENT 2" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, NSD 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, L-19, NSD 8" AGGREGATE BASE COURSE, TYPE B (DOT GRAD CA-6)
	LIGHT DUTY ASPHALT PAVEMENT 2" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, NSD 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, L-19, NSD 1.5" AGGREGATE BASE COURSE, TYPE B (DOT GRAD CA-6)
	CONCRETE PAVEMENT 8" F.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	STAMPED CONCRETE PAVEMENT 8" F.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	SIDEWALK PAVEMENT 5" F.C. CONCRETE PAVEMENT (THICKEN TO 8" AT DRIVEWAYS) 4" AGGREGATE BASE (CA-6)

- GENERAL NOTES**
- ALL DIMENSIONS AND CURB RADI ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - ALL CURB SHALL BE 8.12 CURB AND GUTTER UNLESS OTHERWISE NOTED.
  - SIDEWALK IN FRONT OF THE BUILDING SHALL BE COMBINATION SIDEWALK AND CURB AND GUTTER AND SHALL BE MONOLITHICALLY CAST ACCORDING TO THE DETAIL.
  - ALL RADI ARE 3.0' UNLESS OTHERWISE NOTED.
  - ALL SITE SIGNAGE SHALL BE IN CONFORMANCE WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) STANDARDS, LATEST EDITION.
  - PARKING SPACES SHALL BE ANGLED AT 90° TO THE ASSOCIATED DRIVE AISLE UNLESS OTHERWISE NOTED.
  - DETECTABLE WARNING STRIPS WITH TRUNCATED DOMES ON ALL CURB RAMPS SHALL CONSIST OF CONTRASTING COLOR TO ADJACENT PAVEMENT COLOR PER DOT REGULATIONS, PREFERRED COLOR IS RED (FEDERAL COLOR STANDARD 30166)
  - REFER TO ARCHITECTURAL AND SIGNAGE PLANS FOR ANY ADDITIONAL SITE SIGNAGE AND PAVEMENT MARKINGS.
  - ALL TRAFFIC SIGNS SHALL BE INSTALLED AT 7' HIGH MEASURED FROM THE GROUND ELEVATION TO THE BOTTOM OF SIGN.

- PROPOSED SIGNAGE NOTE**  
ALL SIGNAGE TO BE PERMITTED SEPARATELY
- PAVEMENT MARKING AND SIGN LEGEND**
- 107-B HANDICAP PARKING SIGN (12'X18") WITH S250 FINE
  - R1-1 STOP SIGN (30"X30")
  - R5-1 DO NOT ENTER SIGN (30"X30")
  - R3-5 RIGHT TURN ONLY SIGN (30"X36")
  - R3-2 NO LEFT TURN SIGN (30"X36")
  - R4-7A KEEP RIGHT SIGN (24"X30")
  - HANDICAP SYMBOL PER LATEST ADA STANDARDS
  - 4" SOLID YELLOW AT 24" C.C AT 45 DEGREES
  - 4" SOLID YELLOW STRIPE (19)
  - 24" WIDE WHITE STOP BAR
  - CROSSWALK - STAMPED CONCRETE BRICK PATTERN - SEE ARCHITECTURAL PLANS FOR PATTERN AND COLOR
  - HANDICAP RAMP WITH DEPRESSED CURB AND DETECTABLE WARNING (IF REQUIRED) IN ACCORDANCE WITH CURRENT ADA REQUIREMENTS

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS  
**CIVWORKS consulting, LLC**  
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SHEET NO. **C3.0**  
 SCALE: 1"=30'  
 FIRST ISSUE DATE: 01-04-2021  
 PROJECT: 2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515  
 SHEET: SITE PLAN  
 DRAWN BY: [Signature]  
 CHECKED BY: [Signature]  
 PROJECT NUMBER: 19006



LAND USE (REQUIRED)	WEEKDAY		WEEKEND	
	MIDNIGHT-7:00 AM	7:00 AM-6:00 PM	MIDNIGHT-7:00 AM	7:00 AM-6:00 PM
LODGING (153)	100% = 153	80% = 92	100% = 153	80% = 92
EATING AND DRINKING (24)	50% = 12	70% = 17	100% = 24	45% = 11
TOTAL REQUIRED	165	109	164	116
TOTAL PARKING PROVIDED = 177 SPACES, THEREFORE THE OVERALL SITE MEETS THE SHARED PARKING REQUIREMENTS				

ZONING ANALYSIS	
PROJECT NAME	PROPOSED WOODSPRING SUITES HOTEL
ADDRESS	2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515
PIN(s)	08-01-304-031
ZONING DISTRICT	EXISTING: O-R-M; OFFICE - RESEARCH - MANUFACTURING PROPOSED: B-3; GENERAL SERVICES AND HIGHWAY BUSINESS
MINIMUM DISTRICT AREA	MIN. 4 ACRES; PARCEL AREA = 4.31 ACRES
EXISTING USE	VACANT LAND
PROPOSED USE	WOODSPRING SUITES HOTEL AND FUTURE DRIVE-THROUGH RESTAURANT
PETITION TYPE	P.U.D.
DEVIATIONS	LOT 1: 1. REDUCED NUMBER OF ON-SITE PARKING SPACES LOT 2: 1. NONE OVERALL DEVELOPMENT: 1. NONE

LOT 1 - PROPOSED WOODSPRING SUITES HOTEL					
REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	87,465 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	47 FEET	YES	-13 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.56	YES	-0.19
BEG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	130.84 FEET	YES	+55.84 FEET
BEG. SETBACK (EAST)	MINIMUM	0.0 FEET	59.14 FEET	YES	+59.14 FEET
BEG. SIDE SETBACK (WEST-INTERIOR)	MINIMUM	0.0 FEET	22.00 FEET	YES	+22.00 FEET
BEG. SETBACK (SOUTH)	MINIMUM	10.0 FEET	77.99 FEET	YES	+67.99 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	144.50 FEET TO B/C	YES	+94.50 FEET
PARKING SETBACK (EAST)	MINIMUM	5.0 FEET	10.00 FEET	YES	+5.00 FEET
PARKING SETBACK (WEST-INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (SOUTH)	MINIMUM	3.0 FEET	5.0 FEET	YES	+2.0 FEET
TOTAL PARKING REQUIRED	MINIMUM	125 (ROOM (153))	125	NO	0 SPACES
TWO-WAY DRIVE ASLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
OPEN SPACE (10%)	MINIMUM	8,747 S.F.	10,165 S.F.	YES	+1,418 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	4,373 S.F.	4,973 S.F.	YES	+600 S.F.

LOT 2 - FUTURE DRIVE-THROUGH RESTAURANT (USER TO BE DETERMINED)					
REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	38,304 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	25 FEET (PARAPET)	YES	-35 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.06	YES	-0.69
BEG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	126.92 FEET	YES	+51.92 FEET
BEG. SETBACK (SOUTH)	MINIMUM	10.0 FEET	80.23 FEET	YES	+70.23 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	74.88 FEET TO B/C	YES	+24.88 FEET
PARKING SETBACK (EAST - INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (WEST - WALNUT)	MINIMUM	8.0 FEET	8.5 FEET	YES	+0.5 FEET
TOTAL PARKING REQUIRED	MINIMUM	10/1K SF (24)	57	YES	+33 SPACES
TWO-WAY DRIVE ASLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
DRIVE THRU SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	122.24 FEET	YES	+72.24 FEET
DRIVE THRU SETBACK (EAST - INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
DRIVE THRU SETBACK (WEST - WALNUT)	MINIMUM	8.0 FEET	50.50 FEET	YES	+42.50 FEET
DRIVE THRU SETBACK (SOUTH)	MINIMUM	5.0 FEET	54.00 FEET	YES	+49.00 FEET
STACKING SPACES	MINIMUM	12 CARS	12 CARS	YES	0 SPACES
BICYCLE PARKING	MINIMUM	10 FEET	12 FEET	YES	+2 FEET
DRIVE THROUGH LANE WIDTH	MINIMUM	2 SPACES	4 SPACES	YES	+2 SPACES
OPEN SPACE	MINIMUM	3,850 S.F.	7,783 S.F.	YES	+3,933 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	1,915 S.F.	3,878 S.F.	YES	+1,963 S.F.

SITE DEVELOPMENT SUMMARY	
TOTAL SITE AREA = 3.341+ ACRES	
LOT 1	
AREA = 2.008 ACRES	
PROPOSED HOTEL = 48,640 GSF - 122 ROOMS	
PARKING REQUIRED = 125 SPACES / ROOM = 153 SPACES	
REGULAR PARKING PROVIDED = 120 SPACES	
HANDICAP PARKING PROVIDED = 5 SPACES	
TOTAL PARKING PROVIDED = 125 SPACES	
LOT 2	
AREA = 0.878 ACRES	
PROPOSED RESTAURANT = 2,350 GSF	
PARKING REQUIRED = 10 SPACES / 1,000 SF = 24 SPACES	
REGULAR PARKING PROVIDED = 54 SPACES	
HANDICAP PARKING PROVIDED = 3 SPACES	
TOTAL PARKING PROVIDED = 57 SPACES	
LOT 3	
AREA = 0.455 ACRES	
PROPOSED WET BOTTOM DETENTION BASIN	

SITE PAVING LEGEND	
	HEAVY DUTY ASPHALT PAVEMENT 2" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, NSD 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, U-19, NSD 8" AGGREGATE BASE COURSE, TYPE B (DOT GRAD CA-6)
	LIGHT DUTY ASPHALT PAVEMENT 2" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, NSD 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, U-19, NSD 1" AGGREGATE BASE COURSE, TYPE B (DOT GRAD CA-6)
	CONCRETE PAVEMENT 8" F.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	STAMPED CONCRETE PAVEMENT 8" F.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	SIDEWALK PAVEMENT 5" F.C. CONCRETE PAVEMENT (THICKEN TO 8" AT DRIVEWAYS) 4" AGGREGATE BASE (CA-6)

- GENERAL NOTES**
- ALL DIMENSIONS AND CURB RADI ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - ALL CURB SHALL BE 84.12 CURB AND GUTTER UNLESS OTHERWISE NOTED.
  - SIDEWALK IN FRONT OF THE BUILDING SHALL BE COMBINATION SIDEWALK AND CURB AND GUTTER AND SHALL BE MONOTONICALLY CAST ACCORDING TO THE DETAIL.
  - ALL RADII ARE 3.0' UNLESS OTHERWISE NOTED.
  - ALL SITE SIGNAGE SHALL BE IN CONFORMANCE WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) STANDARDS, LATEST EDITION.
  - PARKING SPACES SHALL BE ANGLED AT 90° TO THE ASSOCIATED DRIVE ASIE UNLESS OTHERWISE NOTED.
  - DETECTABLE WARNING STRIPS WITH TRUNCATED DOMES ON ALL CURB RAMPS SHALL CONSIST OF CONTRASTING COLOR TO ADJACENT PAVEMENT COLOR PER DOT REGULATIONS, PREFERRED COLOR IS RED (FEDERAL COLOR STANDARD 30166)
  - REFER TO ARCHITECTURAL AND SIGNAGE PLANS FOR ADDITIONAL SITE SIGNAGE AND PAVEMENT MARKINGS.
  - ALL TRAFFIC SIGNS SHALL BE INSTALLED AT 7' HIGH MEASURED FROM THE GROUND ELEVATION TO THE BOTTOM OF SIGN.

- PROPOSED SIGNAGE NOTE**  
ALL SIGNAGE TO BE PERMITTED SEPARATELY
- PAVEMENT MARKING AND SIGN LEGEND**
- 17'-8" HANDICAP PARKING SIGN (12'X18") WITH \$250 FINE
  - R1-1 STOP SIGN (30"X30")
  - R5-1 DO NOT ENTER SIGN (30"X30")
  - R3-5 RIGHT TURN ONLY SIGN (30"X36")
  - R3-2 NO LEFT TURN SIGN (30"X36")
  - R4-7A KEEP RIGHT SIGN (24"X30")
  - HANDICAP SYMBOL PER LATEST ADA STANDARDS
  - 4" SOLID YELLOW AT 24" C.C AT 45 DEGREES
  - 4" SOLID YELLOW STRIPE (1Y)
  - 24" WIDE WHITE STOP BAR
  - CROSSWALK - STAMPED CONCRETE BRICK PATTERN - SEE ARCHITECTURAL PLANS FOR PATTERN AND COLOR
  - HANDICAP RAMP WITH DEPRESSED CURB AND DETECTABLE WARNING(S) (IF REQUIRED) IN ACCORDANCE WITH CURRENT ADA REQUIREMENTS

**REVISIONS**

NO.	DATE	DESCRIPTION
1	08/03/21	RECEIVED PER VALUAGE REVIEW
2	08/03/21	RECEIVED PER VALUAGE REVIEW
3	07/02/21	RECEIVED PER VALUAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

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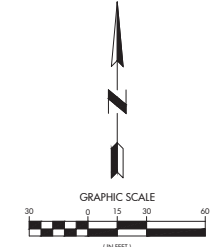
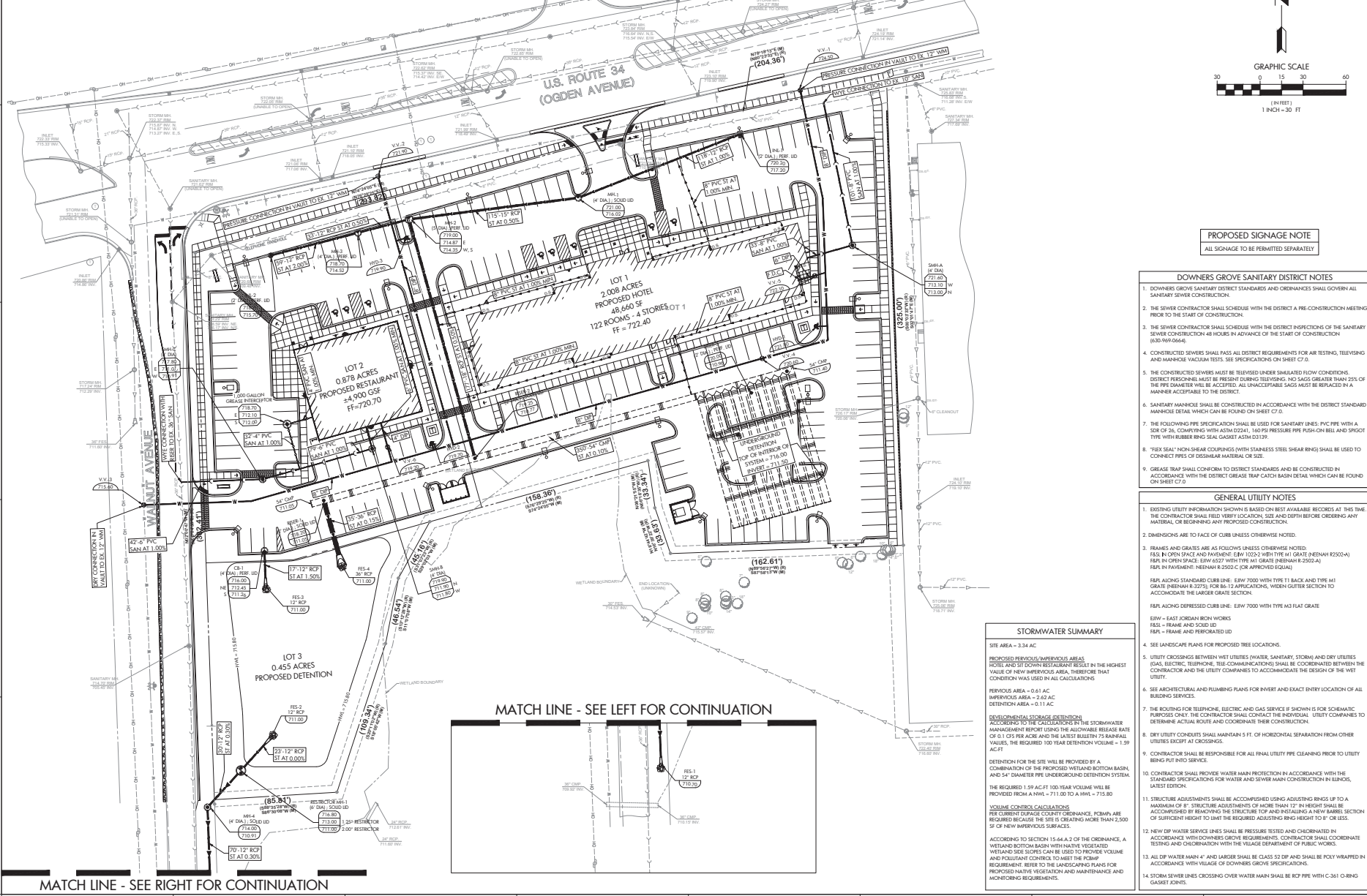
**SITE DIMENSIONAL AND PAVING PLAN  
HOTEL AND DRIVE-THROUGH RESTAURANT**

**PROPOSED WOODSPRING SUITES HOTEL**  
2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515

SHEET NO. **C3.1**

PROJ. NUMBER: 19006

NOTE:  
THE LOCATION, ELEVATION, SIZE, AND TYPES OF ALL EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION, ELEVATION, SIZE AND TYPES OF ALL EXISTING UTILITIES PRIOR TO COMMENCING WORK AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.



**PROPOSED SIGNAGE NOTE**  
ALL SIGNAGE TO BE PERMITTED SEPARATELY

**DOWNS GROVE SANITARY DISTRICT NOTES**

1. DOWNS GROVE SANITARY DISTRICT STANDARDS AND ORDINANCES SHALL GOVERN ALL SANITARY SEWER CONSTRUCTION.
2. THE SEWER CONTRACTOR SHALL SCHEDULE WITH THE DISTRICT A PRE-CONSTRUCTION MEETING PRIOR TO THE START OF CONSTRUCTION.
3. THE SEWER CONTRACTOR SHALL SCHEDULE WITH THE DISTRICT INSPECTIONS OF THE SANITARY SEWER CONSTRUCTION 48 HOURS IN ADVANCE OF THE START OF CONSTRUCTION (AS APPLICABLE).
4. CONSTRUCTED SEWERS SHALL PASS ALL DISTRICT REQUIREMENTS FOR AIR TESTING, TELEVISIONING AND MANHOOD VACUUM TESTS. SEE SPECIFICATIONS ON SHEET C7.0.
5. THE CONSTRUCTED SEWERS MUST BE TELEVISIONED UNDER SIMULATED FLOW CONDITIONS. DISTRICT PERSONNEL MUST BE PRESENT DURING TELEVISIONING. NO SAGS GREATER THAN 25% OF THE PIPE DIAMETER WILL BE ACCEPTED. ALL UNACCEPTABLE SAGS MUST BE REPAIRED IN A MANNER ACCEPTABLE TO THE DISTRICT.
6. SANITARY MANHOOD SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DISTRICT STANDARD MANHOOD DETAIL WHICH CAN BE FOUND ON SHEET C7.0.
7. THE FOLLOWING PIPE SPECIFICATION SHALL BE USED FOR SANITARY LINES: PVC PIPE WITH A SIZE OF 24", COMPLYING WITH ASTM D2241, 160 PSI PRESSURE PIPE FISH-GON BELL AND SPOUT TIE WITH RUBBER RING SEAL GASKET ASTM D3319.
8. "FLEX SEAL" NON-SHEAR COUPLINGS WITH STAINLESS STEEL SHEAR RING(S) SHALL BE USED TO CONNECT PIPES OF DISSIMILAR MATERIAL OR SIZE.
9. GRADE TRAP SHALL CONFORM TO DISTRICT STANDARDS AND BE CONSTRUCTED IN ACCORDANCE WITH THE DISTRICT GRADE TRAP CATCH BASIN DETAIL WHICH CAN BE FOUND ON SHEET C7.0.

**GENERAL UTILITY NOTES**

1. EXISTING UTILITY INFORMATION SHOWN IS BASED ON BEST AVAILABLE RECORDS AT THIS TIME. THE CONTRACTOR SHALL FIELD VERIFY LOCATION, SIZE AND DEPTH BEFORE CHECKING ANY MATERIAL, OR BEGINNING ANY PROPOSED CONSTRUCTION.
2. DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
3. FRAMES AND GRATES ARE AS FOLLOWS UNLESS OTHERWISE NOTED:  
FAS - IN OPEN SPACE AND PAVEMENT: ELEV 1025.2 WITH TYP M1 GRATE (BENHAW R2502-A)  
FAR - IN OPEN SPACE: ELEV 527 WITH TYP M1 GRATE (BENHAW R-2502-A)  
FAR - IN PAVEMENT: BENHAW R-2502-C (OR APPROVED EQUAL)  
FAR ALONG STANDARD CURB LINE: ELEV 700 WITH TYP T1 BACK AND TYP M1 GRATE (BENHAW R-2502); FOR 80-12 APPLICATIONS, WIDER CURB SECTION TO ACCOMMODATE THE LARGER GRADE SECTION.  
FAR ALONG DEPRESSED CURB LINE: ELEV 700 WITH TYP M3 FLAT GRATE  
EWP - EAST JORDAN IRON WORKS  
FAS - FRAME AND SOILD ID  
FAR - FRAME AND PERFORATED LID
4. SEE LANDSCAPE PLANS FOR PROPOSED TREE LOCATIONS.
5. UTILITY CROSSINGS BETWEEN WET UTILITIES (WATER, SANITARY, STORM) AND DRY UTILITIES (GAS, ELECTRIC, TELEPHONE, TELE-COMMUNICATIONS) SHALL BE COORDINATED BETWEEN THE CONTRACTOR AND THE UTILITY COMPANIES TO ACCOMMODATE THE DESIGN OF THE WET UTILITY.
6. SEE ARCHITECTURAL AND PLUMBING PLANS FOR INVERT AND EXACT ENTRY LOCATION OF ALL BUILDING SERVICES.
7. THE ROUTING FOR TELEPHONE, ELECTRIC, AND GAS SERVICE (IF SHOWN) IS FOR SCHEMATIC PURPOSES ONLY. THE CONTRACTOR SHALL CONTACT THE INDIVIDUAL UTILITY COMPANIES TO DETERMINE ACTUAL ROUTE AND COORDINATE THEIR CONSTRUCTION.
8. DRY UTILITY CONDUITS SHALL MAINTAIN 5 FT. OF HORIZONTAL SEPARATION FROM OTHER UTILITIES EXCEPT AT CROSSINGS.
9. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL FINAL UTILITY PIPE CLEANING PRIOR TO UTILITY BRING OUT SERVICE.
10. CONTRACTOR SHALL PROVIDE WATER MAIN PROTECTION IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION.
11. STRUCTURE ADJUSTMENTS SHALL BE ACCOMPLISHED USING ADJUSTING RINGS UP TO A MAXIMUM OF 8" STRUCTURE ADJUSTMENTS OF MORE THAN 12" IN HEIGHT SHALL BE ACCOMPLISHED BY REMOVING THE STRUCTURE TOP AND INSTALLING A NEW BARREL SECTION OF SUFFICIENT HEIGHT TO LIMIT THE REQUIRED ADJUSTING RING HEIGHT TO 8" OR LESS.
12. NEW DIP WATER SERVICE LINES SHALL BE PRESSURE TESTED AND CHROBORATED IN ACCORDANCE WITH DOWNS GROVE REQUIREMENTS. CONTRACTOR SHALL CONDUIT TEST AND CHROBORATE WITHIN THE VILLAGE DEPARTMENT OF PUBLIC WORKS.
13. ALL DIP WATER MAIN 4" AND LARGER SHALL BE CLASS S2 DIP AND SHALL BE POLY WRAPPED IN ACCORDANCE WITH VILLAGE OF DOWNS GROVE SPECIFICATIONS.
14. STORM SEWER LINES CROSSING OVER WATER MAIN SHALL BE RCP PIPE WITH C-361 O-RING GASKET JOINTS.

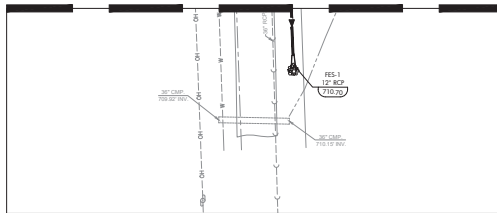
**STORMWATER SUMMARY**

SITE AREA = 3.34 AC  
PROPOSED IMPERVIOUS AREAS: HOTEL AND SIT DOWN RESTAURANT RESULT IN THE HIGHEST VALUE OF NEW IMPERVIOUS AREA, THEREFORE THAT CONDITION WAS USED IN ALL CALCULATIONS.  
PERVIOUS AREA = 0.61 AC  
IMPERVIOUS AREA = 2.62 AC  
DISTRIBUTION AREA = 0.11 AC

**ENVIRONMENTAL COVERAGE DETENTION**  
ACCORDING TO THE CALCULATIONS IN THE STORMWATER MANAGEMENT REPORT USING THE ALLOWABLE RELEASE RATE OF 0.1 CFS PER ACRE AND THE LATEST ILLINOIS 75 RAINFALL VALUES, THE REQUIRED 100 YEAR DETENTION VOLUME = 1.59 AC-FIT  
DETENTION FOR THE SITE WILL BE PROVIDED BY A COMBINATION OF THE PROPOSED WETLAND BOTTOM BASIN, 24" DIAMETER PIPE UNDERGROUND DETENTION SYSTEM. THE REQUIRED 1.59 AC-FIT 100 YEAR VOLUME WILL BE PROVIDED FROM A 100' x 100' x 10' INVA = 715.80

**VOLUME CONTROL CALCULATIONS**  
PER CURRENT DRAINAGE COUNTY ORDINANCE, PCMP, ARE REQUIRED BECAUSE THE SITE IS CREATING MORE THAN 2,500 SF OF NEW IMPERVIOUS SURFACES.  
ACCORDING TO SECTION 15-6A-2 OF THE ORDINANCE, A WETLAND BOTTOM BASIN WITH NATIVE VEGETATED WETLAND SIDE SLOPES CAN BE USED TO PROVIDE VOLUME AND POLLUTANT CONTROL TO MEET THE CSWMP REQUIREMENT. REFER TO THE LANDSCAPING PLANS FOR PROPOSED NATIVE VEGETATION AND MAINTENANCE AND MONITORING REQUIREMENTS.

MATCH LINE - SEE LEFT FOR CONTINUATION



MATCH LINE - SEE RIGHT FOR CONTINUATION

REVISIONS

NO. 1	DATE	DESCRIPTION
1	08/05/21	REVISED PER VILLAGE REVIEW
2	07/02/21	REVISED PER VILLAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

**CIVWORKS**  
consulting, LLC

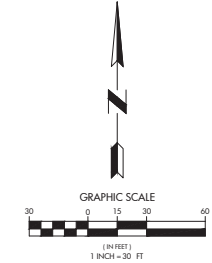
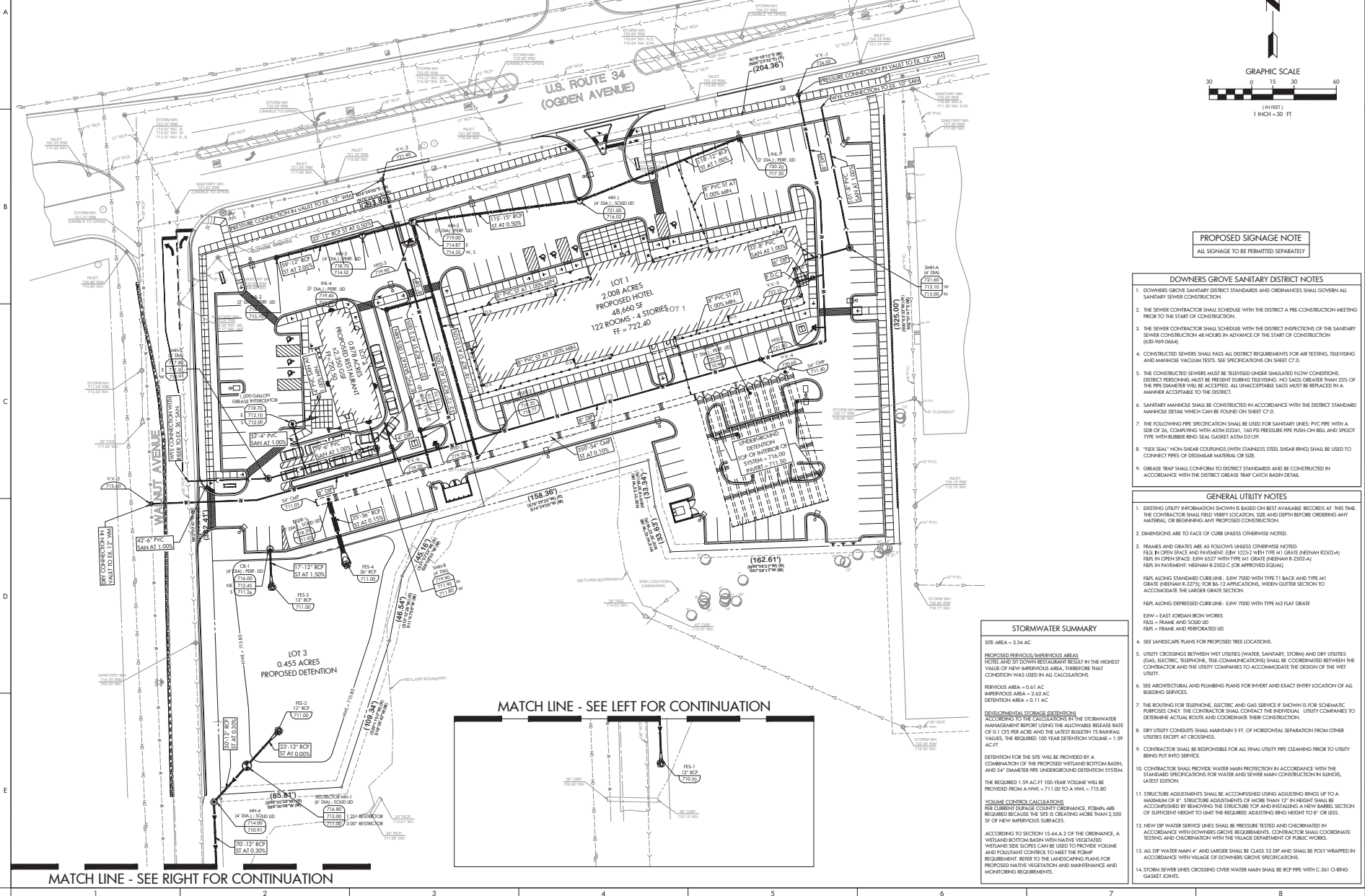
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**SITE UTILITY PLAN**  
**HOTEL AND SIT DOWN RESTAURANT**  
**PROPOSED WOODSPRING SUITES HOTEL**  
**2751 & 2761 OGDEN AVENUE, DOWNS GROVE, IL 60515**

SHEET NO. **C4.0**

PROJ. NUMBER: 19006

NOTE:  
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PROPOSED SIGNAGE NOTE  
ALL SIGNAGE TO BE PERMITTED SEPARATELY

**DOWNS GROVE SANITARY DISTRICT NOTES**

1. DOWNS GROVE SANITARY DISTRICT STANDARDS AND ORDINANCES SHALL GOVERN ALL SANITARY SEWER CONSTRUCTION.
2. THE SEWER CONTRACTOR SHALL SCHEDULE WITH THE DISTRICT A PRE-CONSTRUCTION MEETING PRIOR TO THE START OF CONSTRUCTION.
3. THE SEWER CONTRACTOR SHALL SCHEDULE WITH THE DISTRICT INSPECTIONS OF THE SANITARY SEWER CONSTRUCTION 48 HOURS IN ADVANCE OF THE START OF CONSTRUCTION (AS APPLICABLE).
4. CONSTRUCTED SEWERS SHALL PASS ALL DISTRICT REQUIREMENTS FOR AIR TESTING, TELEVISIONING AND MANHOOD VACUUM TESTS. SEE SPECIFICATIONS ON SHEET C7.0.
5. THE CONSTRUCTED SEWERS MUST BE TELEVISIONED UNDER SIMULATED FLOW CONDITIONS. DISTRICT PERSONNEL MUST BE PRESENT DURING TELEVISIONING. NO SAGS GREATER THAN 25% OF THE PIPE DIAMETER WILL BE ACCEPTED. ALL UNACCEPTABLE SAGS MUST BE REPAIRED IN A MANNER ACCEPTABLE TO THE DISTRICT.
6. SANITARY MANHOLES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DISTRICT STANDARD MANHOLE DETAIL WHICH CAN BE FOUND ON SHEET C7.0.
7. THE FOLLOWING PIPE SPECIFICATION SHALL BE USED FOR SANITARY LINES: PVC PIPE WITH A SDR OF 26, COMPLYING WITH ASTM D2411, 160 PSI PRESSURE PIPE FISH-GUARD BELT AND SPOUT TIRE WITH RUBBER RING SEAL GASKET ASTM D3319.
8. "FLEX SEAL" NON-SHEAR COUPLINGS WITH STAINLESS STEEL SHEAR RINGS SHALL BE USED TO CONNECT PIPES OF DISSIMILAR MATERIAL OR SIZE.
9. GREASE TRAP SHALL CONFORM TO DISTRICT STANDARD AND BE CONSTRUCTED IN ACCORDANCE WITH THE DISTRICT GREASE TRAP CATCH BASIN DETAIL.

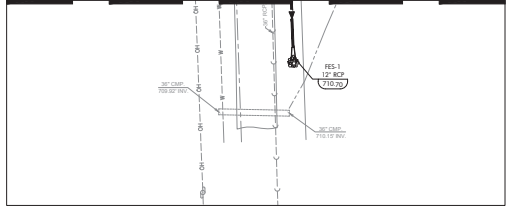
**GENERAL UTILITY NOTES**

1. EXISTING UTILITY INFORMATION SHOWN IS BASED ON BEST AVAILABLE RECORDS AT THIS TIME. THE CONTRACTOR SHALL VERIFY LOCATION, SIZE AND DEPTH BEFORE CHECKING ANY MATERIAL, OR BEGINNING ANY PROPOSED CONSTRUCTION.
2. DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
3. FRAMES AND GRATES ARE AS FOLLOWS UNLESS OTHERWISE NOTED:  
F&S IN OPEN SPACE AND PAVEMENT: ESW 1025-2 WITH TYP M1 GRATE (REINAH R2502-A)  
F&S IN OPEN SPACE: ESW 5257 WITH TYP M1 GRATE (REINAH R-2502-A)  
F&S IN PAVEMENT: REINAH R-2502-C (OR APPROVED EQUAL)  
F&S ALONG STANDARD CURB LINE: ESW 7000 WITH TYP T1 BACK AND TYP M1 GRATE (REINAH R-2525); FOR 80-12 APPLICATIONS, WIDER GRATE SECTION TO ACCOMMODATE THE LARGER GRATE SECTION.  
F&S ALONG DEPRESSED CURB LINE: ESW 7000 WITH TYP M3 FLAT GRATE  
E&W - EAST JORDAN IRON WORKS  
F&S - FRAME AND SOILD ID  
F&S - FRAME AND PERFORATED LID
4. SEE LANDSCAPE PLANS FOR PROPOSED TREE LOCATIONS.
5. UTILITY CROSSINGS BETWEEN WET UTILITIES (WATER, SANITARY, STORM) AND DRY UTILITIES (GAS, ELECTRIC, TELEPHONE, TELE-COMMUNICATIONS) SHALL BE COORDINATED BETWEEN THE CONTRACTOR AND THE UTILITY COMPANIES TO ACCOMMODATE THE DESIGN OF THE WET UTILITY.
6. SEE ARCHITECTURAL AND PLUMBING PLANS FOR INVERT AND EXACT ENTRY LOCATION OF ALL BUILDING SERVICES.
7. THE ROUTING FOR TELEPHONE, ELECTRIC, AND GAS SERVICE (IF SHOWN) IS FOR SCHEMATIC PURPOSES ONLY. THE CONTRACTOR SHALL CONTACT THE INDIVIDUAL UTILITY COMPANIES TO DETERMINE ACTUAL ROUTE AND COORDINATE THEIR CONSTRUCTION.
8. DRY UTILITY CONDUITS SHALL MAINTAIN 5 FT. OF HORIZONTAL SEPARATION FROM OTHER UTILITIES EXCEPT AT CROSSINGS.
9. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL FINAL UTILITY PIPE CLEANING PRIOR TO UTILITY BRING OUT SERVICE.
10. CONTRACTOR SHALL PROVIDE WATER MAIN PROTECTION IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION.
11. STRUCTURE ADJUSTMENTS SHALL BE ACCOMPLISHED USING ADJUSTING RINGS UP TO A MAXIMUM OF 8" STRUCTURE ADJUSTMENTS OF MORE THAN 12" IN HEIGHT SHALL BE ACCOMPLISHED BY REMOVING THE STRUCTURE TOP AND INSTALLING A NEW BARREL SECTION OF SUFFICIENT HEIGHT TO LIMIT THE REQUIRED ADJUSTING RING HEIGHT TO 8" OR LESS.
12. NEW DIP WATER SERVICE LINES SHALL BE PRESSURE TESTED AND CHROBORINATED IN ACCORDANCE WITH DOWNS GROVE REQUIREMENTS. CONTRACTOR SHALL COORDINATE TESTING AND CHROBORINATION WITH THE VILLAGE DEPARTMENT OF PUBLIC WORKS.
13. ALL DIP WATER MAIN 4" AND LARGER SHALL BE CLASS 52 DIP AND SHALL BE POLY WRAPPED IN ACCORDANCE WITH VILLAGE OF DOWNS GROVE SPECIFICATIONS.
14. STORM SEWER LINES CROSSING OVER WATER MAIN SHALL BE RCP PIPE WITH C-361 O-RING GASKET JOINTS.

**STORMWATER SUMMARY**

SITE AREA = 3.34 AC  
PROPOSED IMPERVIOUS AREAS: HOTEL AND SIT DOWN RESTAURANT RESULT IN THE HIGHEST VALUE OF NEW IMPERVIOUS AREA, THEREFORE THAT CONDITION WAS USED IN ALL CALCULATIONS.  
PERVIOUS AREA = 0.61 AC  
IMPERVIOUS AREA = 2.62 AC  
DISTRIBUTION AREA = 0.11 AC  
**PROVISIONAL COVERAGE DEFINITION**  
ACCORDING TO THE CALCULATIONS FOR THE STORMWATER MANAGEMENT REPORT USING THE ALLOWABLE RELEASE RATE OF 0.1 CFS PER ACRE AND THE LATEST BULLETIN 75 RAINFALL VALUES, THE REQUIRED 100 YEAR DETENTION VOLUME = 1.59 AC-FT.  
DETENTION FOR THE SITE WILL BE PROVIDED BY A COMBINATION OF THE PROPOSED WETLAND BOTTOM BASIN, 24" DIAMETER PIPE UNDERGROUND DETENTION SYSTEM. THE REQUIRED 1.59 AC-FT 100 YEAR VOLUME WILL BE PROVIDED FROM A MW = 711.00 TO A MW = 715.80  
**VOLUME CONTROL CALCULATIONS**  
PER CURRENT DRAINAGE COUNTY ORDINANCE, PCWMA, ARE REQUIRED BECAUSE THE SITE IS CREATING MORE THAN 2,500 SF OF NEW IMPERVIOUS SURFACES.  
ACCORDING TO SECTION 15-6-A-2 OF THE ORDINANCE, A WETLAND BOTTOM BASIN WITH NATIVE VEGETATED WETLAND SIDE SLOPES CAN BE USED TO PROVIDE VOLUME AND POLLUTANT CONTROL TO MEET THE "SWMP" REQUIREMENT. REFER TO THE LANDSCAPING PLANS FOR PROPOSED NATIVE VEGETATION AND MAINTENANCE AND MONITORING REQUIREMENTS.

**MATCH LINE - SEE LEFT FOR CONTINUATION**



**MATCH LINE - SEE RIGHT FOR CONTINUATION**

REVISIONS

NO. 1	DATE	DESCRIPTION
1	08/05/21	REVISED PER VILLAGE REVIEW
2	07/02/21	REVISED PER VILLAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

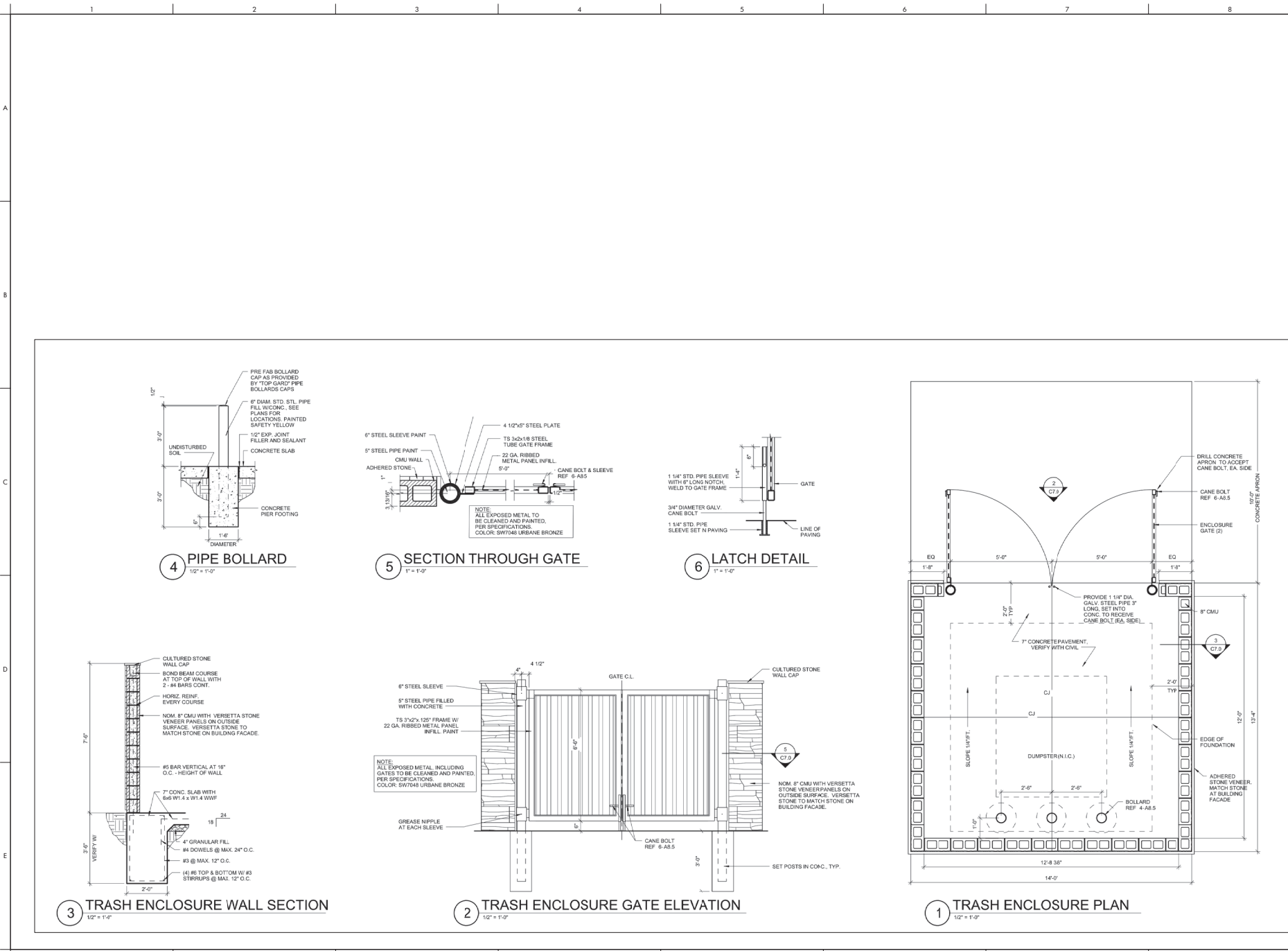
**CIVWORKS Consulting, LLC**

3030 N. WENNA AVENUE  
CHICAGO, ILLINOIS 60641  
Ph: (312) 607-9370  
E-mail: info@civworks.com  
www.civworks.com

**SITE UTILITY PLAN**  
**HOTEL AND DRIVE-THROUGH RESTAURANT**  
**PROPOSED WOODSPRING SUITES HOTEL**  
**2751 & 2761 OGDEN AVENUE, DOWNS GROVE, IL 60015**

SHEET NO. **C4.1**

PROJ. NUMBER: 19006



REVISIONS

1	06/30/21	REVISED PER VALUAGE REVIEW
2	07/02/21	REVISED PER VALUAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

**CivWorks Consulting, LLC**

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E-MAIL: info@civworks.com  
www.civworks.com

**HOTEL TRASH ENCLOSURE DETAILS**

PROPOSED WOODSPRING SUITES HOTEL  
2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515

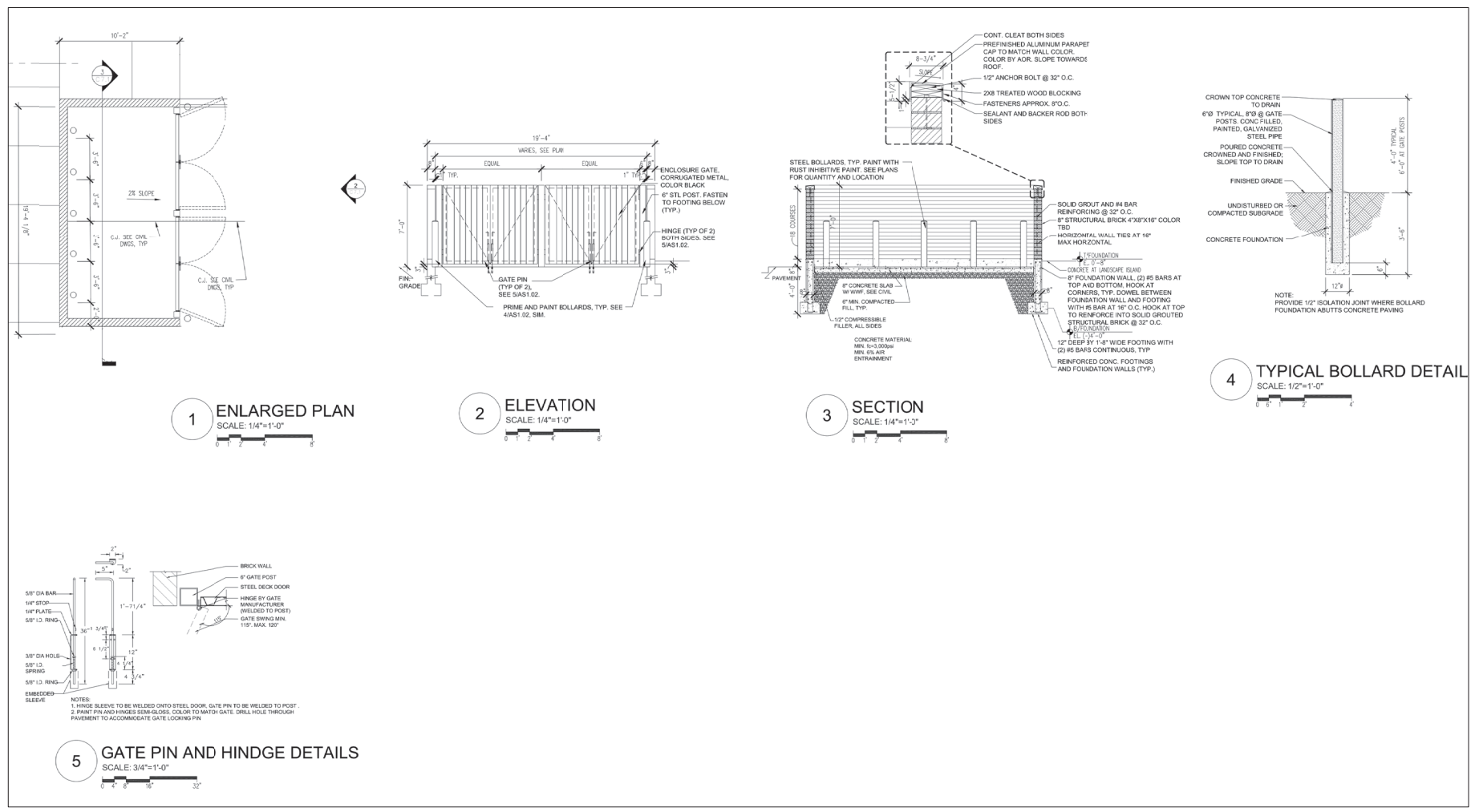
SHEET NO. **C7.0**

PROJ. NUMBER: 19006

DATE: 01-04-2021

SCALE: AS NOTED

AS NOTED



REVISIONS

1	08/30/21	REVISED PER VALUAGE REVIEW
2	07/27/21	REVISED PER VALUAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

**CIVWORKS**  
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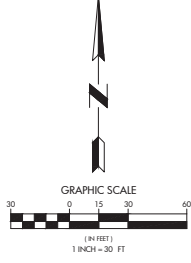
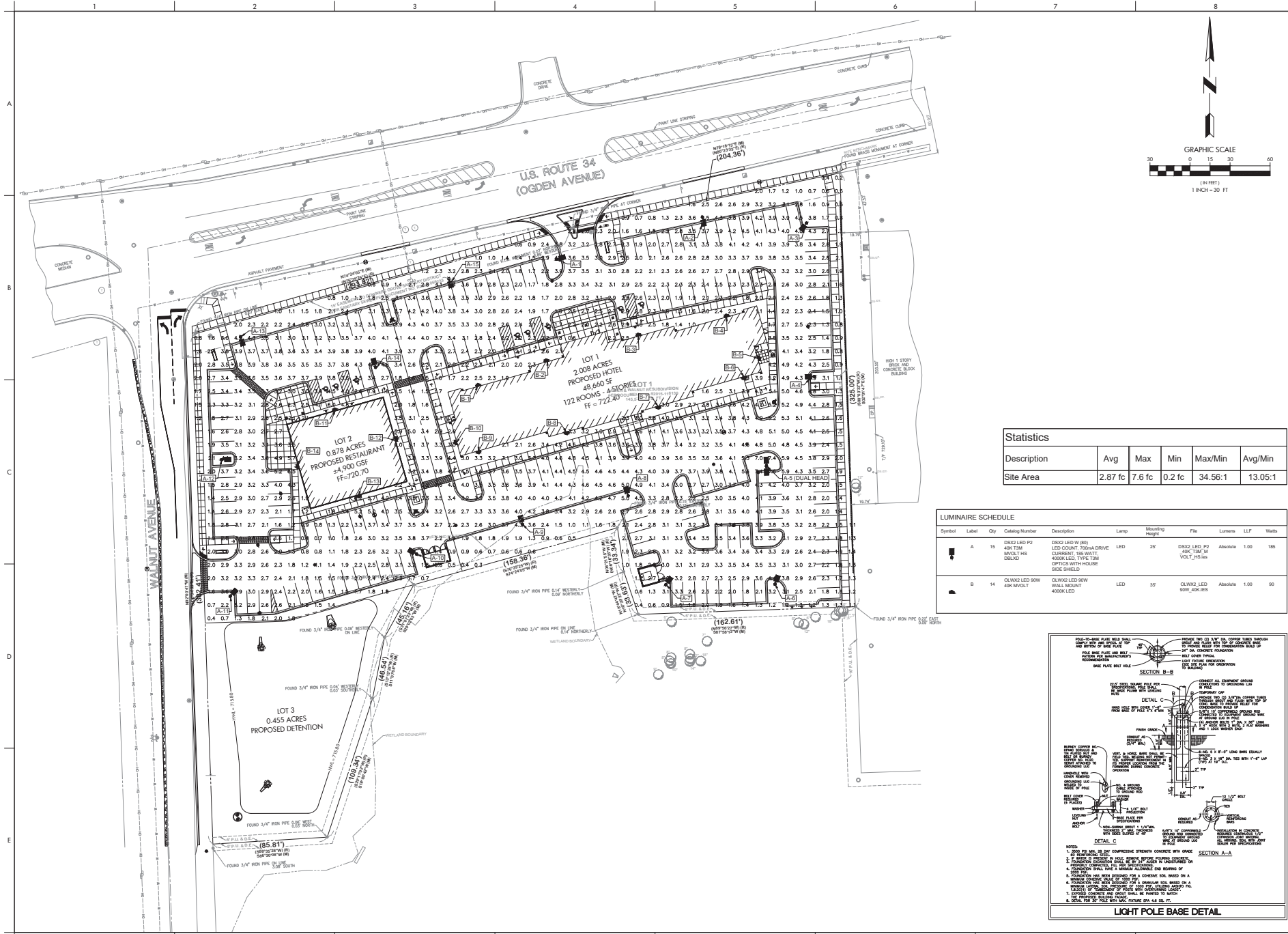
SHEET TITLE: RESTAURANT TRASH ENCLOSURE DETAILS

PROJECT: PROPOSED WOODSPRING SUITES HOTEL  
2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515

SHEET NO. C7.1

PROJ. NUMBER: 19006

DATE: 01-04-2021  
SCALE: AS NOTED

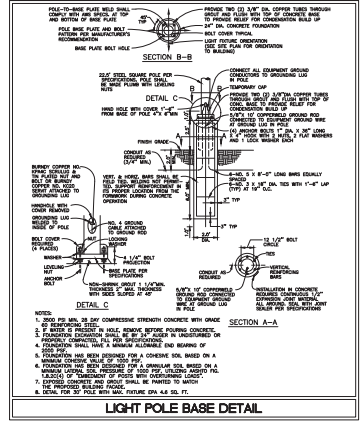


**Statistics**

Description	Avg	Max	Min	Max/Min	Avg/Min
Site Area	2.87 fc	7.6 fc	0.2 fc	34.56:1	13.05:1

**LUMINAIRE SCHEDULE**

Symbol	Label	Qty	Catalog Number	Description	Lamp	Mounting	Height	File	Lumens	LLF	Watts
A	15		DISK LED P2 40K TSM MACLTHS SHIELD	DISK LED W/ (R) LED CURRENT TROMA DRIVE CURRENT: 165 WATT 4000K LED TYPE TSM OFFICE WITH HOUSE SIDE SHIELD	LED	20'	DISK LED P2 40K TSM_M VOL1_House	Absolute	1,000	185	
B	14		OLWIKO LED ROW 40K MVOLT	OLWIKO LED ROW WALL MOUNT 4000K LED	LED	50'	OLWIKO LED ROW_40KJES	Absolute	1,000	90	



REVISIONS

1	08/03/21	REVISED PER VALANCE REVIEW
2	07/02/21	REVISED PER VALANCE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

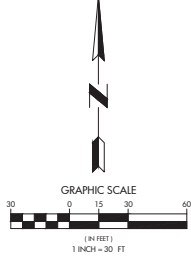
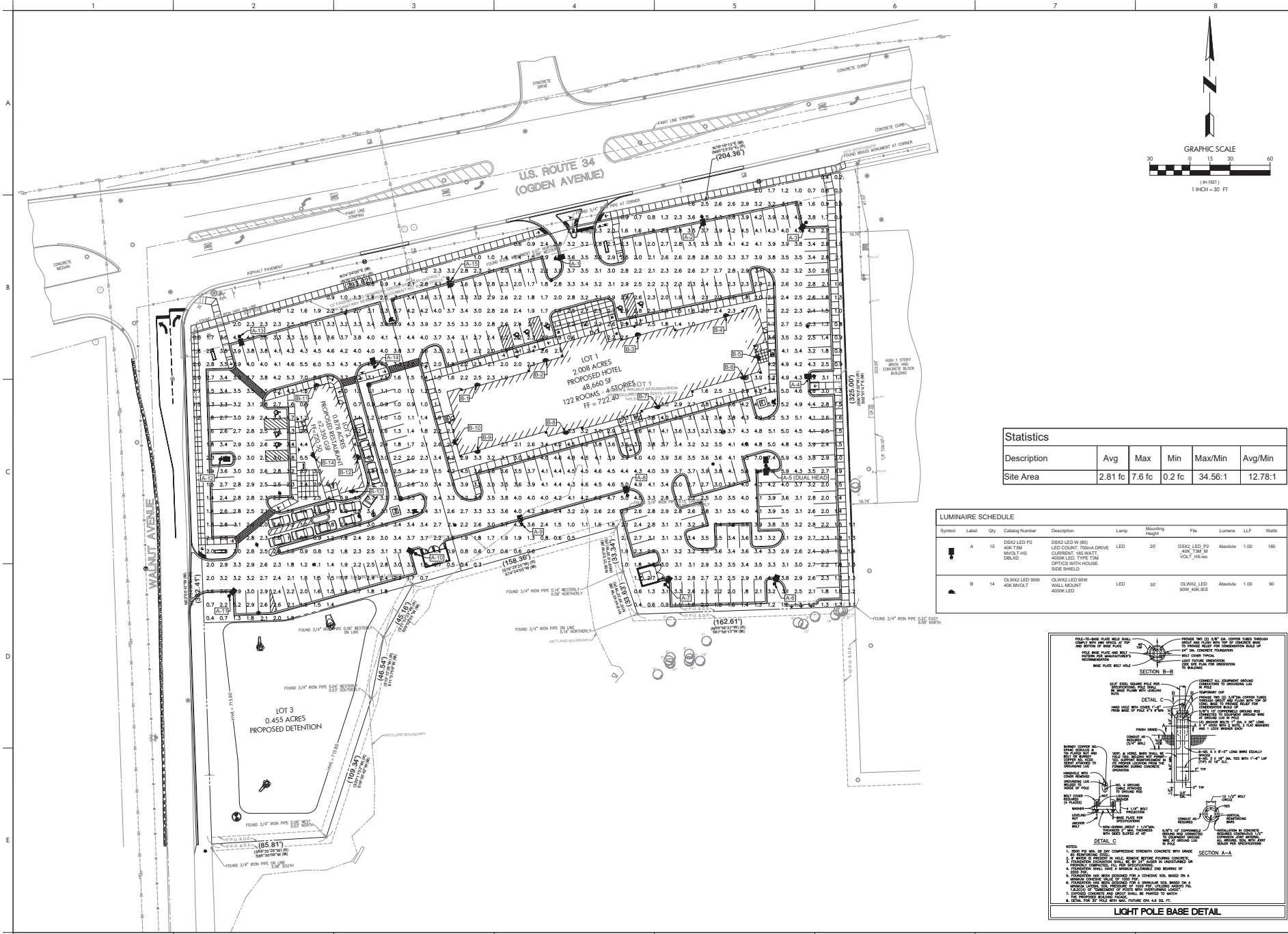
**CIVWORKS Consulting, LLC**

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**SITE PHOTOMETRICS PLAN  
HOTEL AND SIT DOWN RESTAURANT  
PROPOSED WOODSPRING SUITES HOTEL  
2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515**

SHEET NO. **PH1.0**

PROJECT: PH1.0  
SCALE: 1"=30'  
FIRST ISSUE DATE: 01.04.2021  
PROJECT NO.: 19006

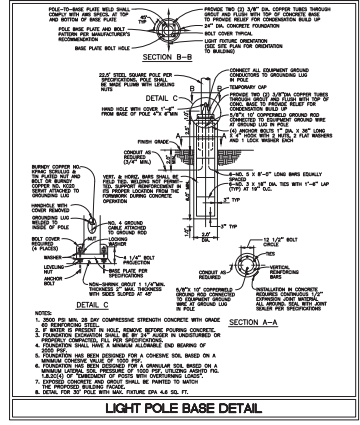


**Statistics**

Description	Avg	Max	Min	Max/Min	Avg/Min
Site Area	2.81 fc	7.6 fc	0.2 fc	34.56:1	12.78:1

**LUMINAIRE SCHEDULE**

Symbol	Label	Qty	Catalog Number	Description	Lamp	Mounting	Fixture	Lumens	LLF	Watts
A	15		DISK LED P2 40K TSM MACLTHS SHIELD	DISK LED W/ (R) LED CURRENT TROMA DRIVE CURRENT 165 WATT 4000K LED TYPE TSM OPTICS WITH HOUSE SIDE SHIELD	LED	20'	DISK LED P2 40K TSM M VOL1_HOUSE	Absolute	1.00	165
B	14		OLWIKO LED ROW 40K MVOLT	OLWIKO LED ROW WALL MOUNT 4000K LED	LED	30'	OLWIKO LED ROW JOHNS	Absolute	1.00	90



REVISIONS

1	08/03/21	REVISED PER VALANCE REVIEW
2	07/02/21	REVISED PER VALANCE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

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**SITE PHOTOMETRICS PLAN  
HOTEL AND DRIVE-THROUGH RESTAURANT**

PROPOSED WOODSPRING SUITES HOTEL  
2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515

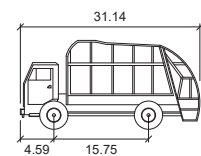
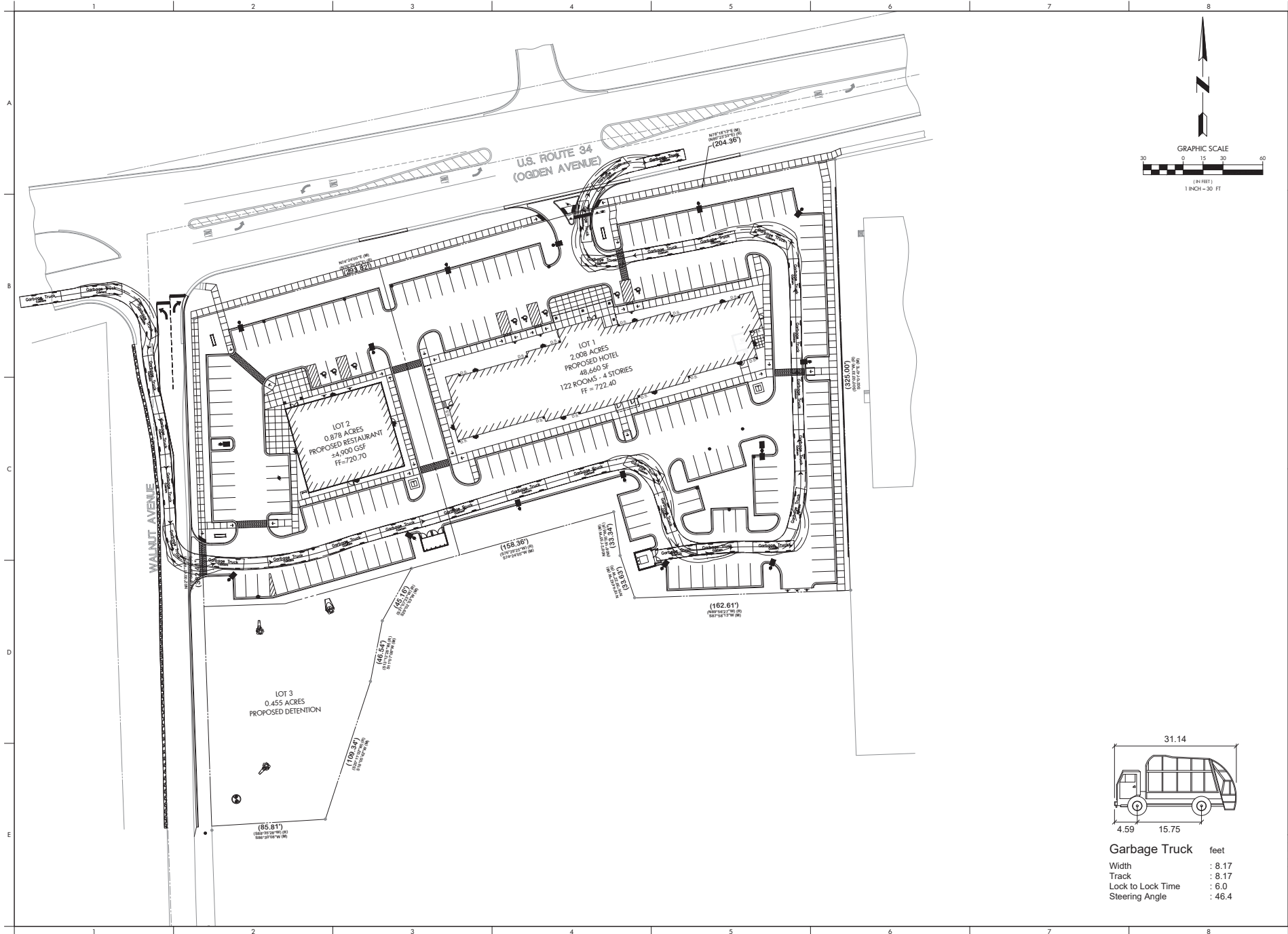
SHEET NO.  
**PH1.1**

PROJECT NO.: 19006

DATE: 01-04-2021  
SCALE: 1"=30'







Garbage Truck feet  
 Width : 8.17  
 Track : 8.17  
 Lock to Lock Time : 6.0  
 Steering Angle : 46.4

REVISIONS

1	08/10/21	REVISED PER VALUAGE REVIEW
2	07/02/21	REVISED PER VALUAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

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**GARBAGE TRUCK TURNING EXHIBIT**

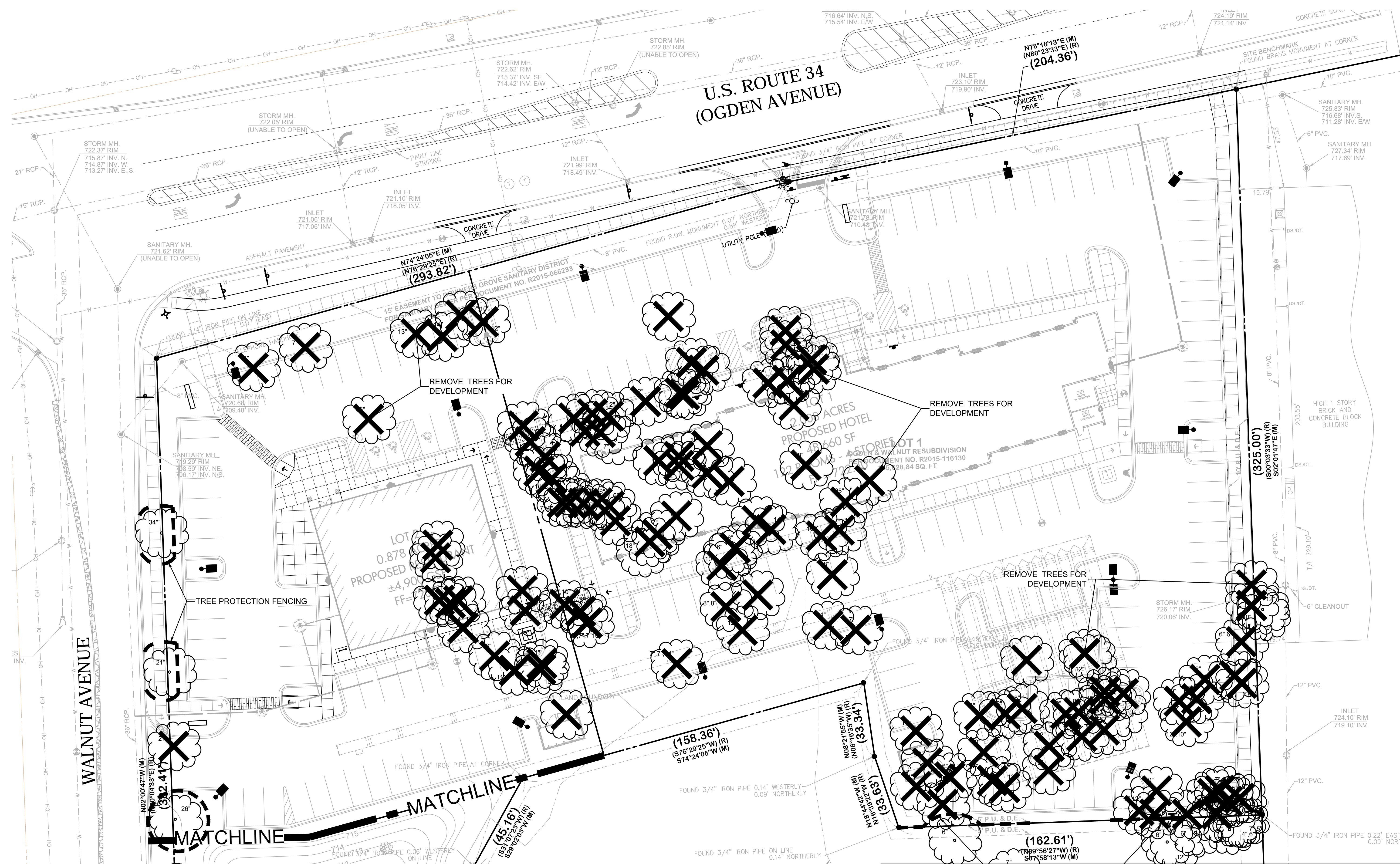
PROPOSED WOODSPRING SUITES HOTEL  
 2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515

SHEET TITLE: GARBAGE TRUCK TURNING EXHIBIT  
 PROJECT: PROPOSED WOODSPRING SUITES HOTEL  
 DRAWN BY: [Signature]  
 FIRST ISSUE DATE: 01-04-2021  
 SCALE: 1"=30'

**SHEET NO.**  
**TR2.0**

PROJ. NUMBER: 19006

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**TREE PROTECTION & REMOVAL LEGEND**

- EXISTING DECIDUOUS TREE TO REMAIN
- EXISTING DECIDUOUS TREE TO BE REMOVED
- BARRIER FENCING TO BE INSTALLED

**EXISTING VEGETATION DESCRIPTION**

THE PROJECT SITE CONSIST OF A VACANT LOT WITH 134 TREES IDENTIFIED, MOST OF WHICH (EXCEPT PARKWAY TREES) WILL BE REMOVED FOR DEVELOPMENT. SPECIES ON SITE INCLUDE POPLAR, SILVER MAPLE, WHITE PINE, GREEN ASH, BOXELDER AND MULBERRY.

**1 TREE PROTECTION & REMOVAL PLAN**

**SHEET INDEX**

- L-1 TREE PROTECTION & REMOVAL PLAN
- L-2 TREE PROTECTION & REMOVAL PLAN SOUTH / DETAILS
- L-3 LANDSCAPE PLAN - HOTEL & SIT DOWN RESTAURANT
- L-4 LANDSCAPE PLAN - HOTEL & DRIVE THROUGH RESTAURANT
- L-5 LANDSCAPE DETAILS



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 Landscape Architecture  
 Site Planning  
 Illustration  
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 Chicago, IL 60647  
 ph. 773.697.4388  
 www.LGWLA.com

**SEAL**

**PROJECT TEAM**

CIVIL ENGINEER:

**PROJECT NAME**

2657 OGDEN AVENUE,  
 DOWNERS GROVE, IL 60515

**DRAWING ISSUED**

NO.	TITLE	DATE
1.	ISSUED PRELIM.	01/04/21
2.	Rev. Per Village Review	07/02/21

**SET TYPE**  
 PRELIMINARY LANDSCAPE PLANS

**PROJECT NUMBER**  
 2012016

**DATE**  
 01-04-2020  
**DRAWN BY:** LCG  
**APPROVED BY:** LCG

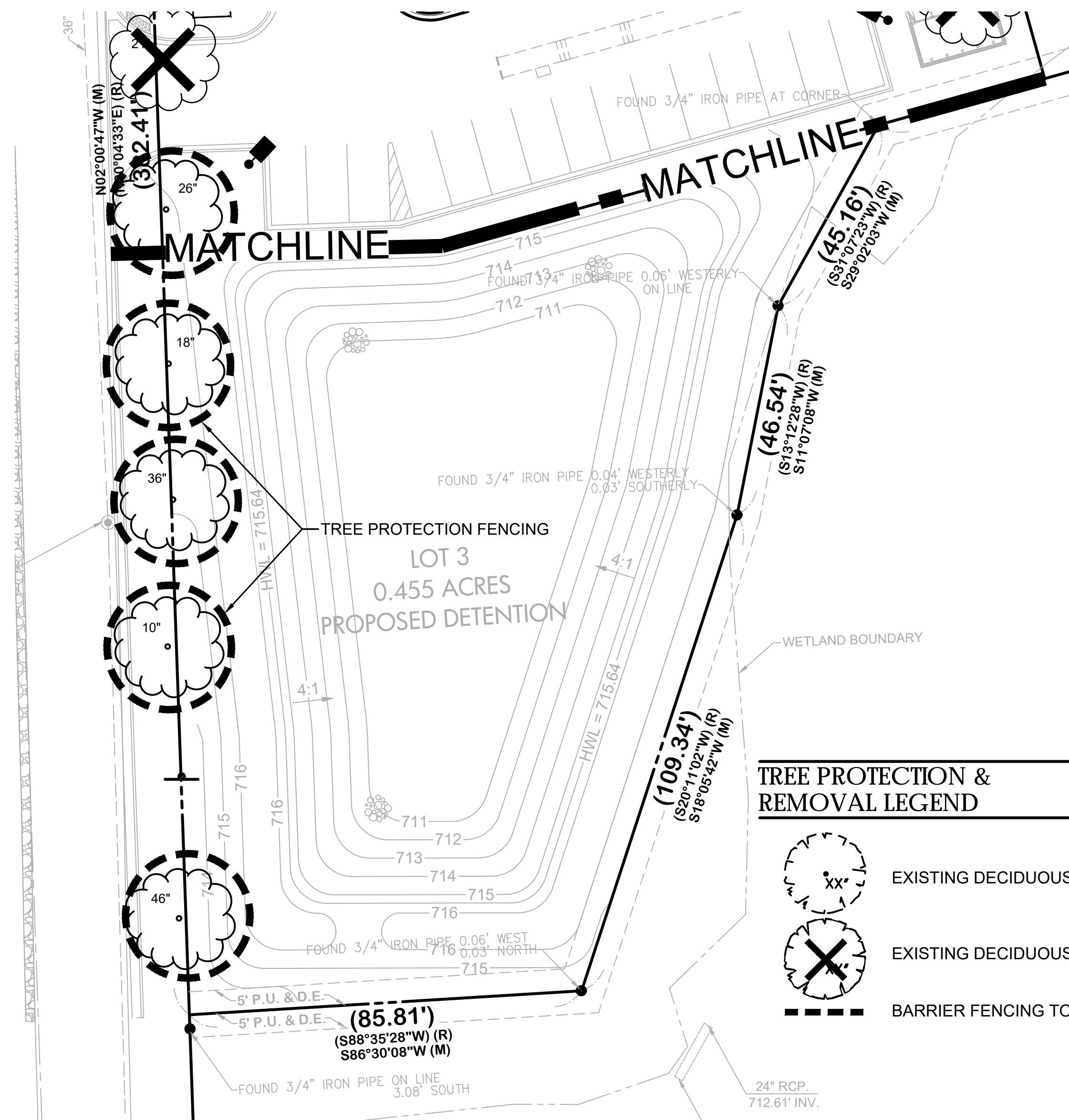
**SHEET TITLE**  
 TREE PROTECTION & REMOVAL PLAN

**SHEET NUMBER**

**L.1**

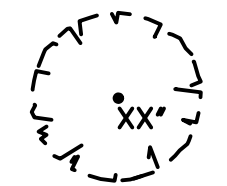
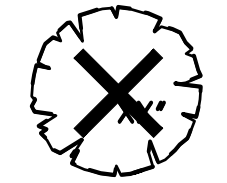

**TREE PROTECTION AND REMOVAL NOTES**

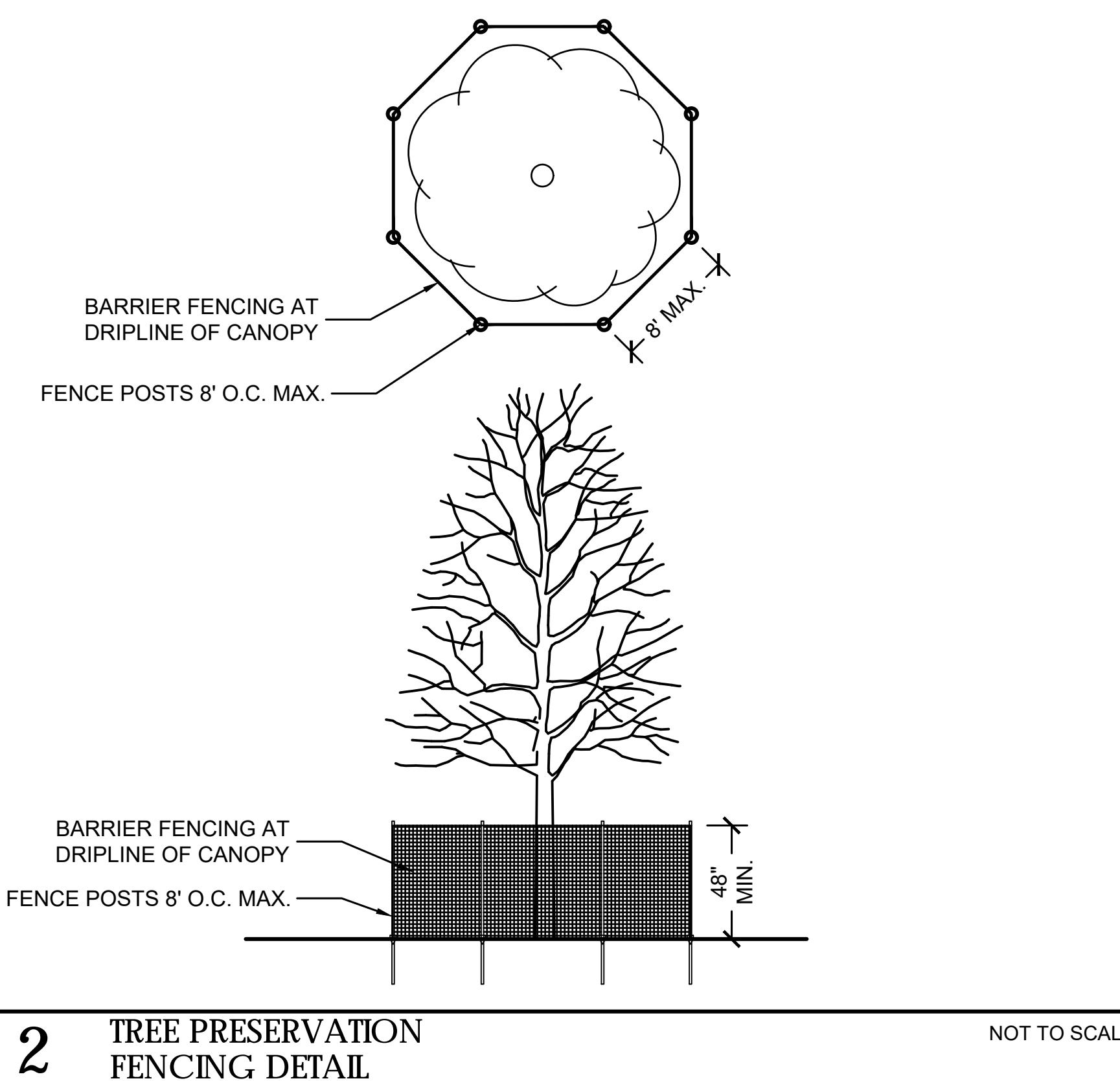
1. CONTRACTOR SHALL OBTAIN ALL NECESSARY STATE AND LOCAL PERMITS AND PERMISSIONS TO PRUNE, REMOVE, AND/OR TRANSPLANT ANY TREES ON SITE.
2. DEAD AND DYING MATERIAL ON THE SITE SHALL BE REMOVED OR PRUNED. MATERIALS NOT LABELED ON THE PROTECTION PLAN SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR REMEDIATION.
3. DURING CONSTRUCTION EXISTING TREES OVER FOUR INCHES IN CALIPER SHALL BE PROTECTED WITH BARRIER FENCING.
4. BARRIER SHALL BE CONSTRUCTED OF A MIN. 4'-5" TALL SNOW FENCE OR SIMILAR AND SUPPORT POSTS MIN. 8' O.C. AND SHALL BE ERECTED ONE FOOT BEYOND THE DRIP LINE OFF ALL EXISTING TREES ON SITE AND ADJACENT SITES TO REMAIN.
5. BARRIER FENCING SHOWN ON THE PLAN IS APPROXIMATE. CONTRACTOR SHALL ADJUST LOCATION OF BARRIER TO POSITION OUTLINED IN COMMENT 4.
6. NO EXCESS SOIL OR ADDITIONAL FILL, BUILDING MATERIALS OR DEBRIS SHALL BE PLACED WITHIN THE PROTECTIVE BARRIER.
7. NO VEHICLES OR HEAVY MACHINERY SHALL BE ALLOWED TO WORK WITHIN THE BARRIER AREA.
8. NO ATTACHMENTS OR WIRES, OTHER THAN PROTECTIVE GUY WIRES, SHALL BE ATTACHED TO ANY OF THE TREES WHICH ARE WITHIN PROTECTIVE BARRIER.



**1 TREE PROTECTION & REMOVAL PLAN SOUTH**

**TREE PROTECTION & REMOVAL LEGEND**

-  EXISTING DECIDUOUS TREE TO REMAIN
-  EXISTING DECIDUOUS TREE TO BE REMOVED
-  BARRIER FENCING TO BE INSTALLED



**2 TREE PRESERVATION FENCING DETAIL** NOT TO SCALE

**LG Workshop, LLC**  
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 ph. 773.697.4388  
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**PROJECT TEAM**

CIVIL ENGINEER:  


**PROJECT NAME**



2657 OGDEN AVENUE,  
 DOWNERS GROVE, IL 60515

**DRAWING ISSUED**

NO.	TITLE	DATE
1.	ISSUED PRELIM.	01/04/21
2.	Rev. Per Village Review	07/02/21

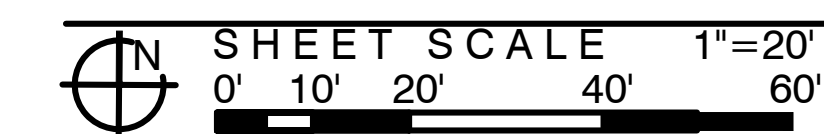
**SET TYPE**  
 PRELIMINARY LANDSCAPE  
 PLANS

**PROJECT NUMBER**  
 2012016

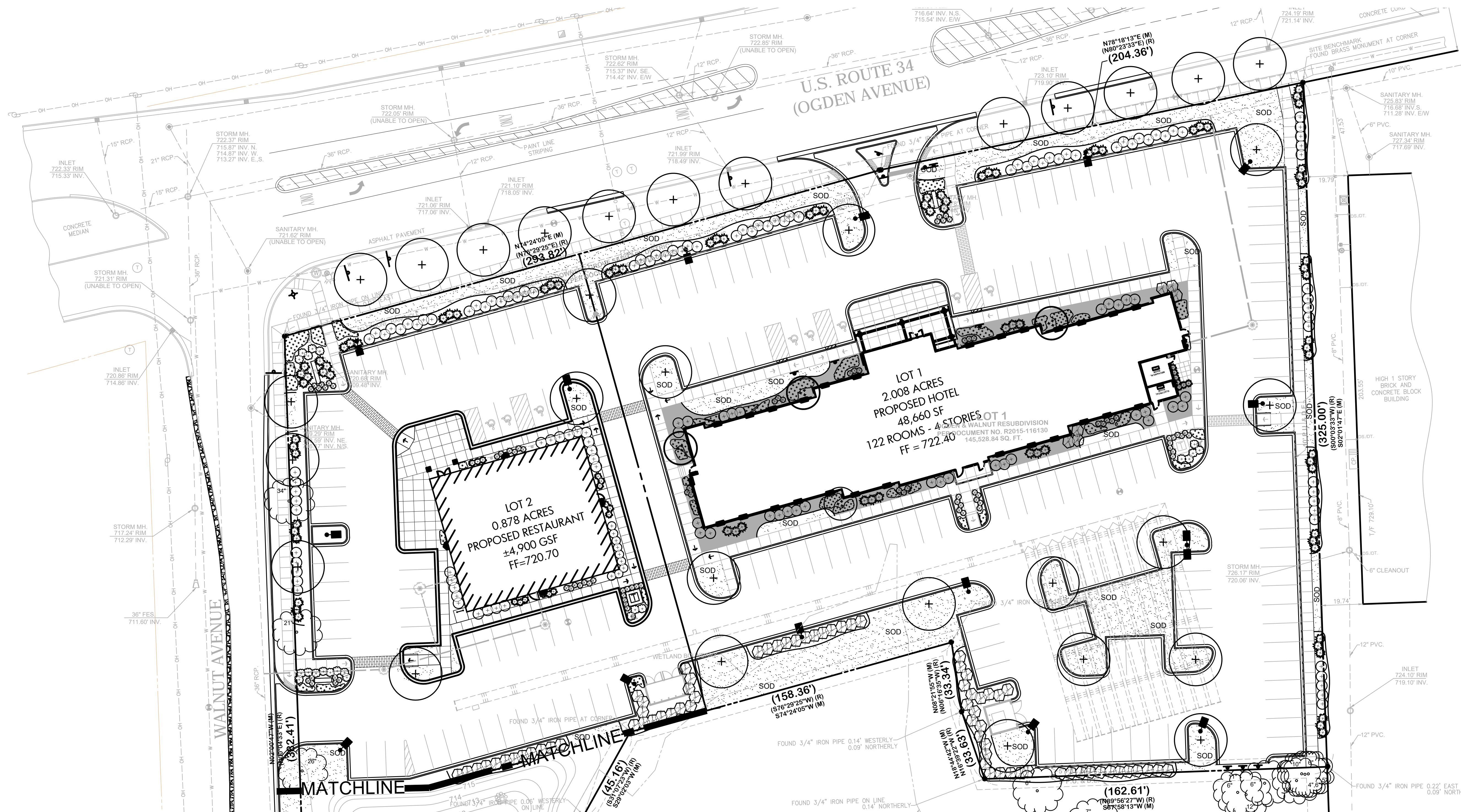
**DATE**  
 01-04-2020  
**DRAWN BY:** LCG **APPROVED BY:** LCG

**SHEET TITLE**  
 TREE PROTECTION & REMOVAL  
 PLAN SOUTH / DETAILS

**SHEET NUMBER**



**L.2**



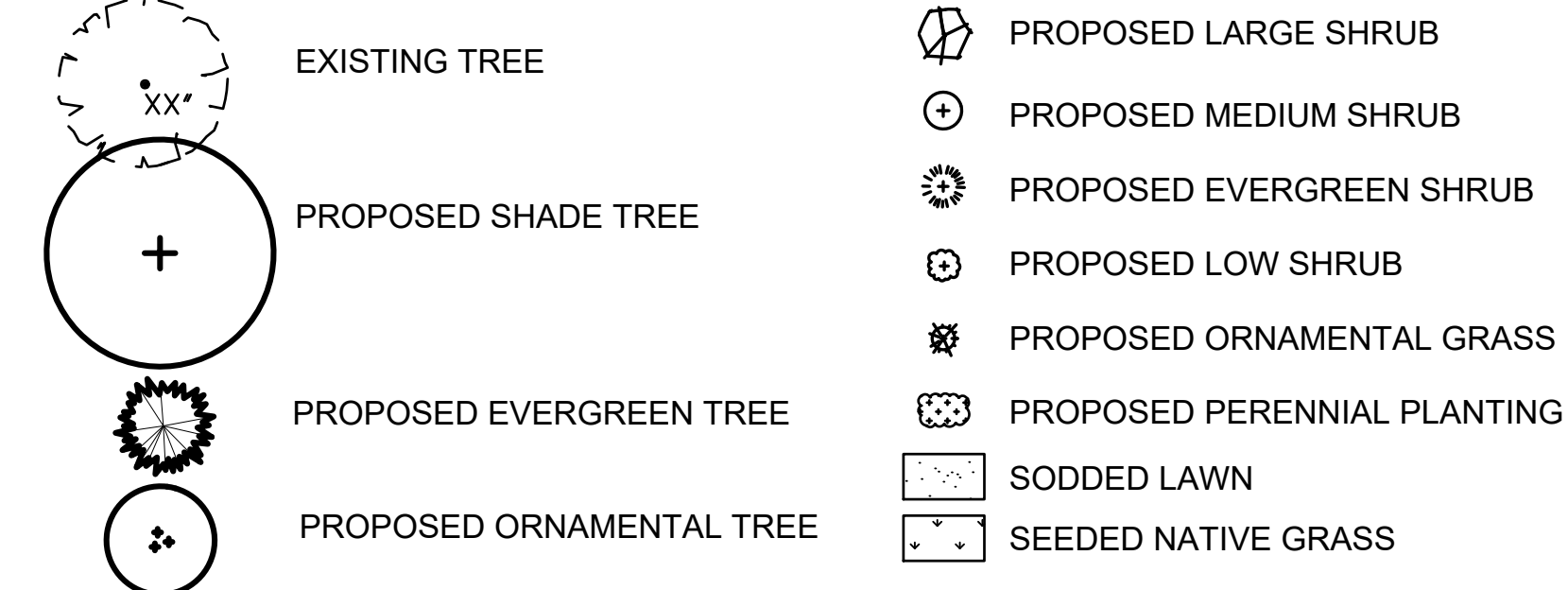
# 1 LANDSCAPE PLAN HOTEL & SIT DOWN RESTAURANT

### PLANT LIST

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT
<b>DECIDUOUS SHADE TREES</b>					
ACF	2.5" CAL.		ACER FREEMANII 'SIENNA'	SIENNA GLEN MAPLE	B&B
AMM	2.5" CAL.		ACER RUBRUM 'RED SUNSET'	RED SUNSET RED MAPLE	B&B
GDE	2.5" CAL.		GYMNOCCLADUS DIOIC. 'ESPRESSO'	ESPRESSO KENTUCKY COFFEETREE	B&B
GTS	2.5" CAL.		GLEDITSIA TRI. 'SHADEMASTER'	SHADEMASTER HONEYLOCUST	B&B
QUE	2.5" CAL.		QUERCUS ELLIPSOIDALIS	NORTHERN PIN OAK	B&B
PYC	2.5" CAL.		PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	B&B
TCG	2.5" CAL.		TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LITTLELEAF LINDEN	B&B
ULH	2.5" CAL.		ULMUS 'REGAL'	REGAL ELM	B&B
<b>ORNAMENTAL TREES</b>					
AGP	6' MULTI		AMELANCHIER GRAN. 'PRINCESS DI'	PRINCESS DIANA SERVICEBERRY	B&B
BNH	6' MULTI		BETULA NIGRA 'HERITAGE'	HERITAGE RIVER BIRCH	B&B
COF	6' MULTI		CORNUS FLORIDA	WHITE FLOWERING DOGWOOD	B&B
MDW	6' MULTI		MALUS SPP. 'DONALD WYMAN'	DONALD WYMAN CRABAPPLE	B&B
<b>DECIDUOUS SHRUBS</b>					
AAB	30" HT.		ARONIA ARBUT. 'BRILLIANTISSIMA'	BRILLIANT RED CHOKEBERRY	B&B
AMA	24" HT.		ARONIA MELANO. 'ELATA'	ELATA CHOKEBERRY	B&B
CAM	30" HT.		CORNUS ALBA 'ARGENTEO MARG.'	CREAM EDGE DOGWOOD	B&B
FOS	30" HT.		FORSYTHIA X INTERMED. 'SUNRISE'	SUNRISE FORSYTHIA	B&B
FVB	18" W.		FORSYTHIA VIRID. 'BRONXENSIS'	BRONX GREENSTEM FORSYTHIA	B&B
HYA	24" HT.		HYDRANGEA ARBOR. 'ANNABELLE'	ANNABELLE HYDRANGEA	B&B
POB	30" HT.		PYHOCARPUS OPUL. 'DIABOLO'	DIABOLO NINEBARK	B&B
RAG	18" W.		RHUS AROMATICA 'GRO-LOW'	GRO-LOW SUMAC	B&B
SNS	24" HT.		SPIREA NIPPONICA 'SNOWMOUND'	SNOWMOUND SPIRAEA	B&B
SPM	24" HT.		SYRINGA PATULA 'MISS KIM'	MISS KIM KOREAN LILAC	B&B
VCC	24" HT.		VIBURNUM CARLESII 'COMPACTUM'	DWARF KOREANSPICE VIBURNUM	B&B
VDS	30" HT.		VIBURNUM DENT. 'SYNNESTVEDT'	CHICAGO LUSTRE VIBURNUM	B&B
VTC	30" HT.		VIBURNUM TRILOBUM 'ALFREDO'	ALFREDO AMER. CRANBERRY BUSH	B&B

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT
<b>EVERGREEN SHRUBS</b>					
BMG	24" HT.		BUXUS MICRO. X. GREEN MOUNTAIN	GREEN MOUNTAIN BOXWOOD	B&B
JCK	24" W.		JUNIP. CHIN. 'KALLAY'S COMPACTA'	KALLAY COMPACT JUNIPER	B&B
TMT	24" W.		TAXUS X MEDIA 'TAUNTON'	TAUNTON YEW	B&B
TME	24" W.		TAXUS X MEDIA 'EVERLOW'	EVERLOW DENSE YEW	B&B
<b>ORNAMENTAL GRASSES</b>					
CAA	#3 CONT.		CALAMAGROSTIS ACUT. 'STRICTUS'	STRICTUS FEATHER REED GRASS	
MSZ	#3 CONT.		MISCANTHUS SIN. 'LITTLE ZEBRA'	LITTLE ZEBRA DWARF JAP. SILVER GRASS	
<b>GROUND COVER / PERENNIALS</b>					
ACH	#1 CONT.		ACHILLEA MILLE. 'PAPRIKA'	PAPRIKA YARROW	18" O.C.
ALS	#1 CONT.		ALLUM TANGUT. 'SUMMER BEAUTY'	SUMMER BEAUTY WILD ONION	18" O.C.
GEM	#1 CONT.		GERANIUM SANGUINEUM 'MAX FREI'	MAX FREI BLOODY CRANESBILL	18" O.C.
HEM	#1 CONT.		HEMEROCALLIS 'STELLA DE ORO'	STELLA DE ORO DAYLILY	15" O.C.
LAV	#1 CONT.		LAVENDULA 'MUNSTEAD STRAIN'	MUNSTEAD ENGLISH LAVENDAR	24" O.C.
NFW	#1 CONT.		NEPETA FASSENI 'WALKER'S LOW'	WALKER'S LOW CATMINT	24" O.C.
PHD	#1 CONT.		PHLOX DIVARICATA	BLUE PHLOX	15" O.C.
SED	#1 CONT.		SEDUM X 'AUTUMN JOY'	AUTUMN JOY SEDUM	18" O.C.
SES	#1 CONT.		SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS	15" O.C.
SEED	SQ. FT.		SEEDED NATIVE BIOSWALE / DETENTION MIX - SEE BELOW		
SOD	SQ. YD.		SOODED LAWN		
<b>SEED APPLICATION</b>					
BIOSWALE / DETENTION BASIN SEED MIX - AVAILABLE THROUGH PRAIRIE NURSERY, WESTFIELD, WI, 1-800-476-9453. WWW.PRAIRIENURSERY.COM					
APPLICATION RATE 10LBS PER ACRE					

### LANDSCAPE LEGEND



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SEAL



### PROJECT TEAM

CIVIL ENGINEER:  
**CivWORKS Consulting, LLC**

### PROJECT NAME



2657 OGDEN AVENUE,  
DOWNERS GROVE, IL 60515

### DRAWING ISSUED

NO.	TITLE	DATE
1.	ISSUED PRELIM.	01/04/21
2.	Rev. Per Village Review	07/02/21

### SET TYPE PRELIMINARY LANDSCAPE PLANS

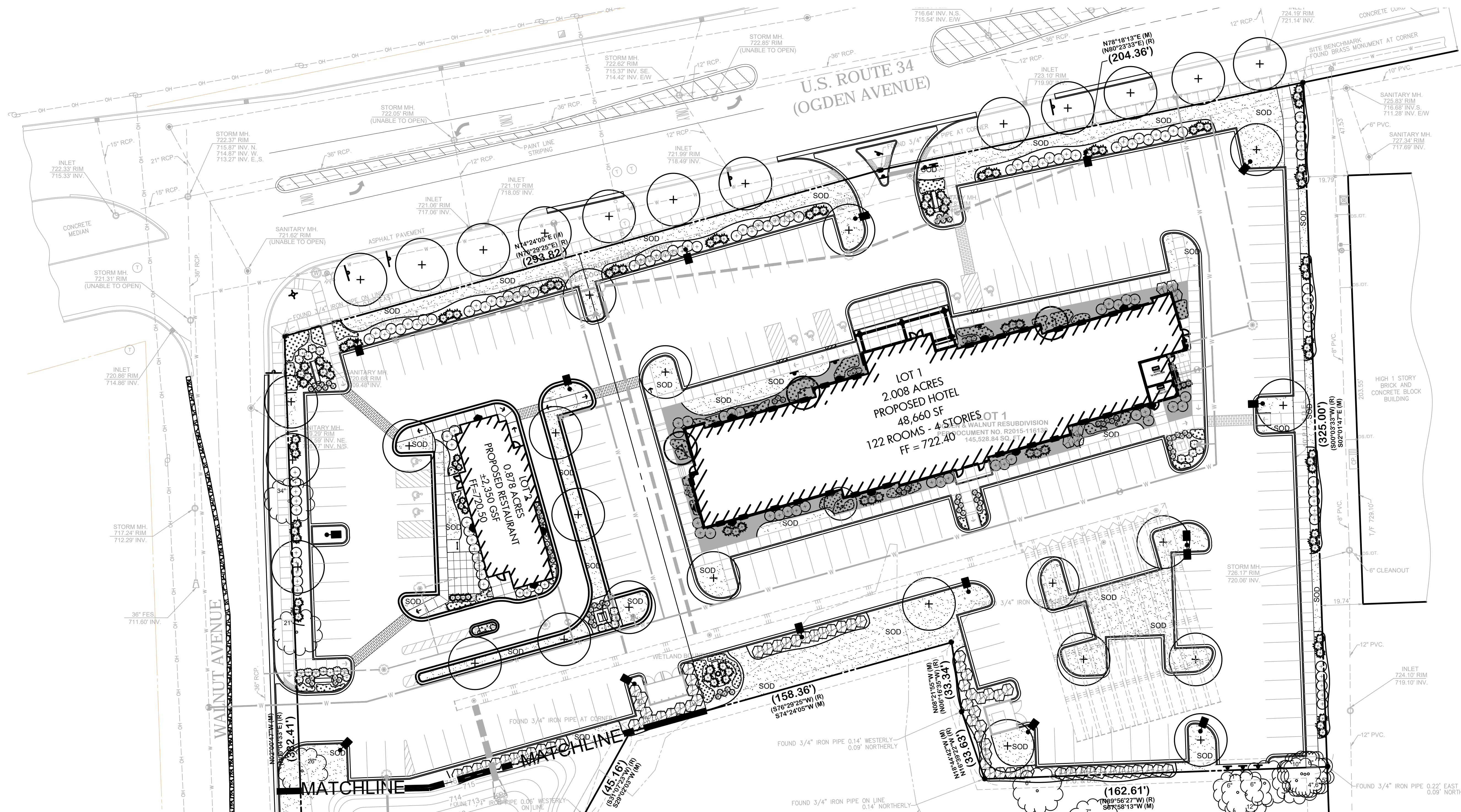
PROJECT NUMBER  
2012016

DATE  
01-04-2020  
DRAWN BY: APPROVED BY:  
LCG LCG

SHEET TITLE  
LANDSCAPE PLAN - HOTEL  
& SIT DOWN RESTAURANT

SHEET NUMBER

# L.3



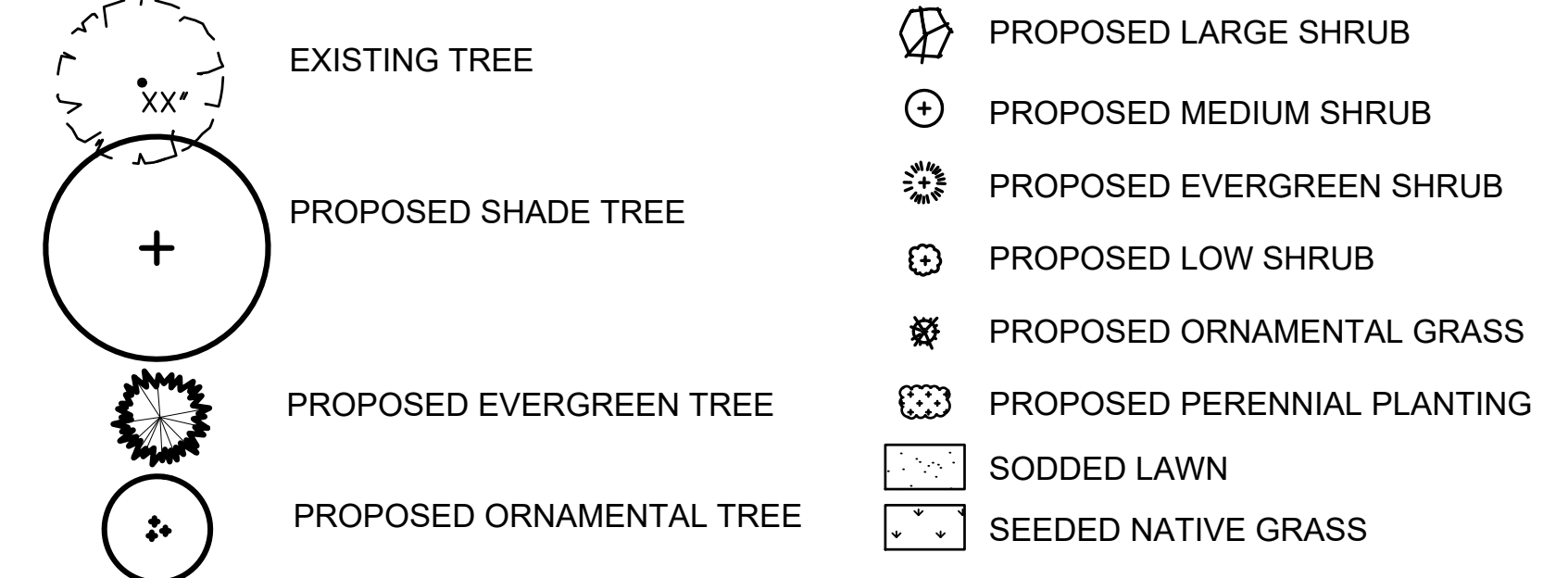
# 1 LANDSCAPE PLAN HOTEL & DRIVE THROUGH RESTAURANT

## PLANT LIST

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT
<b>DECIDUOUS SHADE TREES</b>					
ACF	2.5" CAL.		ACER FREEMANII 'SIENNA'	SIENNA GLEN MAPLE	B&B
AMM	2.5" CAL.		ACER RUBRUM 'RED SUNSET'	RED SUNSET RED MAPLE	B&B
GDE	2.5" CAL.		GYMNOCCLADUS DIOIC. 'ESPRESSO'	ESPRESSO KENTUCKY COFFEETREE	B&B
GTS	2.5" CAL.		GLEDITSIA TRI. 'SHADEMASTER'	SHADEMASTER HONEYLOCUST	B&B
QUE	2.5" CAL.		QUERCUS ELLIPSOIDALIS	NORTHERN PIN OAK	B&B
PYC	2.5" CAL.		PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	B&B
TCG	2.5" CAL.		TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LITTLELEAF LINDEN	B&B
ULH	2.5" CAL.		ULMUS 'REGAL'	REGAL ELM	B&B
<b>ORNAMENTAL TREES</b>					
AGP	6' MULTI		AMELANCHIER GRAN. 'PRINCESS DI'	PRINCESS DIANA SERVICEBERRY	B&B
BNH	6' MULTI		BETULA NIGRA 'HERITAGE'	HERITAGE RIVER BIRCH	B&B
COF	6' MULTI		CORNUS FLORIDA	WHITE FLOWERING DOGWOOD	B&B
MDW	6' MULTI		MALUS SPP. 'DONALD WYMAN'	DONALD WYMAN CRABAPPLE	B&B
<b>DECIDUOUS SHRUBS</b>					
AAB	30" HT.		ARONIA ARBUT. 'BRILLIANTISSIMA'	BRILLIANT RED CHOKEBERRY	B&B
AMA	24" HT.		ARONIA MELANO. 'ELATA'	ELATA CHOKEBERRY	B&B
CAM	30" HT.		CORNUS ALBA 'ARGENTEO MARG.'	CREAM EDGE DOGWOOD	B&B
FOS	30" HT.		FORSYTHIA X INTERMED. 'SUNRISE'	SUNRISE FORSYTHIA	B&B
FVB	18" W.		FORSYTHIA VIRID. 'BRONXENSIS'	BRONX GREENSTEM FORSYTHIA	B&B
HYA	24" HT.		HYDRANGEA ARBOR. 'ANNABELLE'	ANNABELLE HYDRANGEA	B&B
POB	30" HT.		PYHSOCARPUS OPUL. 'DIABOLO'	DIABOLO NINEBARK	B&B
RAG	18" W.		RHUS AROMATICA 'GRO-LOW'	GRO-LOW SUMAC	B&B
SNS	24" HT.		SPIREA NIPPONICA 'SNOWMOUND'	SNOWMOUND SPIRAEA	B&B
SPM	24" HT.		SYRINGA PATULA 'MISS KIM'	MISS KIM KOREAN LILAC	B&B
VCC	24" HT.		VIBURNUM CARLESII 'COMPACTUM'	DWARF KOREANSPICE VIBURNUM	B&B
VDS	30" HT.		VIBURNUM DENT. 'SYNNESTVEDT'	CHICAGO LUSTRE VIBURNUM	B&B
VTC	30" HT.		VIBURNUM TRILOBUM 'ALFREDO'	ALFREDO AMER. CRANBERRY BUSH	B&B

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT
<b>EVERGREEN SHRUBS</b>					
BMG	24" HT.		BUXUS MICRO. X. GREEN MOUNTAIN	GREEN MOUNTAIN BOXWOOD	B&B
JCK	24" W.		JUNIP. CHIN. 'KALLAY'S COMPACTA'	KALLAY COMPACT JUNIPER	B&B
TMT	24" W.		TAXUS X MEDIA 'TAUNTON'	TAUNTON YEW	B&B
TME	24" W.		TAXUS X MEDIA 'EVERLOW'	EVERLOW DENSE YEW	B&B
<b>ORNAMENTAL GRASSES</b>					
CAA	#3 CONT.		CALAMAGROSTIS ACUT. 'STRICTUS'	STRICTUS FEATHER REED GRASS	
MSZ	#3 CONT.		MISCANTHUS SIN. 'LITTLE ZEBRA'	LITTLE ZEBRA DWARF JAP. SILVER GRASS	
<b>GROUND COVER / PERENNIALS</b>					
ACH	#1 CONT.		ACHILLEA MILLE. 'PAPRIKA'	PAPRIKA YARROW	18" O.C.
ALS	#1 CONT.		ALLUM TANGUT. 'SUMMER BEAUTY'	SUMMER BEAUTY WILD ONION	18" O.C.
GEM	#1 CONT.		GERANIUM SANGUINEUM 'MAX FREI'	MAX FREI BLOODY CRANESBILL	18" O.C.
HEM	#1 CONT.		HEMEROCALLIS 'STELLA DE ORO'	STELLA DE ORO DAYLILY	15" O.C.
LAV	#1 CONT.		LAVENDULA 'MUNSTEAD STRAIN'	MUNSTEAD ENGLISH LAVENDAR	24" O.C.
NFW	#1 CONT.		NEPETA FASSENI 'WALKER'S LOW'	WALKER'S LOW CATMINT	24" O.C.
PHD	#1 CONT.		PHLOX DIVARICATA	BLUE PHLOX	15" O.C.
SED	#1 CONT.		SEDUM X 'AUTUMN JOY'	AUTUMN JOY SEDUM	18" O.C.
SES	#1 CONT.		SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS	15" O.C.
SEED	SQ. FT.		SEEDED NATIVE BIOSWALE / DETENTION MIX - SEE BELOW		
SOD	SQ. YD.		SOODED LAWN		
<b>SEED APPLICATION</b>					
BIOSWALE / DETENTION BASIN SEED MIX - AVAILABLE THROUGH PRAIRIE NURSERY, WESTFIELD, WI, 1-800-476-9453. WWW.PRAIRIENURSERY.COM					
APPLICATION RATE 10LBS PER ACRE					

## LANDSCAPE LEGEND



**LG Workshop LLC**  
Landscape Architecture  
Site Planning  
Illustration  
2324 W. Armitage Avenue  
Chicago, IL 60647  
ph. 773.697.4388  
www.LGWLA.com  
SEAL



## PROJECT TEAM

CIVIL ENGINEER:  
**CivWORKS Consulting, LLC**

## PROJECT NAME



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DRAWN BY: APPROVED BY:  
LCG LCG

SHEET TITLE  
LANDSCAPE PLAN - HOTEL  
& DRIVE THROUGH REST.

SHEET NUMBER

# L.4

SEAL



**PROJECT TEAM**

CIVIL ENGINEER:



**PROJECT NAME**



2657 OGDEN AVENUE,  
 DOWNERS GROVE, IL 60515

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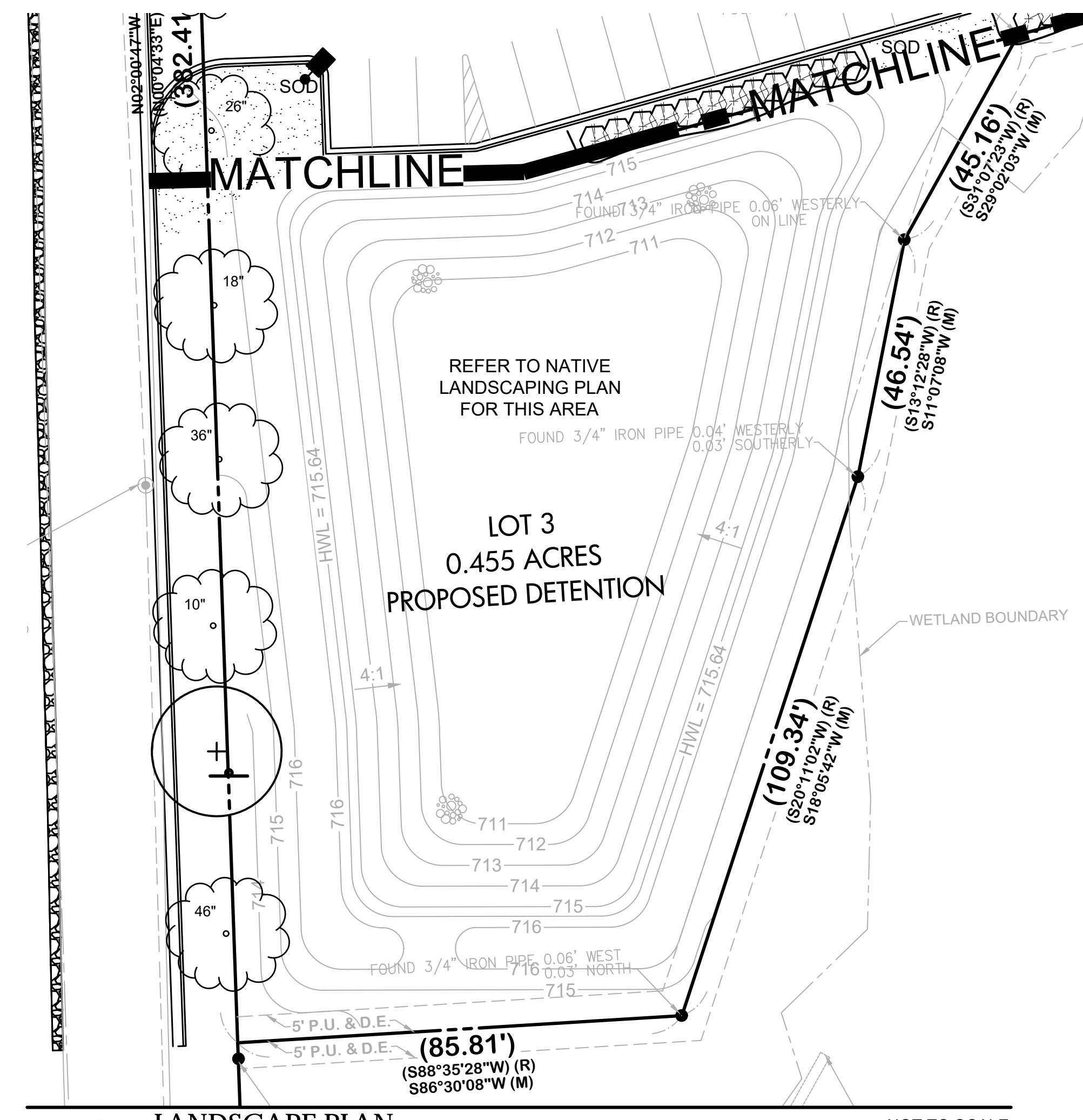
**PROJECT NUMBER**  
 2012016

**DATE**  
 01-04-2020  
**DRAWN BY:** LCG  
**APPROVED BY:** LCG

**SHEET TITLE**  
 LANDSCAPE PLAN / DETAILS  
 & NOTES

**SHEET NUMBER**

**L.5**



**5 LANDSCAPE PLAN SOUTH OF MATCHLINE** NOT TO SCALE

**LANDSCAPE NOTES**

- CONTRACTOR SHALL OBTAIN ALL NECESSARY LOCAL PERMITS AND PERMISSIONS TO INSTALL THE PROPOSED IMPROVEMENTS
- ALL LANDSCAPE MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE VILLAGE OF DOWNERS GROVE LANDSCAPING CODES AND ZONING ORDINANCES.
- PRIOR TO COMMENCING ANY WORK, CONTRACTOR SHALL HAVE DIGGERS HOTLINE LOCATE AND MARK ALL UNDERGROUND UTILITY FACILITIES AND LINES.
- ALL PLANT MATERIALS (EXCEPT FOR GROUNDCOVER, ANNUALS, AND PERENNIALS) SHALL BE BALLED AND BURLAPPED STOCK AND MEET CURRENT STANDARDS OF THE AMERICAN ASSOCIATION OF NURSERYMEN'S STANDARD FOR NURSERY STOCK (ANSI 260.1-1986) OR EQUAL. PLANT MATERIALS MUST BE SUPPLIED WITHIN A 150 MILE RADIUS OF PROJECT SITE WITHIN NORTHEAST ILLINOIS. CONTRACTOR MAY SUBSTITUTE CONTAINER STOCK FOR SHRUBS IF SIZES ARE EQUAL TO SPECIFIED B&B STOCK, WITH THE APPROVAL OF THE LANDSCAPE ARCHITECT.
- IF SPECIFIED PLANTS ARE NOT AVAILABLE AT THE TIME OF ORDERING, PLANTS WITH SIMILAR WHOLESALE VALUE AND LANDSCAPE CHARACTERISTICS MAY BE SUBSTITUTED UPON THE APPROVAL OF THE LANDSCAPE ARCHITECT AND VILLAGE STAFF.
- SOIL IN GROUNDCOVER BEDS SHALL BE AMENDED USING 2 INCHES OF MUSHROOM COMPOST INCORPORATED INTO THE TOP 4 INCHES OF SOIL.
- DISTURBED AREAS TO RECEIVE SOD SHALL BE TILLED TO 6" DEPTH AND FINE GRADED TO PROVIDE SMOOTH BASE SURFACE. IF EXISTING SOIL IS A MAJORITY OF CLAY OR UNSUITABLE, 2" OF FINE GRADED TOPSOIL SHALL BE ADDED PRIOR TO TILLING. EXISTING SOD AREAS SHALL HAVE TURF REMOVED WITH AUTOMATED SODCUTTER OR HAND SPACE TO REMOVE ALL BLADES AND ROOTS. 1" OF FINE GRADED TOPSOIL SHALL BE TILLED AND GRADED.
- TREE AND SHRUB BACKFILL MIXTURE SHALL BE 2 PARTS EXIST. NATIVE TOPSOIL AND 1 PART SPHAGNUM PEAT MOSS W/ DECOMPOSED MANURE.
- ALL SHRUB BEDS AND INDIVIDUAL TREE PLANTINGS, UNLESS OTHERWISE NOTED, SHALL RECEIVE A 4 INCH LAYER OF SHREDDED HARDWOOD MULCH. ALL GROUNDCOVER, ANNUAL AND PERENNIAL BEDS SHALL RECEIVE A 2 INCH LAYER OF THE SAME MULCH MATERIAL. COSTS FOR MULCH SHALL BE CONSIDERED INCIDENTAL AND SHALL BE INCLUDED IN THE COST OF PLANTINGS.
- NURSERY TAGS (SPECIES, SIZE) FOR ALL SHADE TREES SHALL REMAIN ATTACHED TO TREES UNTIL FINAL APPROVAL FROM MUNICIPALITY.
- THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER A BONDED WRITTEN ONE-YEAR WARRANTY AGREEMENT (BEGINNING ON THE OWNER'S POSSESSION DATE). THIS AGREEMENT SHALL COVER MAINTENANCE, REPAIR, AND REPLACEMENT OF ALL DEAD OR DAMAGED LANDSCAPING TO PRESERVE THE SAME QUANTITY AND QUALITY AS INITIALLY APPROVED.
- CONTRACTOR SHALL PROVIDE A SEPARATE ESTIMATE FOR AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM FOR COMPLETE EFFECTIVE COVERAGE OF ALL LAWN AREAS AND SHRUB BEDS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL AND APPLY FOR ALL NECESSARY PERMITS PRIOR TO COMMENCING WORK. IRRIGATION PLANS SHALL INCLUDE HUNTER PRO-C CONTROLLER W/WIRELESS SOLAR SYNC STATION AND HUNTER SPRAYHEADS AND NOZZLES. IRRIGATION WORK SHALL BE WARRANTY ALL LABOR AND MATERIALS FOR 1 FULL YEAR AFTER INSTALLATION AND TESTING.
- SEEDED LAWN AREAS SHALL BE BID WITH A BID ALTERNATE FOR HYDROSEEDING LAWN. PRIOR TO SEEDING, 2" OF FINE TOPSOIL SHALL BE TILLED INTO EXIST SOIL MIXTURE. A MIX CONSISTING OF ROUGHLY 30% BLUEGRASS / 30% FINE FESCUES / 40% RYE GRASSES (AND TACKIFIER FOR HYDROSEEDING) SHALL BE APPLIED AT MANUFACTURERS SPECIFIED RATES FOR NEW LAWNS BETWEEN 5 AND 10 LBS PER 1,000 SF.
- TREES AND SHRUBS SHALL NOT BE LOCATED CLOSER THAN TEN (10) FEET TO FIRE HYDRANTS, TRANSFORMERS OR OTHER ABOVE GROUND UTILITIES. ANY DISCREPANCY ON THE PLAN RELATED TO THESE PROXIMATE UTILITIES SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR RESOLUTION.
- PLANTING BEDS BETWEEN BUILDING AND PERIMETER SIDEWALK SHALL HAVE 3/4" TO 1-1/2" DECORATIVE RIVER ROCK - BLUE / GREY MIX - INSTALLED AT A DEPTH OF 2" IN LIEU OF MULCH.

**VILLAGE LANDSCAPE REQUIREMENT CALCULATIONS**

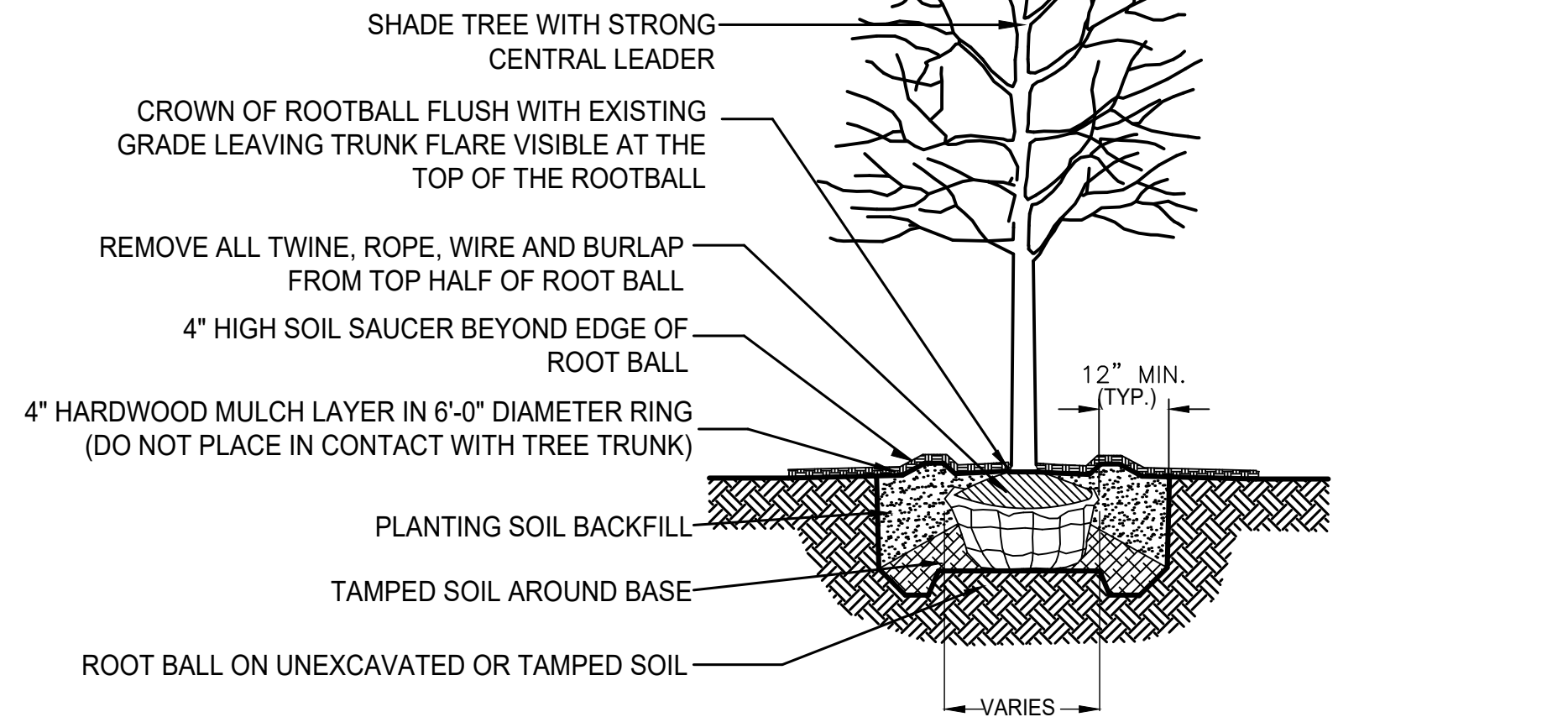
OPEN SPACE

LOT SIZE	VILLAGE REQ. OPEN SPACE	PROPOSED OPEN SPACE	VILLAGE REQ. STREETYARD OS	PROPOSED STREETYARD OS
145,529 SF	10% = 14,552 SF	43,040 SF = 30.6%	50% = 7,276 SF	83% = 12,067 SF

SEC 28.8.020 PARKING LOT PERIMETER LANDSCAPING

PERIMETER LOCATION	LENGTH	REQUIRED COVERAGE	PROPOSED COVERAGE	REQUIRED TREES	PROPOSED TREES
NORTH (OGDEN)	498 LF	75% = 373 LF	374 LF	498 / 30 = 17	12 TREES
EAST	325 LF	50% = 163 LF	180 LF	N/A	N/A
SOUTH	545 LF	50% = 273 LF	290 LF	N/A	N/A
WEST (WALNUT)	382 LF	50% = 191 LF	192 LF	382 / 30 = 13	11 TREES

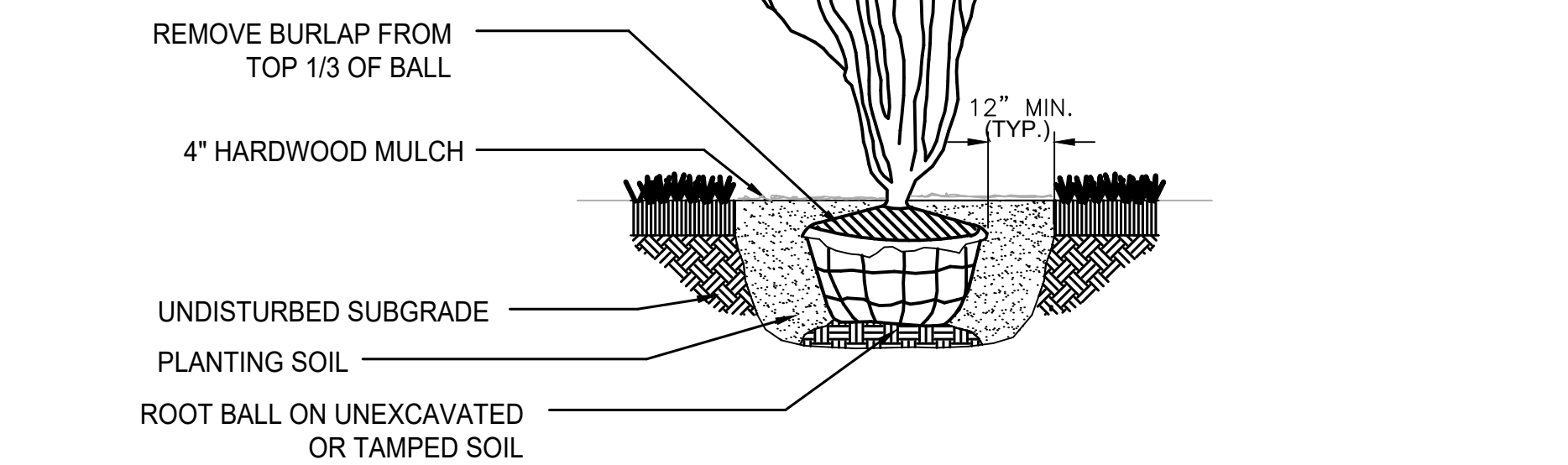
\* CALCULATIONS SHOWN BASED ON HOTEL & SIT DOWN RESTAURANT PLAN



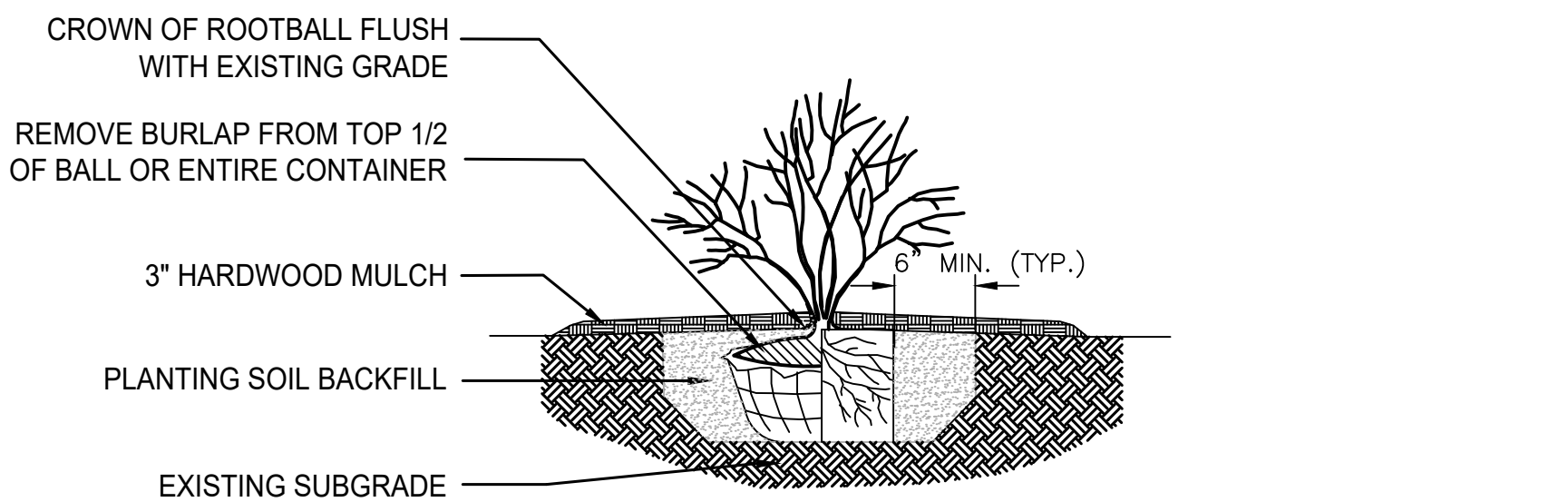
**1 SHADE TREE PLANTING DETAIL** NOT TO SCALE

NOTES:

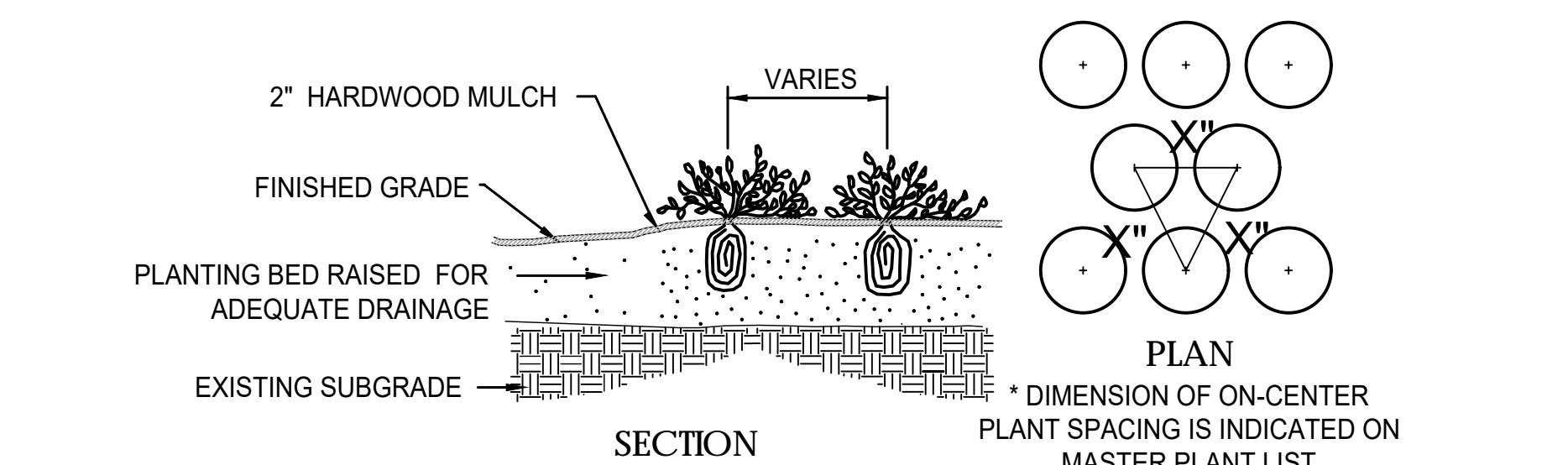
NEVER CUT LEADERS TREE SHALL BEAR SAME RELATION TO FINISHED GRADE AS IT BORE TO PREVIOUS GRADE.



**2 ORNAMENTAL TREE PLANTING DETAIL** NOT TO SCALE



**3 SHRUB PLANTING DETAIL** NOT TO SCALE



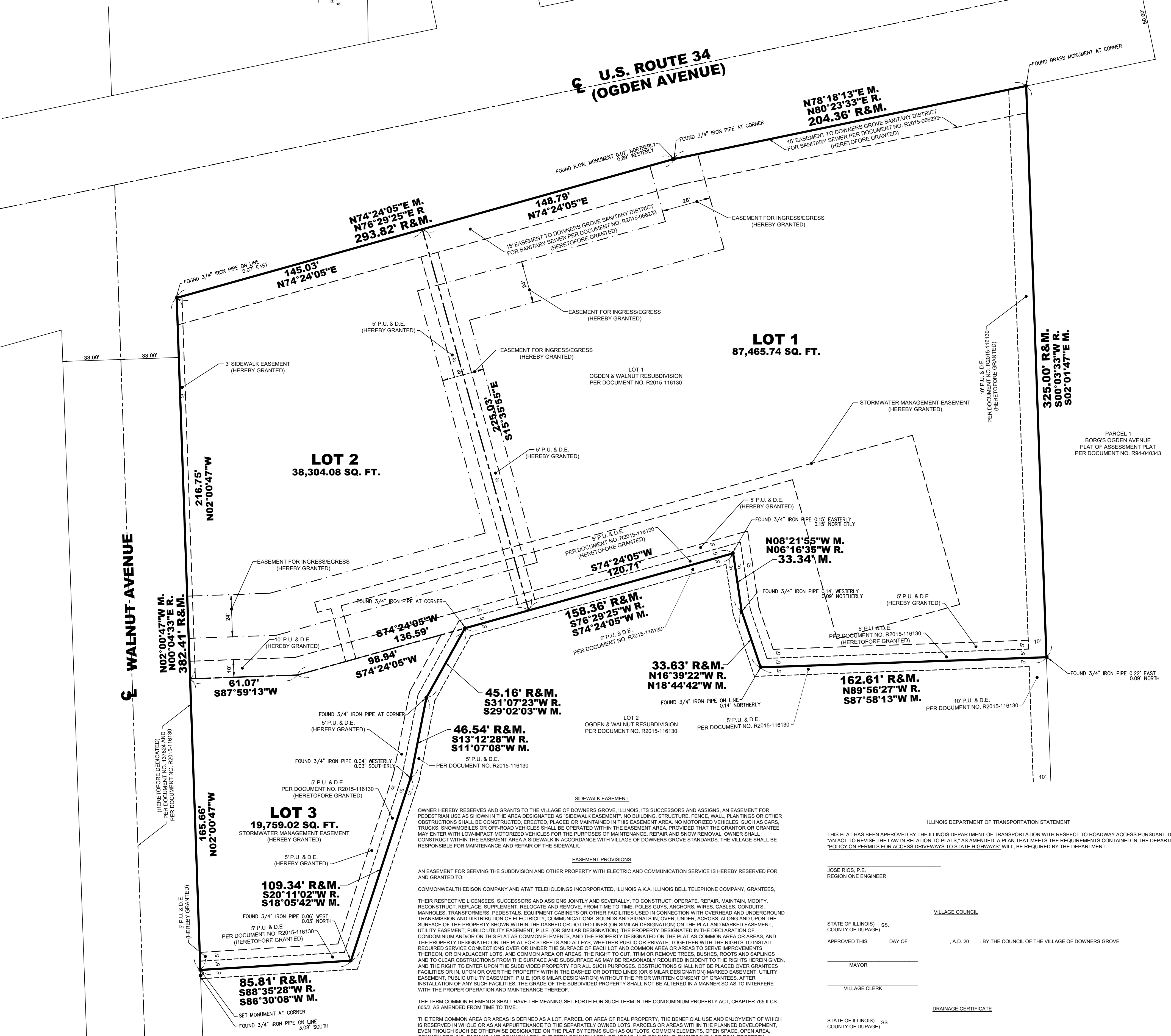
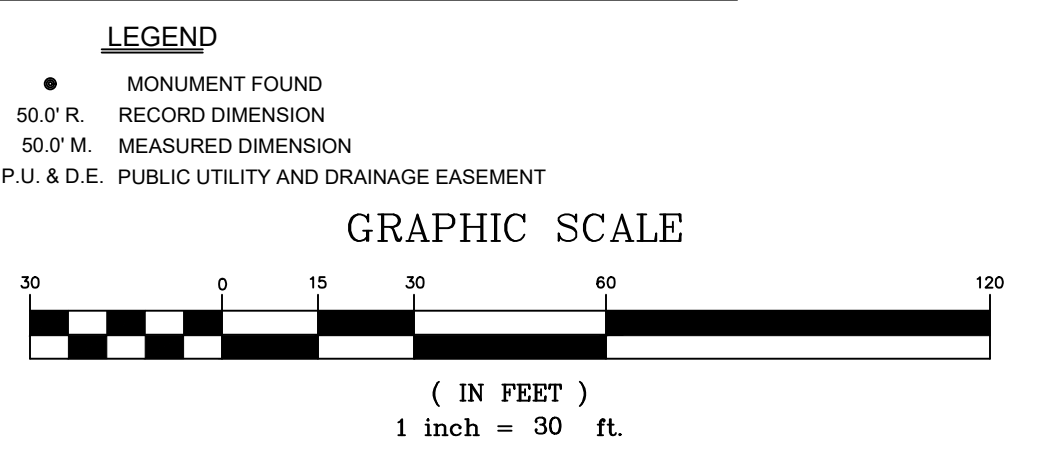
**4 PERENNIAL / ANNUAL PLANTING DETAIL** NOT TO SCALE



COMMON ADDRESS: 2657 OGDEN AVENUE, DOWNERS GROVE, ILLINOIS, PIN: 08-01-304-031-0000

EVH FRANKENSTRAT PLAT OF SUBDIVISION

OF PART OF THE SOUTHWEST QUARTER OF SECTION 1, TOWNSHIP 38 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DOWNERS GROVE, ILLINOIS.



OWNER HEREBY RESERVES AND GRANTS TO THE VILLAGE OF DOWNERS GROVE, ILLINOIS, ITS SUCCESSORS AND BELIEF, AN EASEMENT FOR PEDESTRIAN USE AS SHOWN IN THE AREA DESIGNATED AS "SIDEWALK EASEMENT" NO BUILDING, STRUCTURE, FENCE, WALL, PLANTINGS OR OTHER OBSTRUCTIONS SHALL BE CONSTRUCTED, ERRECTED, PLACED OR MAINTAINED IN THIS EASEMENT AREA...

ILLINOIS DEPARTMENT OF TRANSPORTATION STATEMENT: THIS PLAT HAS BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION WITH RESPECT TO ROADWAY ACCESS PURSUANT TO §2 OF AN ACT TO REVISE THE LAW IN RELATION TO PLATS...

VILLAGE COUNCIL: APPROVED THIS \_\_\_ DAY OF \_\_\_ A.D. 20\_\_\_ BY THE COUNCIL OF THE VILLAGE OF DOWNERS GROVE. MAYOR: VILLAGE CLERK: COUNTY CLERK: COUNTY RECORDER: SURVEYORS CERTIFICATE: STATE OF ILLINOIS, SS. COUNTY OF DUPAGE: I, MICHAEL NELSON, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-3095, HAVE SURVEYED AND RESUBDIVIDED THE PROPERTY DESCRIBED AS FOLLOWS...

OWNERS CERTIFICATE: STATE OF ILLINOIS, SS. COUNTY OF DUPAGE: I, DO HEREBY CERTIFY THAT AS A HOLDER OF THE RECORD TITLE TO THE PROPERTY DESCRIBED HEREON, THAT WE HAVE CAUSED THE SAME TO BE SURVEYED AND SUBDIVIDED AS SHOWN ON THE PLAT HEREOF DRAWN...

NOTARY CERTIFICATE: STATE OF ILLINOIS, SS. COUNTY OF DUPAGE: I, A NOTARY PUBLIC, IN AND FOR SAID COUNTY, IN THE STATE OF ILLINOIS, DO HEREBY CERTIFY THAT I AM PERSONALLY KNOWN BY ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT AS OWNERS...

SCHOOL DISTRICT CERTIFICATE: STATE OF ILLINOIS, SS. COUNTY OF DUPAGE: THE UNDERSIGNED, AS OWNER OF THE PROPERTY DESCRIBED HEREON IN THE SURVEYORS'S CERTIFICATE, IS LOCATED WITHIN THE BOUNDARIES OF THE DISTRICT 99 HIGH SCHOOL DISTRICT AND DISTRICT 58 ELEMENTARY SCHOOL DISTRICT, IN DUPAGE COUNTY, ILLINOIS.

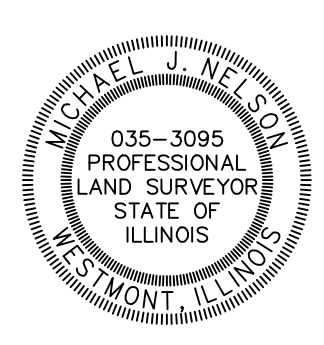
PLAN COMMISSION: STATE OF ILLINOIS, SS. COUNTY OF DUPAGE: APPROVED BY THE PLAN COMMISSION OF THE VILLAGE OF DOWNERS GROVE, ILLINOIS, THIS \_\_\_ DAY OF \_\_\_ A.D. 20\_\_\_

DOWNERS GROVE SANITARY DISTRICT: STATE OF ILLINOIS, SS. COUNTY OF DUPAGE: I, COLLECTOR OF THE DOWNERS GROVE SANITARY DISTRICT, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE BEEN APPOINTED AGAINST THE TRACT OF LAND INCLUDED IN THIS PLAT.

DOWNERS GROVE VILLAGE COLLECTOR: STATE OF ILLINOIS, SS. COUNTY OF DUPAGE: I, COLLECTOR OF THE VILLAGE OF DOWNERS GROVE, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE BEEN APPOINTED AGAINST THE TRACT OF LAND INCLUDED IN THIS PLAT.

DECLARATION OF RESTRICTIVE COVENANTS: DECLARATION OF RESTRICTIVE COVENANTS THE UNDERSIGNED OWNER HEREBY DECLARES THAT THE REAL PROPERTY DESCRIBED IN AND DEPICTED ON THIS PLAT OF SUBDIVISION SHALL BE HELD, TRANSFERRED, SOLD, CONVEYED AND OCCUPIED SUBJECT TO THE FOLLOWING COVENANTS AND RESTRICTIONS: (A) ALL PUBLIC UTILITY STRUCTURES AND FACILITIES, WHETHER LOCATED ON PUBLIC OR PRIVATE PROPERTY, SHALL BE CONSTRUCTED WHOLLY UNDERGROUND, EXCEPT FOR TRANSFORMERS, TRANSFORMER PADS, LIGHT POLES, REGULATORS, VALVES, MARKERS AND SIMILAR STRUCTURES APPROVED BY THE VILLAGE ENGINEER OF THE VILLAGE OF DOWNERS GROVE, ILLINOIS...

STORMWATER MANAGEMENT EASEMENT PROVISIONS: OWNER HEREBY GRANTS TO THE VILLAGE OF DOWNERS GROVE A STORMWATER MANAGEMENT EASEMENT FOR THE USE AND BENEFIT OF THE VILLAGE. OVER THE STORMWATER FACILITIES WITHIN THE PROPERTY AND A RIGHT OF ACCESS TO PRIVATELY OWNED LAND FOR THE REASONABLE EXERCISE OF THE RIGHTS GRANTED TO THE VILLAGE. OWNER SHALL BE RESPONSIBLE TO INSPECT AND MAINTAIN THE STORMWATER FACILITIES ON THE PROPERTY, NO BUILDINGS OR STRUCTURES OF ANY KIND SHALL BE PLACED ON SAID EASEMENT NOR SHALL ANY OTHER CHANGE BE MADE ON THE PROPERTY THAT MIGHT MATERIALLY AFFECT THE PROPER MANAGEMENT, OPERATION OR CONTINUED MAINTENANCE OF ANY STORMWATER FACILITY...



# Traffic Impact Study

## Proposed Hotel Development

Downers Grove, Illinois



Prepared For:



September 3, 2021

# 1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed hotel development to be located in the southeast quadrant of the intersection of Ogden Avenue (US Route 34) with Walnut Avenue in Downers Grove, Illinois. As proposed, the site is to be developed with a 122-room business hotel and an approximately 4,900 square foot sit-down restaurant. Access to the development is proposed to be provided via a proposed right-in/right-out access drive off Ogden Avenue and via a full movement access drive off Walnut Avenue.

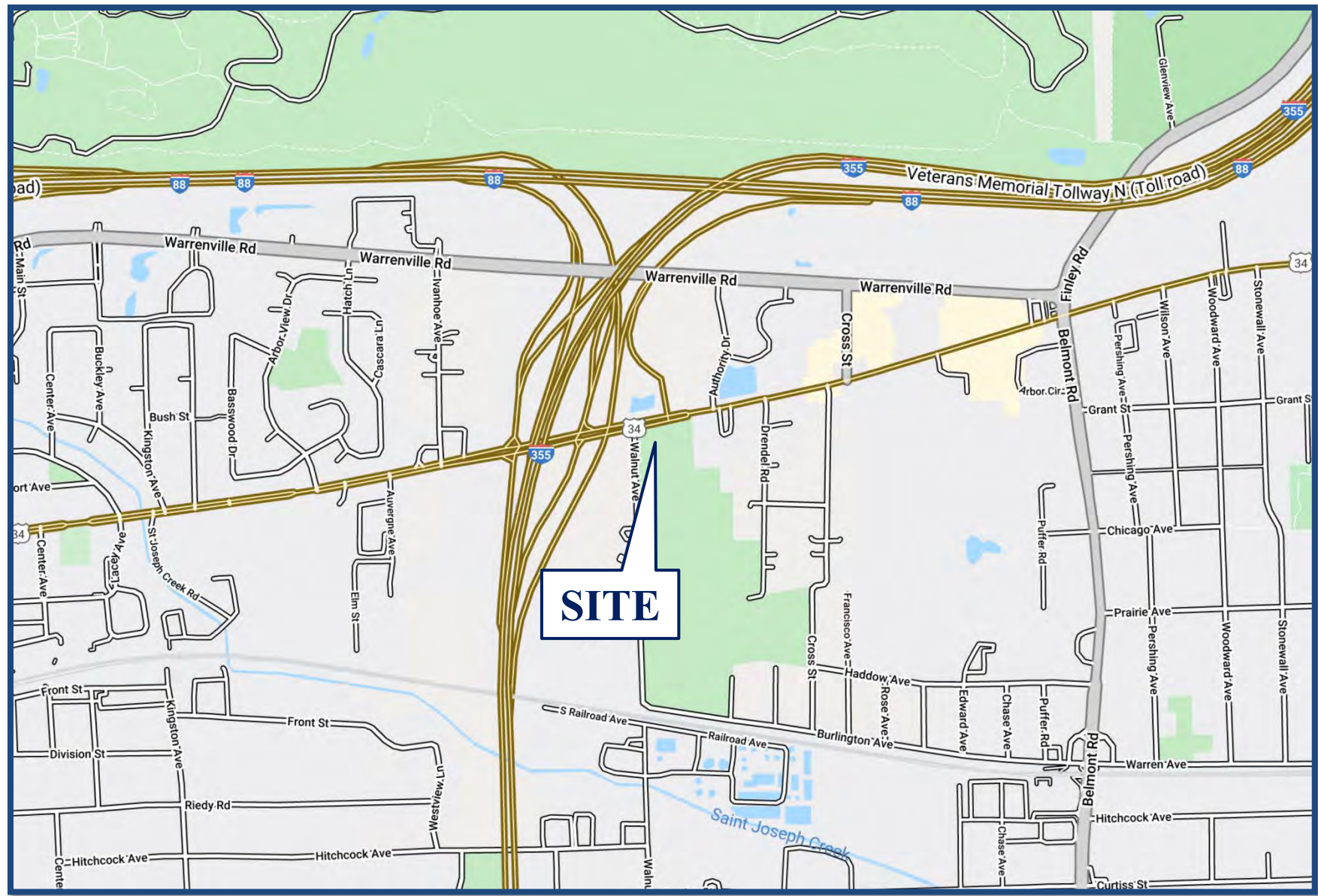
The purpose of this study was to examine background traffic conditions, assess the impact that the proposed development will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate the traffic generated by the proposed development. **Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site.

The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed development
- Directional distribution of the development traffic
- Vehicle trip generation for the development
- Future traffic conditions including access to the development
- Traffic analyses for the weekday morning and weekday evening peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning and weekday evening peak hours for the following conditions:

1. Year 2020 Base Conditions – Analyzes the capacity of the existing roadway system using peak hour traffic volumes conducted in 2020 adjusted to represent pre-pandemic conditions.
2. Year 2026 No-Build Conditions – Analyzes the capacity of the existing roadway system using Year 2020 base traffic volumes increased by an ambient area growth factor not attributable to any particular development.
3. Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the existing traffic volumes, ambient area growth not attributable to any particular development, and the traffic estimated to be generated by the proposed development.



Site Location

*Proposed Hotel Development  
Downers Grove, Illinois*

Figure 1





Aerial View of Site

Figure 2

## 2. Existing Conditions

Existing transportation conditions in the vicinity of the site were documented based on field visits conducted by KLOA, Inc. in order to obtain a database for projecting future conditions. The following provides a description of the geographical location of the site, physical characteristics of the area roadway system including lane usage and traffic control devices, and existing peak hour traffic volumes.

### Site Location

The site, which is currently vacant, is bounded by Ogden Avenue to the north, Grainger Industrial Supply to the east, Belmont Prairie Nature Preserve to the south, and Walnut Avenue to the west. Other land uses in the vicinity of the site are primarily residential south of the site and commercial east of the site. The Illinois Tollway Headquarters is located on the north side of Ogden Avenue opposite the site.

### Existing Roadway System Characteristics

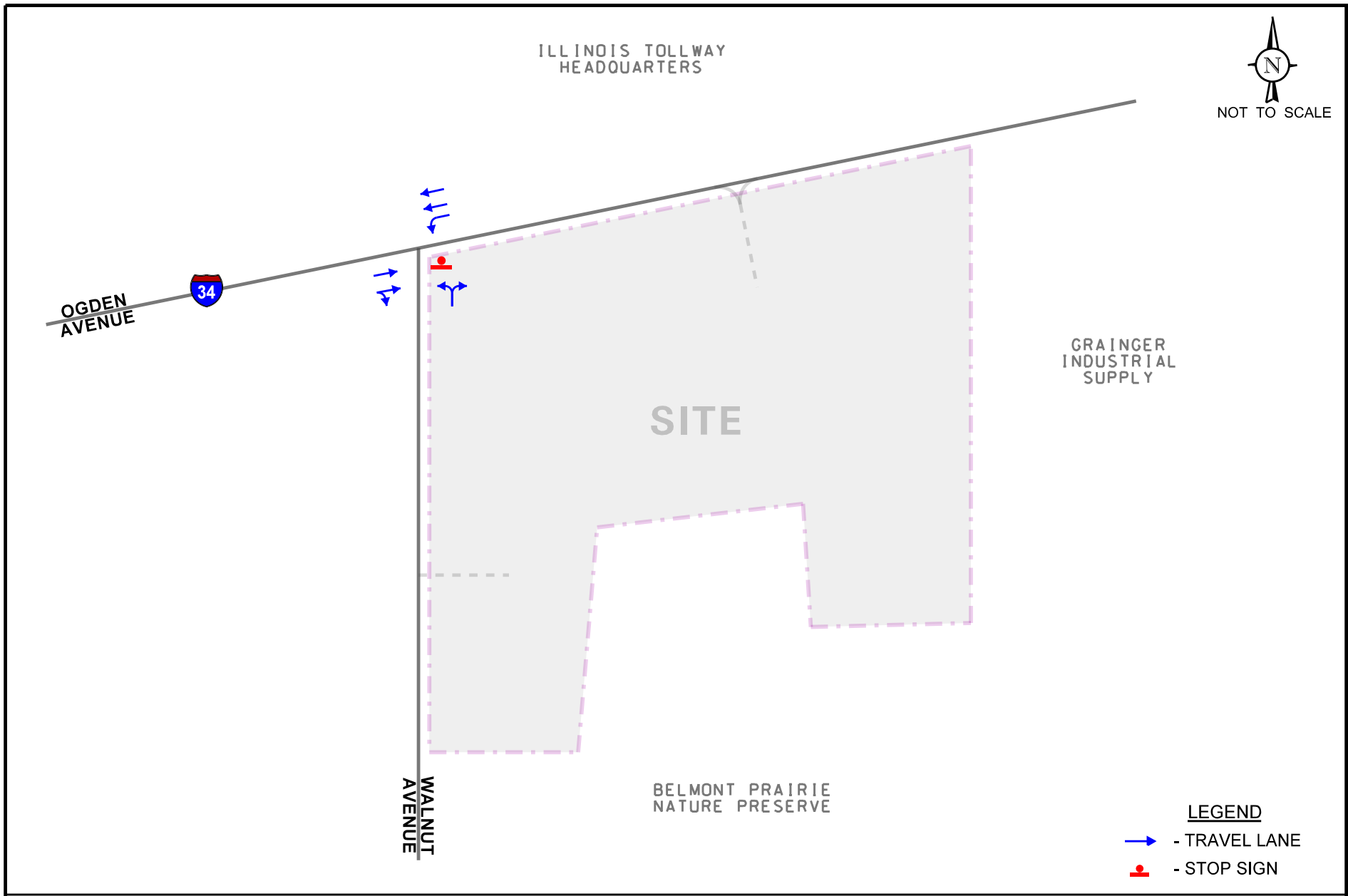
The characteristics of the existing roadways that surround the proposed development are illustrated in **Figure 3** and described below.

*Ogden Avenue (US Route 34)* is an east-west, other principal arterial roadway that provides two lanes in each direction generally divided by two-way left-turn lane. At its unsignalized intersection with Walnut Avenue, Ogden Avenue provides a through lane and a shared through/right-turn lane on the eastbound approach and an exclusive left-turn lane and two through lanes on the westbound approach. Ogden Avenue is under the jurisdiction of the Illinois Department of Transportation (IDOT), has a posted speed limit of 40 mph, is not designated as a Strategic Regional Arterial (SRA) route, and carries an Annual Average Daily Traffic (AADT) volume of 22,400 vehicles.

*Walnut Avenue* is a north-south local roadway that extends south from Ogden Avenue provides one lane in each direction and has a posted speed limit of 25 mph. At its unsignalized intersection with Ogden Avenue, Walnut Avenue terminates and provides a shared left-turn/right-turn lane on the northbound approach. 94<sup>th</sup> Avenue is under the jurisdiction of the village of Downers Grove and carries an AADT of 1,250 vehicles (IDOT 2016)

### Year 2020 Base Traffic Volumes

In order to determine current traffic conditions within the study area, KLOA, Inc. conducted peak period traffic, pedestrian, and bicycle counts at the intersection of and Ogden Avenue with the Walnut Avenue. The traffic counts were conducted on Tuesday September 22, 2020 during the morning (7:00 A.M. to 9:00 A.M.) and evening (4:00 P.M. to 6:00 P.M.) peak periods. The results of the traffic counts show that the peak hours of traffic generally occur between 7:30 A.M. and 8:30 A.M. during the weekday morning peak period and between 4:30 P.M. and 5:30 P.M. during the weekday evening peak period.



Proposed Hotel  
Development  
Downers Grove, Illinois

Existing Roadway Characteristics

In order to accurately represent Year 2020 conditions due to the ongoing pandemic, the traffic volumes were compared with traffic counts previously conducted at the intersection of Ogden Avenue with Cross Street by KLOA, Inc. in 2016 increased by a regional growth factor (as discussed later in the report). The comparison indicated that the previous adjusted counts were approximately 50 percent higher during the weekday morning peak hour and 25 percent higher during the weekday evening peak hour than the 2020 traffic counts. As such, the 2020 traffic counts were adjusted accordingly to reflect Year 2020 base (normal conditions) traffic volumes.

Copies of the traffic count summary sheets are included in the Appendix. **Figure 4** illustrates the Year 2020 base traffic volumes.

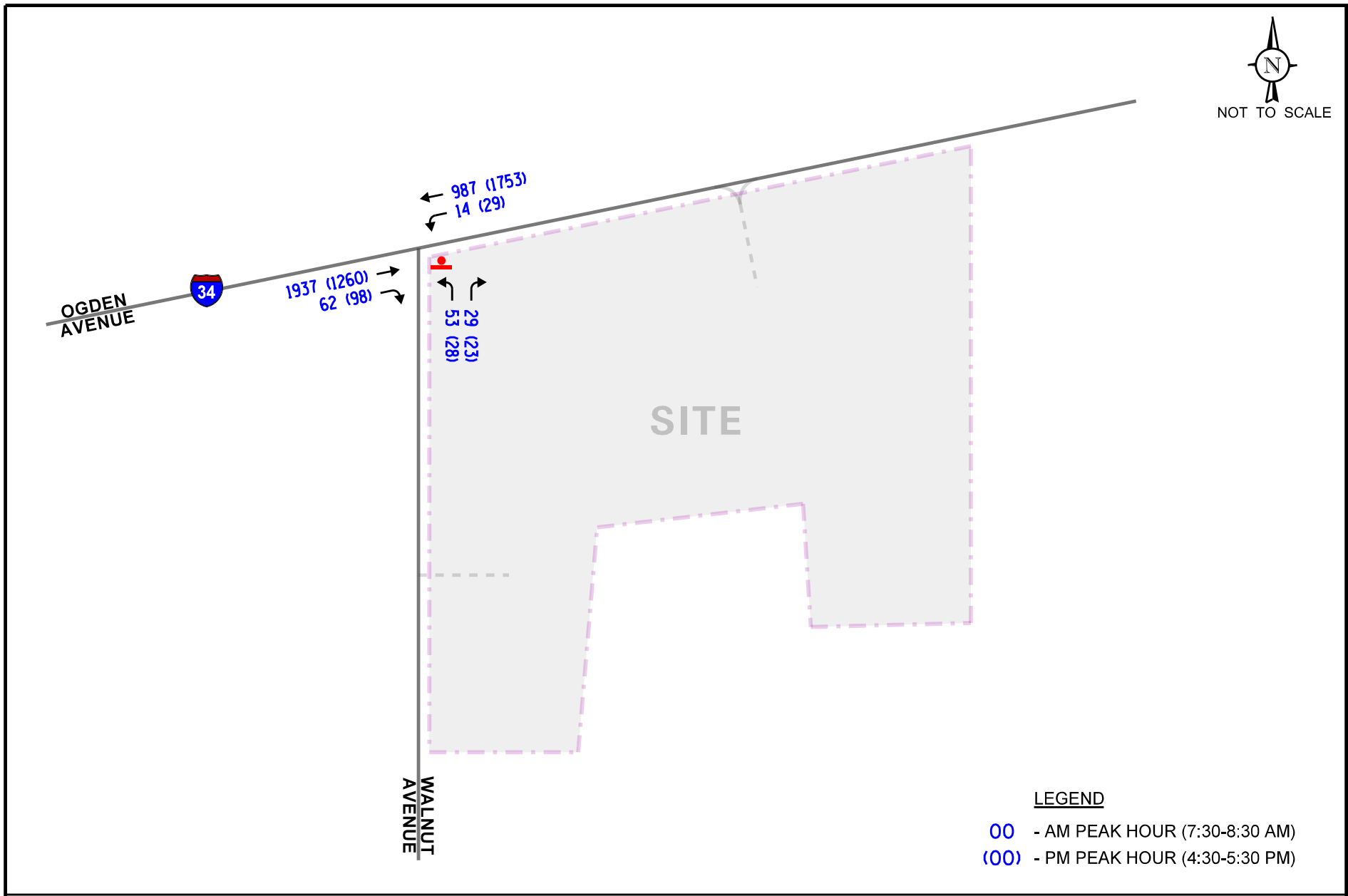
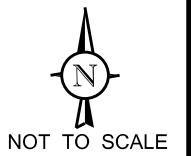
## Crash Analysis

KLOA, Inc. obtained accident data for the most recent available past five years (2014 to 2018) for the intersections of Ogden Avenue with Walnut Avenue. A review of the crash data revealed no fatalities were reported at the intersection during the review period. A summary of the crash data for the intersection is shown in **Table 1**.<sup>1</sup>

Table 1  
OGDEN AVENUE WITH WALNUT AVENUE - CRASH SUMMARY

Year	Type of Accident Frequency						Total
	Angle	Object	Rear End	Sideswipe	Turning	Other	
2014	0	0	2	0	0	0	2
2015	0	0	2	1	2	0	5
2016	0	0	6	1	1	0	8
2017	0	0	5	0	2	0	7
2018	0	1	5	0	4	0	10
<b>Total</b>	<b>0</b>	<b>1</b>	<b>20</b>	<b>2</b>	<b>9</b>	<b>0</b>	<b>32</b>
<b>Average/Year</b>	--	<1.0	<b>4.0</b>	<1.0	<b>1.8</b>	--	<b>6.4</b>

<sup>1</sup> IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel.



Proposed Hotel  
Development  
Downers Grove, Illinois

Year 2020 Base Traffic Volumes

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Job No: 20-198      Figure: 4

### 3. Traffic Characteristics of the Proposed Development

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed development, including the directional distribution and volumes of traffic that it will generate.

#### Proposed Site and Development Plan

As proposed, the plans call for developing the site with a 122-room business hotel and an approximately 4,900 square foot sit-down restaurant. The development will provide a surface parking lot with 184 parking spaces. Access to the development will be provided via the following:

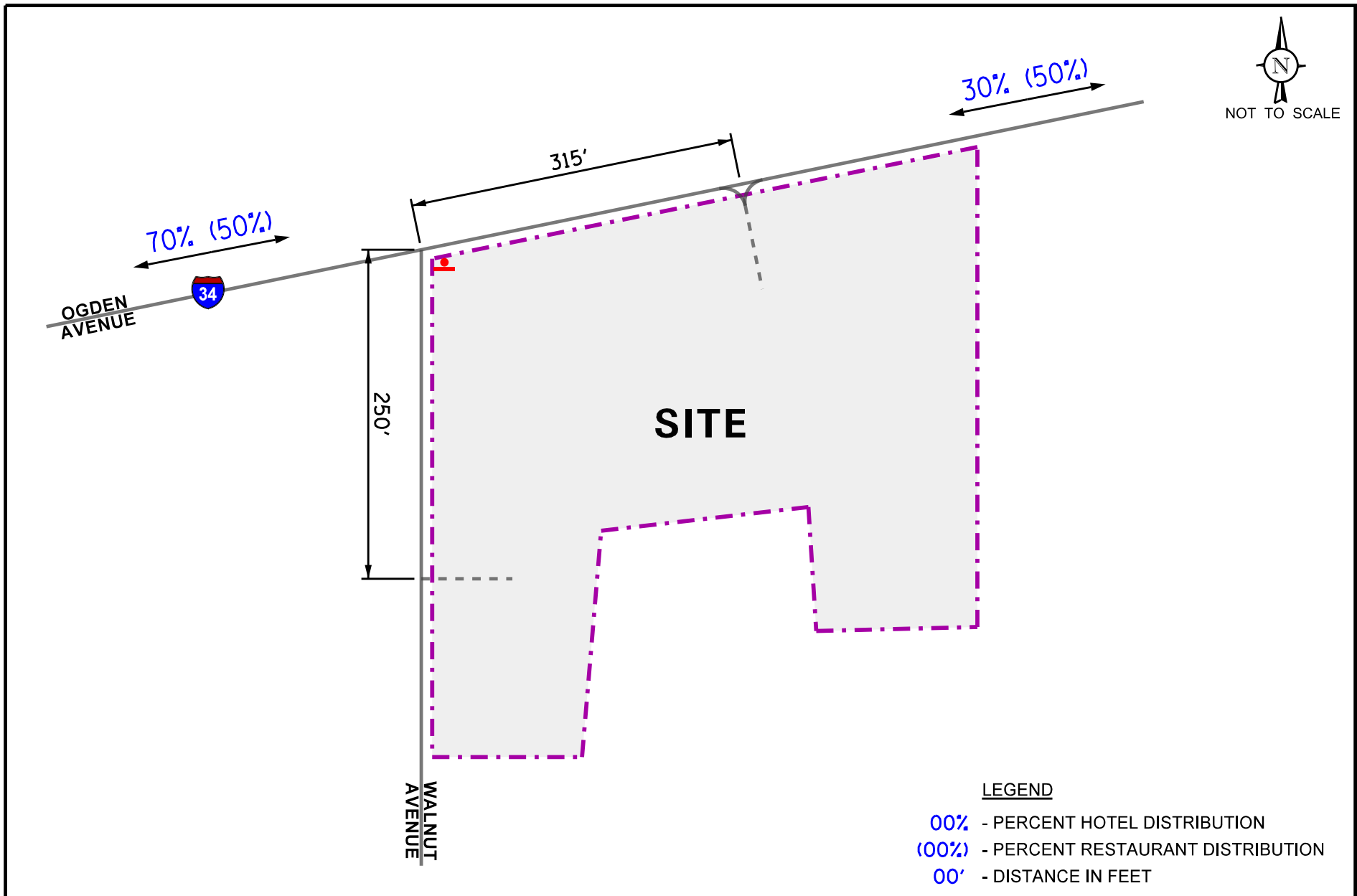
- A right-in/right-out access drive on Ogden Avenue approximately 315 feet east of Walnut Avenue. The access drive will provide one inbound lane and one outbound lane signed and physically restricted to prevent left-turn movements. Outbound movements will be under stop sign control.
- A full movement access drive on Walnut Avenue approximately 250 feet south of Ogden Avenue. The access drive will provide one inbound lane and one outbound lane under stop sign control.

A copy of the preliminary site plan is included in the Appendix.

#### Directional Distribution

The directions from which patrons and employees of the proposed development will approach and depart the site were estimated based on existing travel patterns, as determined from the traffic counts. **Figure 5** illustrates the directional distribution of the site-generated traffic. Figure 5 also shows the distance, in feet, between the existing and proposed access intersections.

It should be noted that all traffic is projected to access the site via Ogden Avenue and the development will not increase the volume of traffic traveling on Walnut Avenue south of the site.



Proposed Hotel  
Development  
Downers Grove, Illinois

Estimated Directional Distribution

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Job No: 20-198      Figure: 5

## Development Traffic Generation

The number of peak hour trips estimated to be generated by the proposed redevelopment was based on vehicle trip generation rates contained in *Trip Generation Manual*, 10<sup>th</sup> Edition, published by the Institute of Transportation Engineers (ITE). The “Business Hotel” (Land-Use Code 312) rate was used for the hotel and the “Quality Restaurant” (Land-Use Code 931) rate was used for the restaurant. Copies of the trip generation worksheets are included in the Appendix. It should be noted that some patrons of the restaurant development may be patrons or employees of the proposed hotel and not new traffic to the roadway system. However, in order to provide a conservative analysis, no interaction reduction was taken. **Table 2** shows the estimated vehicle trip generation for the weekday morning and weekday evening peak hours as well as the weekday daily two-way traffic volumes for the overall development.

Table 2

ESTIMATED VEHICLE TRIP GENERATION FOR PROPOSED DEVELOPMENT

ITE Land Use Code	Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour			Daily Two-Way Trips
		In	Out	Total	In	Out	Total	
310	Business Hotel (122 Rooms)	18	25	43	21	18	39	506
931	Quality Restaurant (4,900 s.f.)	2	2	4	25	13	38	412
<b>Total Development Traffic</b>		<b>20</b>	<b>27</b>	<b>47</b>	<b>46</b>	<b>31</b>	<b>77</b>	<b>918</b>

## 4. Projected Traffic Conditions

The total projected traffic volumes include the existing traffic volumes increased by a regional growth rate and the traffic estimated to be generated by the proposed subject development.

### Development Traffic Assignment

The estimated weekday morning and weekday evening peak hour traffic volumes that will be generated by the proposed development were assigned to the roadway system in accordance with the previously described directional distribution (Figure 5). The traffic assignment for the proposed hotel is illustrated in **Figure 6**. The traffic assignment for the proposed restaurant is illustrated in **Figure 7**.

### Background Traffic Conditions

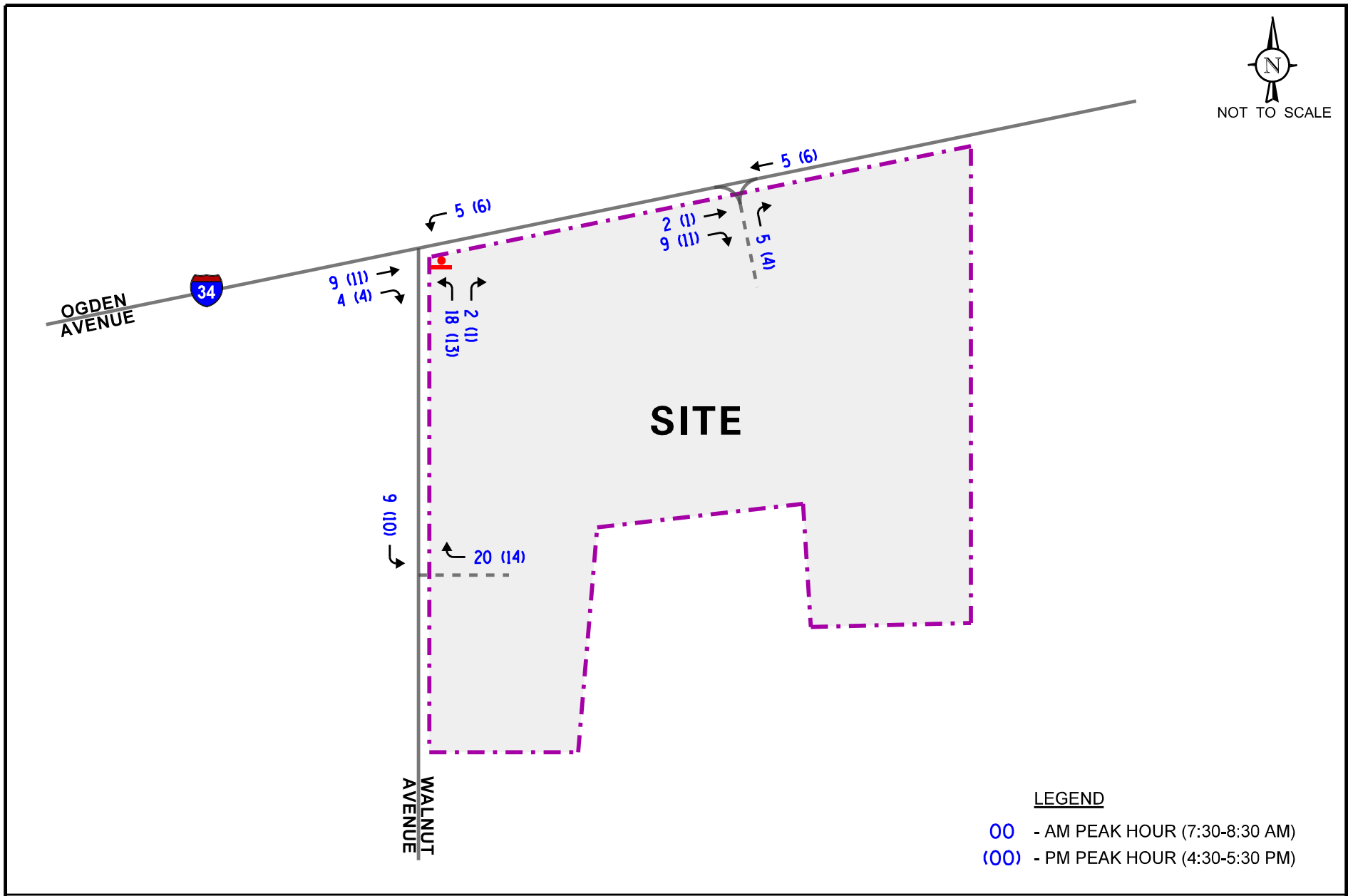
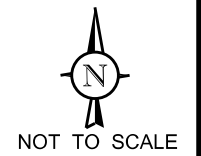
The following additional traffic growth was also included in the projected traffic volumes:

- The traffic projected to be generated by the Walnut Park Fieldhouse that is planned to be built south of the site within Walnut Park. Table A, included in the appendix, summarizes the trips projected to be generated by this development.
- The existing traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on ADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP) in a letter, the existing traffic volumes are projected to increase by a compound annual growth rate of 0.5 percent per year. As such, traffic volumes were increased by approximately 3.0 percent total to represent Year 2026 conditions (one-year buildout plus five years). A copy of the CMAP projections letter is included in the Appendix.

The Year 2026 no-build traffic volumes, which include the existing traffic volumes increased by the regional growth factor and the traffic to be generated by the proposed Walnut Park Fieldhouse, are illustrated in **Figure 8**.

### Year 2026 Total Projected Traffic Conditions

The development-generated traffic was added to the Year 2026 no-build volumes to determine the Year 2026 total projected traffic volumes, as shown in **Figure 9**.

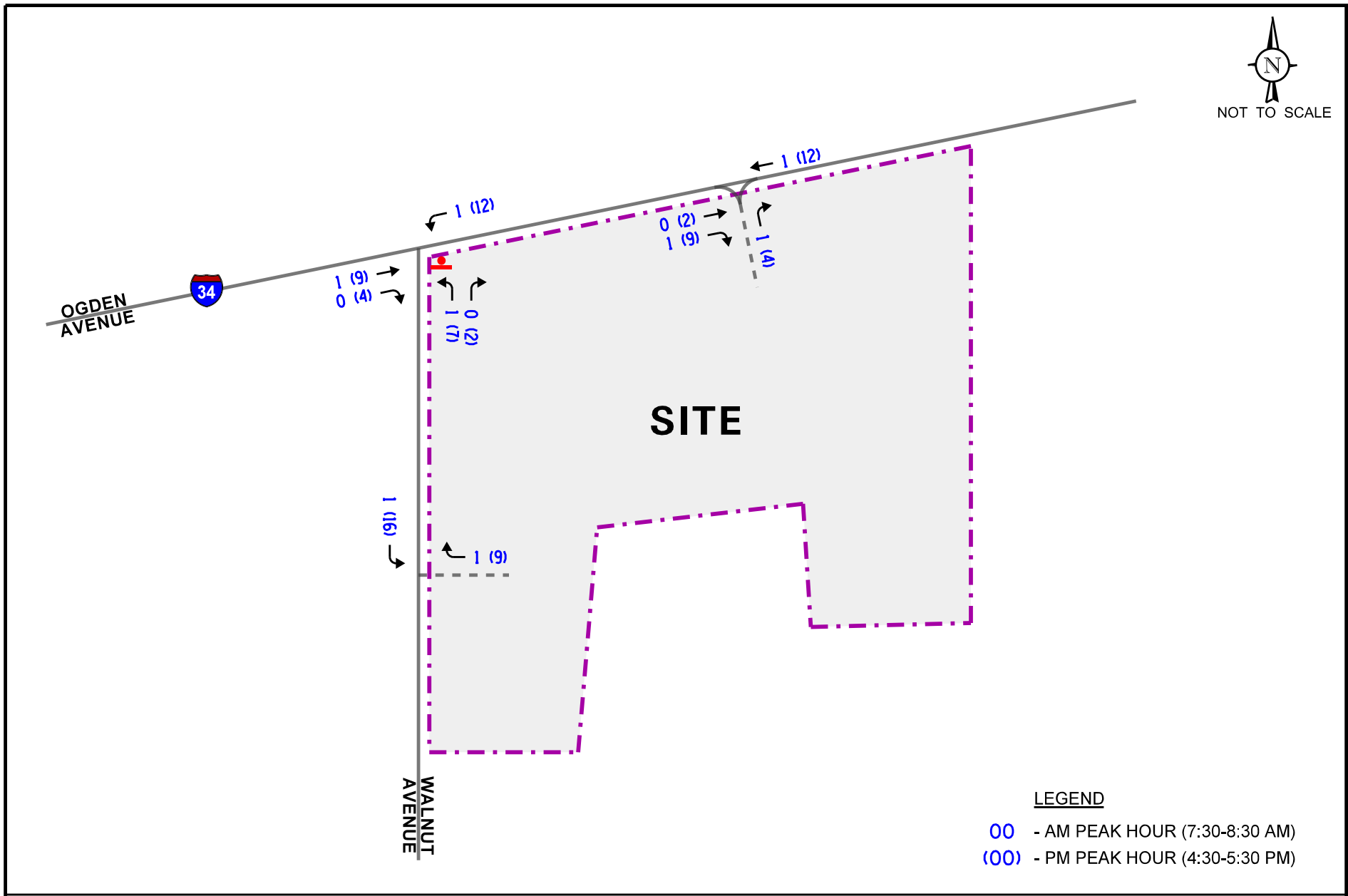
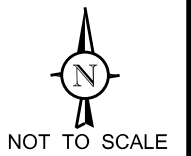


Proposed Hotel  
Development  
Downers Grove, Illinois

Estimated Hotel-Generated Traffic Volumes



Job No: 20-198 Figure: 6

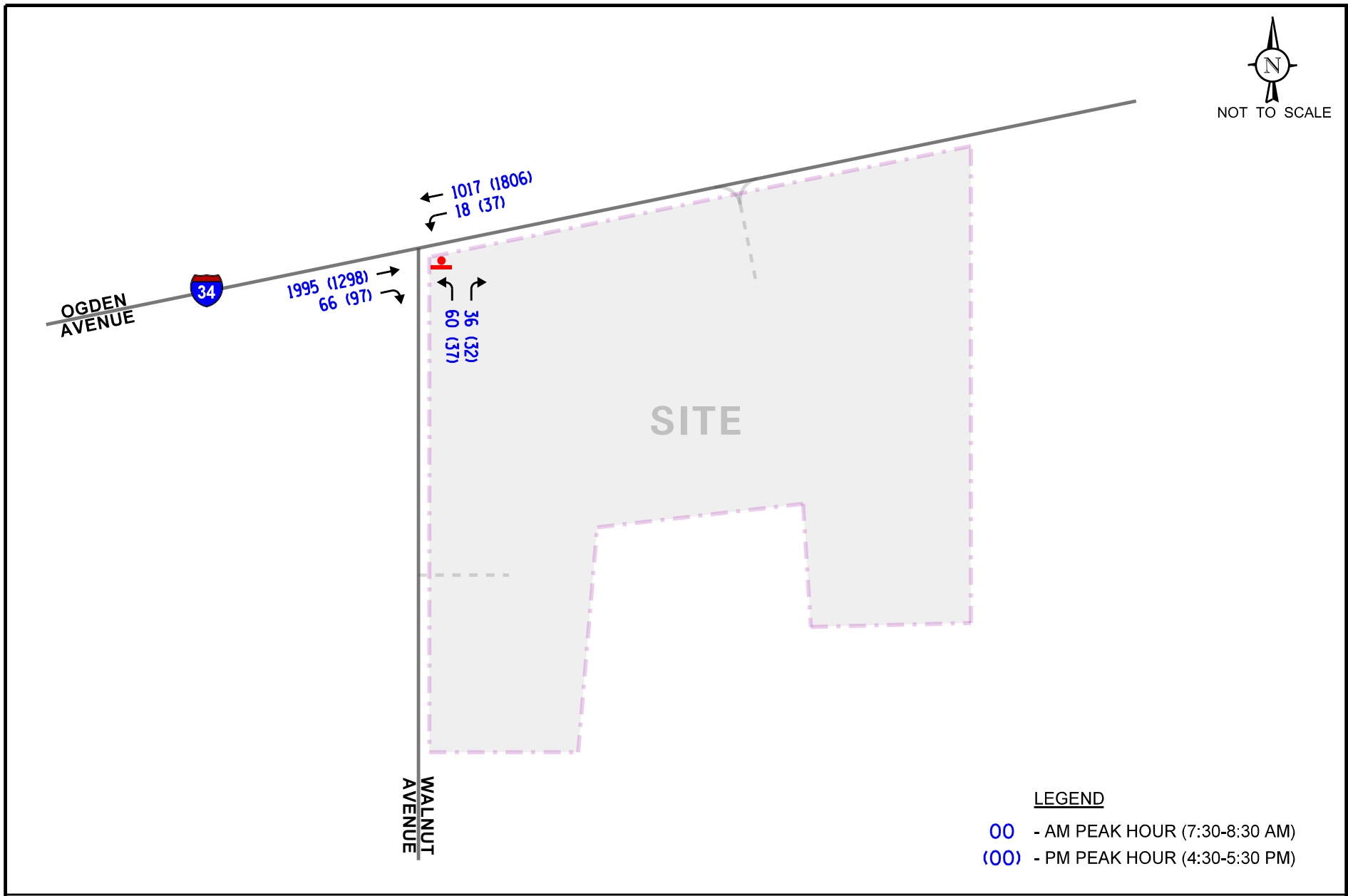
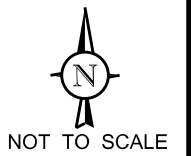


Proposed Hotel  
Development  
Downers Grove, Illinois

Estimated Restaurant-Generated Traffic Volumes

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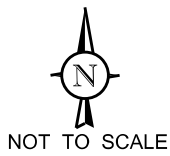
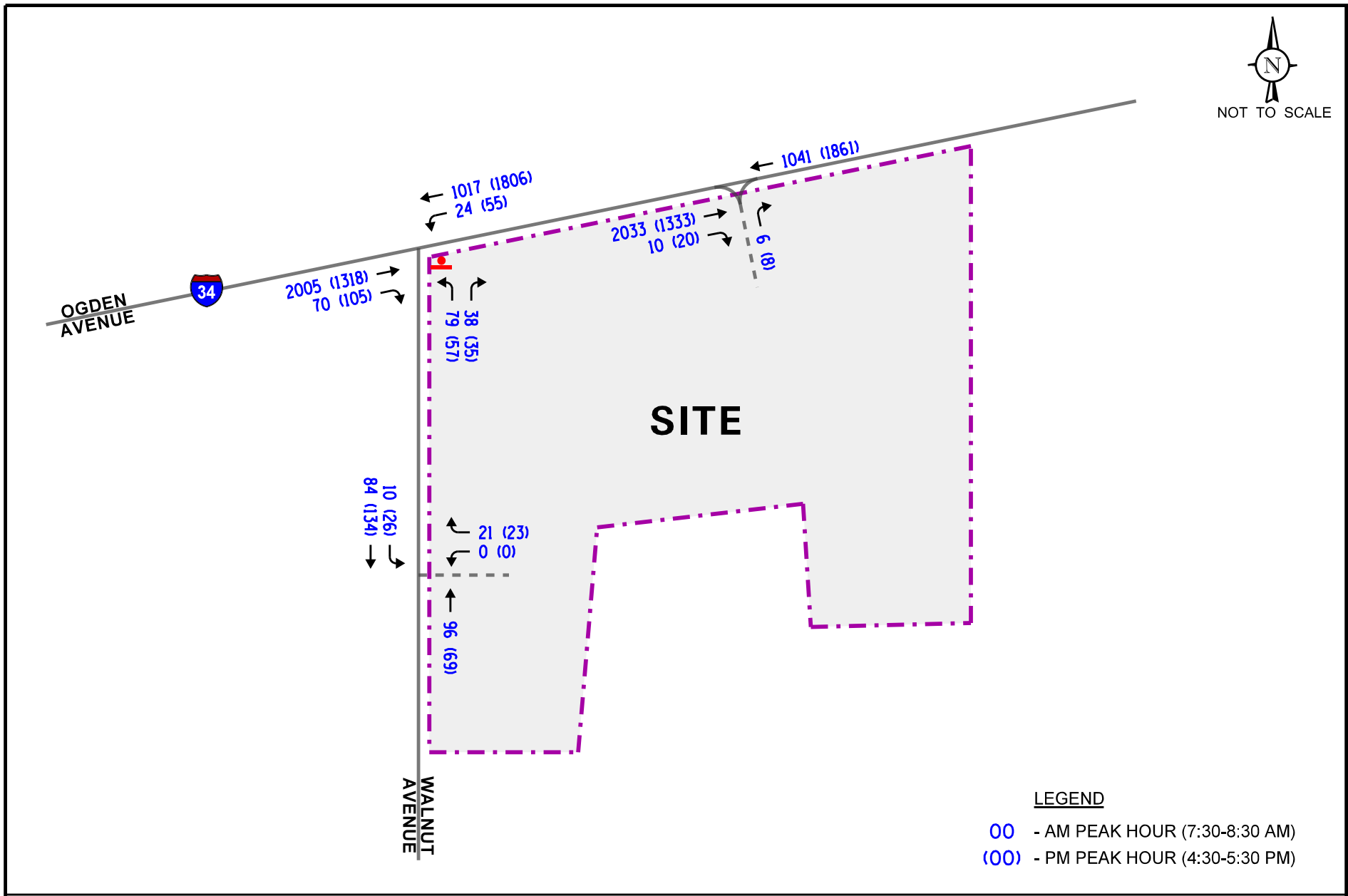


Proposed Hotel  
Development  
Downers Grove, Illinois

Year 2026 No-Build Traffic Volumes

**KLOA**  
Kenig, Lindgren, O'Hara, Aboona, Inc.

Job No: 20-198      Figure: 8



Proposed Hotel  
Development  
Downers Grove, Illinois

Year 2026 Total Projected Traffic Volumes

**KLOA**  
Kenig, Lindgren, O'Hara, Aboona, Inc.

Job No: 20-198      Figure: 9

## 5. Traffic Analysis and Recommendations

Capacity analyses were performed for the key intersections included in the study area to determine the ability of the existing roadway system to accommodate existing and future traffic demands. Analyses were performed for the weekday morning and weekday evening peak hours for the Year 2020 base, Year 2026 no-build, and Year 2026 total projected conditions.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6<sup>th</sup> Edition and analyzed using Synchro/SimTraffic 10 software.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

A summary of the traffic analysis results showing the level of service and delay (measured in seconds) for the intersections for the Year 2020 base, Year 2026 no-build, and Year 2026 total projected conditions are presented in **Tables 3** through **5**. A discussion of each of the intersections follows.

Table 3  
CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS  
YEAR 2020 BASE CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
<b>Ogden Avenue (US Route 34) with Walnut Avenue</b>				
• Westbound Left Turn	C	21.6	B	12.9
• Northbound Approach	F	99+	E	36.4
LOS = Level of Service Delay is measured in seconds.				

Table 4  
CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS  
YEAR 2026 NO-BUILD CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
<b>Ogden Avenue (US Route 34) with Walnut Avenue</b>				
• Westbound Left Turn	C	23.1	B	13.4
• Northbound Approach	F	99+	E	46.7
LOS = Level of Service Delay is measured in seconds.				

Table 5  
CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS  
YEAR 2026 TOTAL PROJECTED CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
<b>Ogden Avenue (US Route 34) with Walnut Avenue<sup>1</sup></b>				
• Westbound Left Turn	C	24.1	B	14.1
• Northbound Approach	F	99.9+	F	55.0
<b>Ogden Avenue (US Route 34) with the Proposed Right-In/Right-Out Access Drive</b>				
• Northbound Approach	C	21.8	B	14.6
<b>Walnut Avenue with the Proposed Full Movement Access Drive</b>				
• Westbound Approach	A	8.8	A	8.7
• Southbound Left Turn	A	7.4	A	7.4
LOS = Level of Service Delay is measured in seconds. 1 – Assumes the restriping of Walnut Avenue to provide an exclusive left-turn lane and an exclusive right-turn lane.				

## Discussion and Recommendations

The following is an evaluation of the analyzed intersections based on the projected traffic volumes and the capacity analyses performed.

### *Ogden Avenue (US Route 34) with Walnut Avenue*

The results of the capacity analysis indicate that the northbound approach currently operates at Level of Service (LOS) F during the weekday morning peak hour and at LOS E during the weekday evening peak hour. This LOS is typical and expected at the unsignalized intersection of a local roadway with an arterial roadway such as Ogden Avenue. Further, this analysis does not take into consideration the signalized intersections of Ogden Avenue with the I-355 Ramps and Ogden Avenue with Cross Street which will create gaps in the traffic stream allowing vehicles to turn onto Ogden Avenue. Eastbound left turn movements onto Walnut Avenue currently operate at LOS C or better during both peak hours with 95<sup>th</sup> percentile queues of one to two vehicles, which can be accommodated within the existing turn lane. Under Year 2026 no-build traffic conditions, all critical movements are projected to continue operate at same LOS.

As part of the development, Walnut Avenue will be restriped to provide two outbound lanes striped for an exclusive left-turn lane and an exclusive right-turn lane. Under Year 2026 total projected traffic conditions and assuming this improvement, the northbound approach is projected to operate at LOS F during the weekday morning peak hour and LOS D during the weekday midday peak hour. As previously mentioned, the LOS F is typical, and vehicles are expected to be able to continue to turn onto Ogden Avenue. Further, left turn movements onto Walnut Avenue are projected to continue operating at LOS C or better with 95<sup>th</sup> percentile queues of one to two vehicles which can be accommodated within the existing turn lane. As such, this intersection has sufficient reserve capacity and no additional geometric or traffic control improvements will be required as part of the development.

### *Ogden Avenue (US Route 34) with the Proposed Right-in/Right-out Access Drive*

As proposed, a right-in/right-out access drive will be provided on Ogden Avenue approximately 315 feet east of Walnut Avenue. The access drive will provide one inbound lane and one outbound lane signed and physically restricted to prevent left-turn movements. Outbound movements will be under stop sign control.

Under Year 2026 total projected traffic conditions, outbound movements from the proposed access drive are projected to operate at LOS C during the weekday morning peak hour and LOS B during the weekday evening peak hour. Further, 95<sup>th</sup> percentile northbound queues will not exceed one to two vehicles thus indicating that vehicles will be able to exit the site efficiently and will not impact internal circulation.

When the projected traffic volumes are compared to the turn lane warrant guidelines published in Chapter 36 of the IDOT *Bureau of Design and Environment* (BDE) Manual, an exclusive right-turn lane serving the proposed access drive will not be warranted during either peak hour. As such, the proposed access drive will be adequate in accommodating the traffic projected to be generated by the proposed development and will provide efficient and flexible access.

### *Walnut Avenue with the Proposed Full Movement Access Drive*

As proposed, a full movement access drive will be provided on Walnut Avenue approximately 250 feet south of Ogden Avenue. The access drive will provide one inbound lane and one outbound lane with outbound movements under stop sign control.

Under Year 2026 total projected traffic conditions, outbound movements from the proposed access drive are projected to operate at LOS A during the weekday morning and evening peak hours. Further, 95<sup>th</sup> percentile westbound queues will not exceed one to two vehicles thus indicating that vehicles will be able to exit the site efficiently and will not impact internal circulation. In addition, southbound left turn movements into the site are projected to operate at LOS A during both peak hours with 95<sup>th</sup> percentile queues of one to two vehicles. As such, the proposed access drive will be adequate in accommodating the traffic projected to be generated by the proposed development and will provide efficient and flexible access.

### Parking Evaluation

In order to assess the adequacy of the provided parking supply to meet the expected parking demand of the proposed development, the peak parking demand was estimated based on rates included in ITE's *Parking Generation Manual*, 5<sup>th</sup> Edition. **Table 6** summarizes the projected average peak parking demand of the development on a weekly basis.

Table 6  
AVERAGE PEAK PARKING DEMAND

ITE Land Use Code	Type/Size	Weekday (Monday-Thursday) Peak Demand	Friday Peak Demand	Saturday Peak Demand
312	Business Hotel (122 Rooms)	88	88	79
931	Quality Restaurant (4,900 s.f.)	52	73	84
<b>Total Parking Required</b>		<b>140</b>	<b>161</b>	<b>163</b>

Based on ITE rates, the proposed development is projected to have a weekly peak parking demand of 163 vehicles occurring on Saturday. As such, the proposed 184-space surface parking lot will be adequate in accommodating the projected parking demand for the proposed development.

## 6. Alternate Plan Evaluation

The developer of the site has indicated that, in addition to the proposed hotel, the site may be developed with a fast-food restaurant with a drive-through lane instead of a sit-down restaurant. The following provides an evaluation of the alternate development plan.

### Alternate Development Plan

The alternate development plans would call for developing the site with a 122-room hotel, similar to the proposed development plan, an approximately 2,350 square-foot fast food restaurant with a drive-through lane, and a 182-space surface parking lot. Access to the development will be the same in both developments plans and will consist of a right-in/right-out access drive on Ogden Avenue and a full movement access drive on Walnut Avenue. A copy of the alternate development site plan is included in the Appendix.

### Alternate Development Traffic Generation

The number of peak hour trips estimated to be generated by the alternate development plan was also based on vehicle trip generation rates contained in ITE's *Trip Generation Manual*, 10<sup>th</sup> Edition. The "Hotel" (Land-Use Code 310) rate was used for the hotel and the "Fast Food Restaurant with Drive-Through Window" (Land-Use Code 934) rate was used for the fast food restaurant. Copies of the trip generation worksheets are included in the Appendix.

It is important to note that surveys conducted by ITE have shown that approximately 50 percent of trips made to fast food restaurants with drive-throughs are diverted from the existing traffic on the roadway system. This is particularly true during the weekday morning and evening peak hours when traffic is diverted from the home-to-work and work-to-home trips (pass-by traffic). As such, the new site traffic to be generated by restaurant was reduced by 50 percent to account for pass-by traffic. Further, as with the proposed development plan, a percentage of the trips expected to be generated by the restaurant may be patrons of the hotel. However, to provide for conservative analysis, the number of new trips was not reduced to account for this interaction. **Table 7** summarizes the trips projected to be generated by the proposed alternate development plane.

### Alternate Total Projected Traffic Conditions

The traffic generated by the alternate development was added to the Year 2026 no-build volumes to determine the Year 2026 total projected traffic volumes – Alternate Development Plan, as shown in **Figure 10**.

### Alternate Traffic Analysis and Recommendations

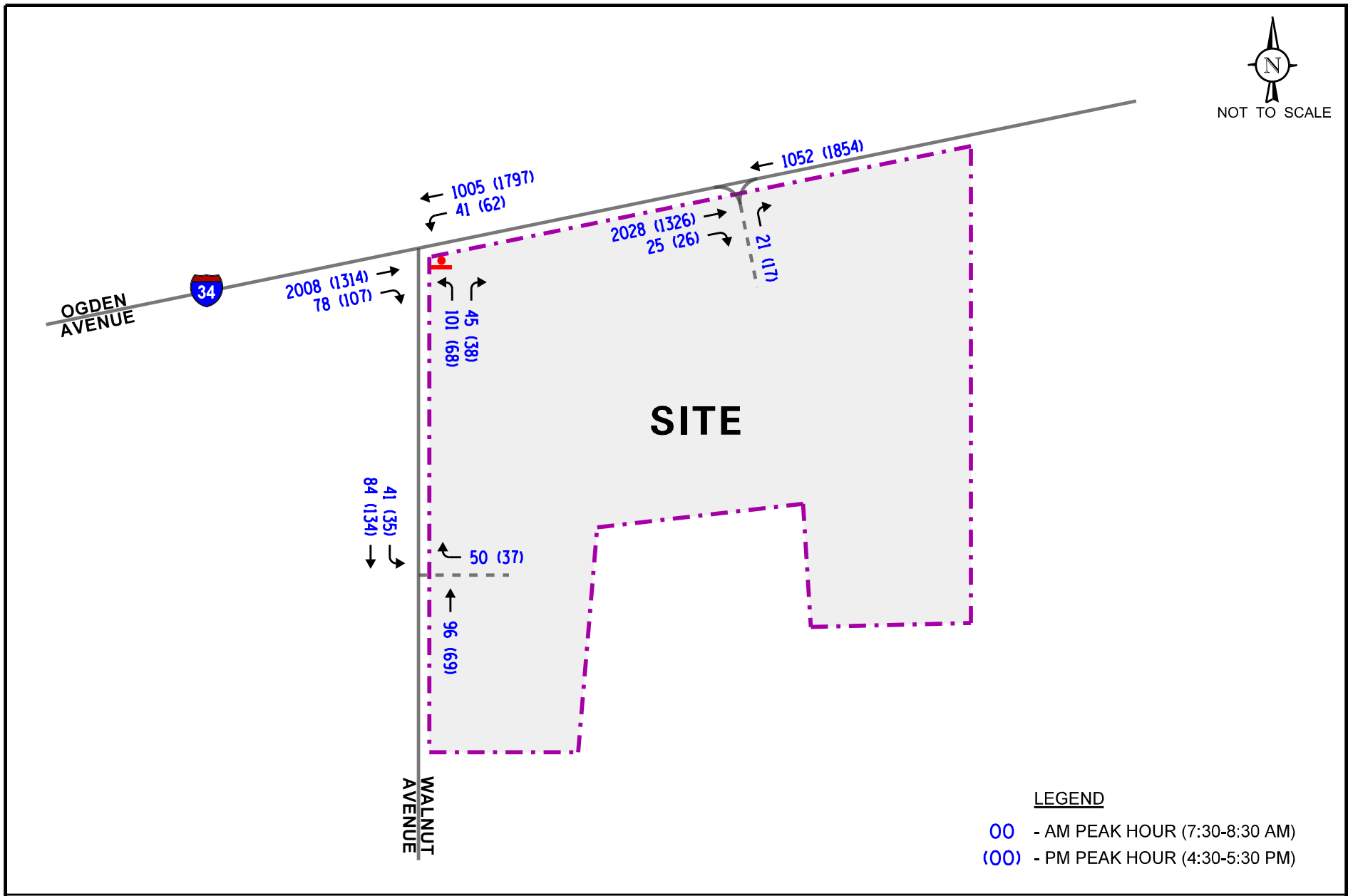
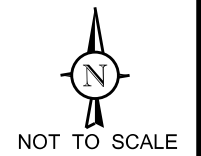
Capacity analyses were performed for the key intersections for the Year 2026 Total Projected – Alternate Development Plan condition in the same way as the other conditions as is discussed above. **Table 8** summarizes the results.

Table 7  
ESTIMATED VEHICLE TRIP GENERATION FOR ALTERNATE DEVELOPMENT PLAN

ITE Land Use Code	Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour			Daily Two-Way Trips
		In	Out	Total	In	Out	Total	
310	Business Hotel (122 Rooms)	33	23	56	33	32	65	950
934	Fast-Food Restaurant with Drive-Through Window (2,350 s.f.)	48	46	94	40	36	76	1,108
	<i>50% Pass-by Reduction</i>	<i>-24</i>	<i>-24</i>	<i>-48</i>	<i>-19</i>	<i>-19</i>	<i>-38</i>	<i>-554</i>
	<b>New Restaurant Traffic</b>	<b>24</b>	<b>22</b>	<b>46</b>	<b>21</b>	<b>17</b>	<b>38</b>	<b>554</b>
	<b>Total New Development Traffic</b>	<b>57</b>	<b>45</b>	<b>102</b>	<b>54</b>	<b>49</b>	<b>103</b>	<b>1,504</b>

Table 8  
CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS  
YEAR 2026 TOTAL PROJECTED CONDITIONS – ALTERNATE DEVELOPMENT PLAN

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
<b>Ogden Avenue (US Route 34) with Walnut Avenue<sup>1</sup></b>				
• Westbound Left Turn	C	19.0	A	9.8
• Northbound Approach	F	85.9	D	31.4
<b>Ogden Avenue (US Route 34) with the Proposed Right-in/Right-out Access Drive</b>				
• Northbound Approach	C	23.0	B	14.8
<b>Walnut Avenue with the Proposed Full Movement Access Drive</b>				
• Westbound Approach	A	8.9	A	8.7
• Southbound Left Turn	A	7.4	A	7.4
LOS = Level of Service Delay is measured in seconds. 1 – Assumes the restriping of Walnut Avenue to provide an exclusive left-turn lane and an exclusive right-turn lane.				



**LEGEND**

- 00 - AM PEAK HOUR (7:30-8:30 AM)
- (00) - PM PEAK HOUR (4:30-5:30 PM)

Proposed Hotel  
Development  
Downers Grove, Illinois

Year 2026 Total Projected Traffic Volumes  
Alternate Plan Evaluation

**KLOA**  
Kenig, Lindgren, O'Hara, Aboona, Inc.  
Job No: 20-198    Figure: 10

The results of the capacity analysis indicate that all movements are projected to operate at a similar LOS to the Year 2026 projected conditions for the proposed development plan. Further, all movements are projected to operate at an acceptable LOS with the exception of the northbound approach at the intersection of Ogden Avenue with Walnut Avenue, which will operate at LOS F during the weekday morning peak hour. However, as previously discussed, this is typical and vehicles are expected to continue to be able to turn onto Ogden Avenue.

When the projected traffic volumes at the intersection of Ogden Avenue with the proposed right-in/right-out access drive are compared to the turn lane warrant guidelines published in Chapter 36 of the IDOT BDE Manual, an exclusive right-turn lane serving the proposed access drive will not be warranted during either peak hour.

As such, the proposed access system will be adequate in accommodating the traffic projected to be generated by the alternate development plan and will provide efficient and flexible access.

### Drive-Through and On-Site Circulation

As proposed, the fast-food restaurant would provide a drive-through lane with dual ordering boards. The pick-up window will be located on the east side of the building with the dual order boards located on the south side of the building. Vehicles will enter the drive-through from the on the south side of the building and travel around the south and east sides of the building. A review of the preliminary site plan indicates that approximately six vehicles can queue between the pick-up window and the order boards and approximately six additional vehicles (three per order board) can stack from the order boards before reaching internal drive aisle. As such, the proposed drive-through design provides stacking for a minimum of 12 vehicles which will be adequate to accommodate the peak demand of the drive-through operation. Further, approximately three vehicles can queue within the site before reaching the Walnut Avenue access drive and numerous additional vehicles between the drive-through lane and the Ogden Avenue access drive. In order to provide efficient and orderly internal traffic flow, wayfinding signs directing traffic to the drive-through lane should be provided throughout the site.

### Alternate Plan Parking Evaluation

In order to assess the adequacy of the provided parking supply to meet the expected parking demand of the proposed alternate development plan, the peak parking demand was estimated based on rates included in ITE's *Parking Generation Manual*, 5<sup>th</sup> Edition. **Table 9** summarizes the projected average peak parking demand of the development on a weekly basis.

Based on ITE rates, the proposed alternate development plan is projected to have a weekly peak parking demand of 118 vehicles occurring on Friday. As such, the proposed 182-space surface parking lot will be adequate in accommodating the projected parking demand for the proposed development.

Table 9  
AVERAGE PEAK PARKING DEMAND

ITE Land Use Code	Type/Size	Weekday (Monday-Thursday) Peak Demand	Friday Peak Demand	Saturday Peak Demand
310	Business Hotel (122 Rooms)	88	88	79
934	Fast-Food Restaurant with Drive Through Window (2,350 s.f.)	21	30	22
<b>Total Parking Required</b>		<b>109</b>	<b>118</b>	<b>101</b>

## 7. Conclusion

Based on the preceding analyses and recommendations, the following conclusions have been made:

- The traffic that will be generated by the proposed development can be accommodated by the area roadway system.
- The proposed access system will be adequate in accommodating the traffic projected to be generated by the proposed development and will provide efficient and flexible access.
- The site may alternatively be developed with a fast-food restaurant as opposed to a sit-down restaurant. This alternate plan can also be accommodated by the proposed access system.
- As part of the development under both alternatives, Walnut Avenue will be restriped to provide one inbound lane and two outbound lanes striped for an exclusive left-turn lane and an exclusive right-turn lane.
- The proposed surface parking lot will be adequate in accommodating the projected parking demand for the proposed development or for the alternate development plan.

# Appendix

Traffic Count Summary Sheets  
Preliminary Site Plan  
ITE Trip Generation Worksheets  
Walnut Park Development Plan  
CMAP 2050 Projections Letter  
Level of Service Criteria  
Capacity Analysis Summary Sheets  
Alternate Development Plan Site Plan  
Alternate Development Plan Capacity Analysis Summary Sheets

# Traffic Count Summary Sheets



Kenig Lindgren O'Hara Aboona, Inc.  
 9575 W. Higgins Rd., Suite 400  
 Rosemont, Illinois, United States 60018  
 (847)518-9990

Count Name: Ogden Avenue with Walnut Avenue  
 Site Code:  
 Start Date: 09/22/2020  
 Page No: 1

### Turning Movement Data

Start Time	Ogden Avenue Eastbound					Ogden Avenue Westbound					Walnut Avenue Northbound					Int. Total
	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
7:00 AM	1	270	10	0	281	0	1	140	0	141	1	6	6	0	13	435
7:15 AM	0	264	10	0	274	0	1	170	0	171	1	11	2	0	14	459
7:30 AM	0	359	13	0	372	0	2	182	0	184	0	15	6	0	21	577
7:45 AM	0	355	16	0	371	0	3	153	0	156	0	6	3	1	9	536
Hourly Total	1	1248	49	0	1298	0	7	645	0	652	2	38	17	1	57	2007
8:00 AM	0	271	6	0	277	0	3	175	0	178	0	5	5	0	10	465
8:15 AM	0	306	6	0	312	0	1	148	0	149	0	9	5	0	14	475
8:30 AM	0	301	6	0	307	0	2	165	0	167	0	4	3	0	7	481
8:45 AM	1	308	6	0	315	0	1	189	0	190	0	1	4	0	5	510
Hourly Total	1	1186	24	0	1211	0	7	677	0	684	0	19	17	0	36	1931
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	0	233	9	0	242	0	2	367	0	369	0	7	8	0	15	626
4:15 PM	0	303	9	0	312	1	5	333	0	339	0	4	2	0	6	657
4:30 PM	0	230	13	0	243	1	2	343	0	346	0	7	6	1	13	602
4:45 PM	0	250	19	0	269	0	5	340	0	345	0	4	2	0	6	620
Hourly Total	0	1016	50	0	1066	2	14	1383	0	1399	0	22	18	1	40	2505
5:00 PM	0	256	16	0	272	0	6	362	0	368	0	8	4	0	12	652
5:15 PM	0	258	22	0	280	0	7	372	0	379	1	4	6	1	11	670
5:30 PM	1	244	14	0	259	0	5	328	0	333	0	6	6	0	12	604
5:45 PM	0	229	14	0	243	2	2	264	0	268	0	5	2	0	7	518
Hourly Total	1	987	66	0	1054	2	20	1326	0	1348	1	23	18	1	42	2444
Grand Total	3	4437	189	0	4629	4	48	4031	0	4083	3	102	70	3	175	8887
Approach %	0.1	95.9	4.1	-	-	0.1	1.2	98.7	-	-	1.7	58.3	40.0	-	-	-
Total %	0.0	49.9	2.1	-	52.1	0.0	0.5	45.4	-	45.9	0.0	1.1	0.8	-	2.0	-
Lights	2	4307	183	-	4492	4	47	3916	-	3967	2	100	69	-	171	8630
% Lights	66.7	97.1	96.8	-	97.0	100.0	97.9	97.1	-	97.2	66.7	98.0	98.6	-	97.7	97.1
Buses	1	13	0	-	14	0	0	11	-	11	0	0	0	-	0	25
% Buses	33.3	0.3	0.0	-	0.3	0.0	0.0	0.3	-	0.3	0.0	0.0	0.0	-	0.0	0.3
Single-Unit Trucks	0	64	4	-	68	0	0	61	-	61	0	1	1	-	2	131
% Single-Unit Trucks	0.0	1.4	2.1	-	1.5	0.0	0.0	1.5	-	1.5	0.0	1.0	1.4	-	1.1	1.5
Articulated Trucks	0	52	1	-	53	0	0	43	-	43	0	1	0	-	1	97
% Articulated Trucks	0.0	1.2	0.5	-	1.1	0.0	0.0	1.1	-	1.1	0.0	1.0	0.0	-	0.6	1.1
Bicycles on Road	0	1	1	-	2	0	1	0	-	1	1	0	0	-	1	4
% Bicycles on Road	0.0	0.0	0.5	-	0.0	0.0	2.1	0.0	-	0.0	33.3	0.0	0.0	-	0.6	0.0
Pedestrians	-	-	-	0	-	-	-	-	0	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



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Rosemont, Illinois, United States 60018  
(847)518-9990

Count Name: Ogden Avenue with Walnut Avenue  
Site Code:  
Start Date: 09/22/2020  
Page No: 2

### Turning Movement Peak Hour Data (7:30 AM)

Start Time	Ogden Avenue Eastbound					Ogden Avenue Westbound					Walnut Avenue Northbound					Int. Total
	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
7:30 AM	0	359	13	0	372	0	2	182	0	184	0	15	6	0	21	577
7:45 AM	0	355	16	0	371	0	3	153	0	156	0	6	3	1	9	536
8:00 AM	0	271	6	0	277	0	3	175	0	178	0	5	5	0	10	465
8:15 AM	0	306	6	0	312	0	1	148	0	149	0	9	5	0	14	475
Total	0	1291	41	0	1332	0	9	658	0	667	0	35	19	1	54	2053
Approach %	0.0	96.9	3.1	-	-	0.0	1.3	98.7	-	-	0.0	64.8	35.2	-	-	-
Total %	0.0	62.9	2.0	-	64.9	0.0	0.4	32.1	-	32.5	0.0	1.7	0.9	-	2.6	-
PHF	0.000	0.899	0.641	-	0.895	0.000	0.750	0.904	-	0.906	0.000	0.583	0.792	-	0.643	0.890
Lights	0	1242	41	-	1283	0	9	633	-	642	0	34	19	-	53	1978
% Lights	-	96.2	100.0	-	96.3	-	100.0	96.2	-	96.3	-	97.1	100.0	-	98.1	96.3
Buses	0	4	0	-	4	0	0	4	-	4	0	0	0	-	0	8
% Buses	-	0.3	0.0	-	0.3	-	0.0	0.6	-	0.6	-	0.0	0.0	-	0.0	0.4
Single-Unit Trucks	0	21	0	-	21	0	0	17	-	17	0	0	0	-	0	38
% Single-Unit Trucks	-	1.6	0.0	-	1.6	-	0.0	2.6	-	2.5	-	0.0	0.0	-	0.0	1.9
Articulated Trucks	0	24	0	-	24	0	0	4	-	4	0	1	0	-	1	29
% Articulated Trucks	-	1.9	0.0	-	1.8	-	0.0	0.6	-	0.6	-	2.9	0.0	-	1.9	1.4
Bicycles on Road	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	0	-	-	-	-	0	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



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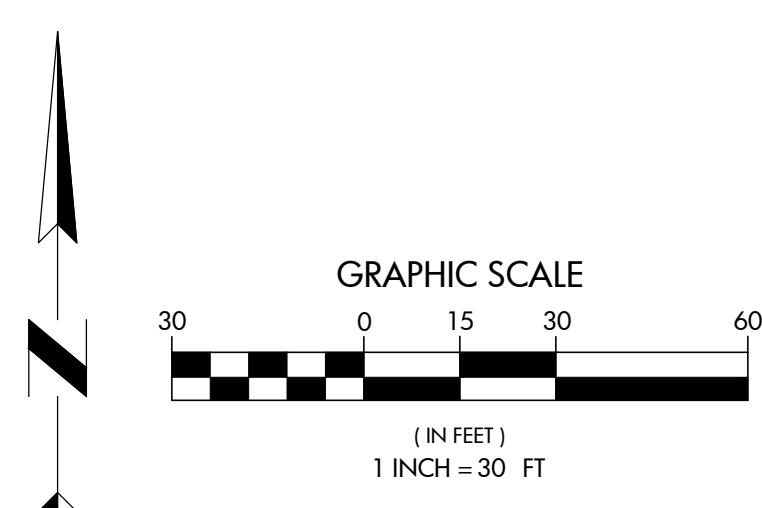
Rosemont, Illinois, United States 60018  
(847)518-9990

Count Name: Ogden Avenue with Walnut Avenue  
Site Code:  
Start Date: 09/22/2020  
Page No: 3

### Turning Movement Peak Hour Data (4:45 PM)

Start Time	Ogden Avenue Eastbound					Ogden Avenue Westbound					Walnut Avenue Northbound					Int. Total
	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
4:45 PM	0	250	19	0	269	0	5	340	0	345	0	4	2	0	6	620
5:00 PM	0	256	16	0	272	0	6	362	0	368	0	8	4	0	12	652
5:15 PM	0	258	22	0	280	0	7	372	0	379	1	4	6	1	11	670
5:30 PM	1	244	14	0	259	0	5	328	0	333	0	6	6	0	12	604
Total	1	1008	71	0	1080	0	23	1402	0	1425	1	22	18	1	41	2546
Approach %	0.1	93.3	6.6	-	-	0.0	1.6	98.4	-	-	2.4	53.7	43.9	-	-	-
Total %	0.0	39.6	2.8	-	42.4	0.0	0.9	55.1	-	56.0	0.0	0.9	0.7	-	1.6	-
PHF	0.250	0.977	0.807	-	0.964	0.000	0.821	0.942	-	0.940	0.250	0.688	0.750	-	0.854	0.950
Lights	1	992	68	-	1061	0	23	1382	-	1405	1	21	18	-	40	2506
% Lights	100.0	98.4	95.8	-	98.2	-	100.0	98.6	-	98.6	100.0	95.5	100.0	-	97.6	98.4
Buses	0	2	0	-	2	0	0	2	-	2	0	0	0	-	0	4
% Buses	0.0	0.2	0.0	-	0.2	-	0.0	0.1	-	0.1	0.0	0.0	0.0	-	0.0	0.2
Single-Unit Trucks	0	9	1	-	10	0	0	14	-	14	0	1	0	-	1	25
% Single-Unit Trucks	0.0	0.9	1.4	-	0.9	-	0.0	1.0	-	1.0	0.0	4.5	0.0	-	2.4	1.0
Articulated Trucks	0	5	1	-	6	0	0	4	-	4	0	0	0	-	0	10
% Articulated Trucks	0.0	0.5	1.4	-	0.6	-	0.0	0.3	-	0.3	0.0	0.0	0.0	-	0.0	0.4
Bicycles on Road	0	0	1	-	1	0	0	0	-	0	0	0	0	-	0	1
% Bicycles on Road	0.0	0.0	1.4	-	0.1	-	0.0	0.0	-	0.0	0.0	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	0	-	-	-	-	0	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-

# Preliminary Site Plan



SHARED PARKING SPACE CALCULATIONS						
LAND USE (REQUIRED)	WEEKDAY			WEEKEND		
	MIDNIGHT-7:00 AM	7:00 AM-6:00 PM	6:00 PM-MIDNIGHT	MIDNIGHT-7:00 AM	7:00 AM-6:00 PM	6:00 PM-MIDNIGHT
LODGING (153)	100% = 153	60% = 92	90% = 138	100% = 153	65% = 99	80% = 122
EATING AND DRINKING (49)	50% = 25	70% = 34	100% = 49	45% = 22	70% = 34	100% = 49
<b>TOTAL REQUIRED</b>	<b>175</b>	<b>126</b>	<b>187</b>	<b>175</b>	<b>133</b>	<b>171</b>
TOTAL PARKING PROVIDED = 184 SPACES. THEREFORE THE OVERALL SITE WILL BE 3 SPACES SHORT DURING THE PEAK PARKING TIME						

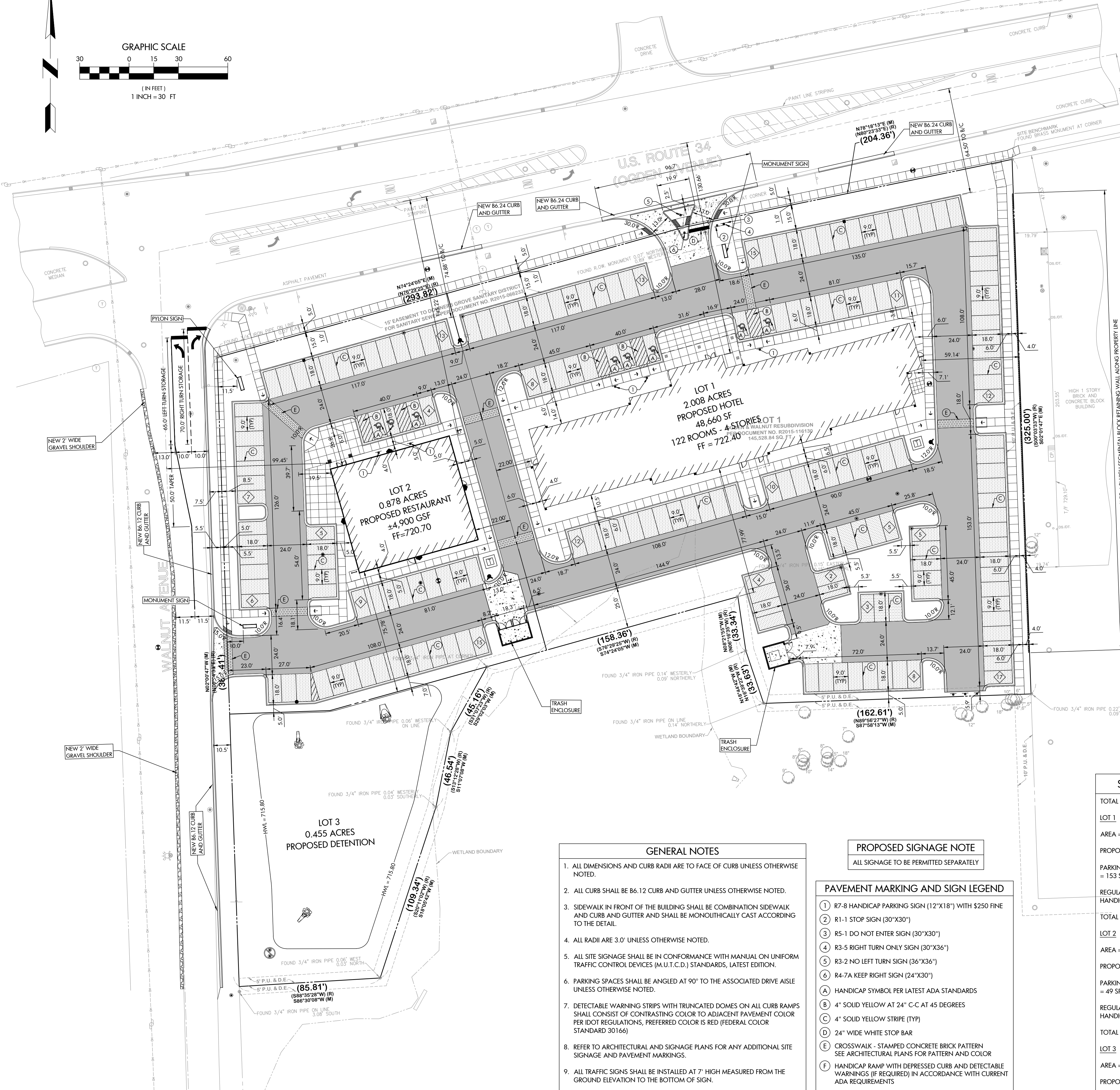
ZONING ANALYSIS	
PROJECT NAME	PROPOSED WOODSPRING SUITES HOTEL
ADDRESS	2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515
PIN(s)	08-01-304-031
ZONING DISTRICT	EXISTING: O-R-M ; OFFICE - RESEARCH - MANUFACTURING PROPOSED: B-3 ; GENERAL SERVICES AND HIGHWAY BUSINESS
MINIMUM DISTRICT AREA	MIN. 4 ACRES ; PARCEL AREA = 4.31 ACRES
EXISTING USE	VACANT LAND
PROPOSED USE	WOODSPRING SUITES HOTEL AND FUTURE SIT DOWN RESTAURANT
PETITION TYPE	P.U.D.
DEVIATIONS	LOT 1 : 1. REDUCED NUMBER OF ON-SITE PARKING SPACES LOT 2 : 1. NONE OVERALL DEVELOPMENT : 1. REDUCED NUMBER OF SHARED PARKING SPACES

LOT 1 - PROPOSED WOODSPRING SUITES HOTEL					
REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	87,465 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	47 FEET	YES	-13 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.56	YES	-0.19
BLDG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	130.66 FEET	YES	+55.66 FEET
BLDG. SIDE SETBACK (EAST)	MINIMUM	0.0 FEET	59.14 FEET	YES	+59.14 FEET
BLDG. SIDE SETBACK (WEST, INTERIOR)	MINIMUM	0.0 FEET	22.00 FEET	YES	+22.00 FEET
BLDG. SETBACK (SOUTH)	MINIMUM	10.0 FEET	77.99 FEET	YES	+67.99 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	64.50 FEET TO B/C	YES	+14.50 FEET
PARKING SETBACK (EAST)	MINIMUM	5.0 FEET	10.00 FEET	YES	+5.00 FEET
PARKING SETBACK (WEST, INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (SOUTH)	MINIMUM	5.0 FEET	5.0 FEET	YES	0.0 FEET
TOTAL PARKING REQUIRED	MINIMUM	1,25/FROOM (153)	125	NO	-28 SPACES
TWO-WAY DRIVE AISLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
OPEN SPACE	MINIMUM	8,747 S.F.	19,165 S.F.	YES	+10,418 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	4,373 S.F.	4,973 S.F.	YES	+600 S.F.

LOT 2 - FUTURE SIT DOWN RESTAURANT (USER TO BE DETERMINED)					
REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	38,304 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	25 FEET (PARAPEL)	YES	-35 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.06	YES	-0.69
BLDG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	126.92 FEET	YES	+51.92 FEET
BLDG. SIDE SETBACK (EAST - INTERIOR)	MINIMUM	10.0 FEET	49.53 FEET	YES	+39.53 FEET
BLDG. SIDE SETBACK (WEST - WALNUT)	MINIMUM	35.0 FEET	80.53 FEET	YES	+45.53 FEET
BLDG. SETBACK (SOUTH)	MINIMUM	10.0 FEET	80.23 FEET	YES	+70.23 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	74.88 FEET TO B/C	YES	+24.88 FEET
PARKING SETBACK (EAST - INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (WEST - WALNUT)	MINIMUM	8.0 FEET	8.5 FEET	YES	0.5 FEET
PARKING SETBACK (SOUTH)	MINIMUM	5.0 FEET	5.0 FEET	YES	0.0 FEET
TOTAL PARKING REQUIRED	MINIMUM	10/1K SF (49)	59	YES	+10 SPACES
TWO-WAY DRIVE AISLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
BICYCLE PARKING	MINIMUM	2 SPACES	4 SPACES	YES	+2 SPACES
OPEN SPACE	MINIMUM	3,830 S.F.	6,766 S.F.	YES	+2,936 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	1,915 S.F.	3,378 S.F.	YES	+1,463 S.F.

SITE DEVELOPMENT SUMMARY	
TOTAL SITE AREA = 3.341± ACRES	
<b>LOT 1</b>	
AREA = 2.008 ACRES	
PROPOSED HOTEL = 48,660 GSF = 122 ROOMS	
PARKING REQUIRED = 1.25 SPACES / ROOM = 153 SPACES	
REGULAR PARKING PROVIDED = 120 SPACES HANDICAP PARKING PROVIDED = 5 SPACES	
<b>TOTAL PARKING PROVIDED = 125 SPACES</b>	
<b>LOT 2</b>	
AREA = 0.878 ACRES	
PROPOSED RESTAURANT = 4,900 GSF	
PARKING REQUIRED = 10 SPACES / 1,000 SF = 49 SPACES	
REGULAR PARKING PROVIDED = 56 SPACES HANDICAP PARKING PROVIDED = 3 SPACES	
<b>TOTAL PARKING PROVIDED = 59 SPACES</b>	
<b>LOT 3</b>	
AREA = 0.455 ACRES	
PROPOSED WETLAND BOTTOM DETENTION BASIN	

SITE PAVING LEGEND	
	HEAVY DUTY ASPHALT PAVEMENT 2.0" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, N50 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, IL-19, N50 8" AGGREGATE BASE COURSE, TYPE B (IDOT GRAD CA-6)
	LIGHT DUTY ASPHALT PAVEMENT 2.0" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, N50 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, IL-19, N50 10" AGGREGATE BASE COURSE, TYPE B (IDOT GRAD CA-6)
	CONCRETE PAVEMENT 8" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	STAMPED CONCRETE PAVEMENT 8" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	SIDEWALK PAVEMENT 5" P.C. CONCRETE PAVEMENT (THICKEN TO 8" AT DRIVEWAYS) 4" AGGREGATE BASE (CA-6)



- GENERAL NOTES**
- ALL DIMENSIONS AND CURB RADII ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - ALL CURB SHALL BE 86.12 CURB AND GUTTER UNLESS OTHERWISE NOTED.
  - SIDEWALK IN FRONT OF THE BUILDING SHALL BE COMBINATION SIDEWALK AND CURB AND GUTTER AND SHALL BE MONOLITHICALLY CAST ACCORDING TO THE DETAIL.
  - ALL RADII ARE 3.0' UNLESS OTHERWISE NOTED.
  - ALL SITE SIGNAGE SHALL BE IN CONFORMANCE WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) STANDARDS, LATEST EDITION.
  - PARKING SPACES SHALL BE ANGLED AT 90° TO THE ASSOCIATED DRIVE AISLE UNLESS OTHERWISE NOTED.
  - DETECTABLE WARNING STRIPS WITH TRUNCATED DOMES ON ALL CURB RAMPS SHALL CONSIST OF CONTRASTING COLOR TO ADJACENT PAVEMENT COLOR PER IDOT REGULATIONS, PREFERRED COLOR IS RED (FEDERAL COLOR STANDARD 30166)
  - REFER TO ARCHITECTURAL AND SIGNAGE PLANS FOR ANY ADDITIONAL SITE SIGNAGE AND PAVEMENT MARKINGS.
  - ALL TRAFFIC SIGNS SHALL BE INSTALLED AT 7' HIGH MEASURED FROM THE GROUND ELEVATION TO THE BOTTOM OF SIGN.

- PROPOSED SIGNAGE NOTE**  
ALL SIGNAGE TO BE PERMITTED SEPARATELY
- PAVEMENT MARKING AND SIGN LEGEND**
- (1) R7-8 HANDICAP PARKING SIGN (12"X18") WITH \$250 FINE
  - (2) R1-1 STOP SIGN (30"X30")
  - (3) R5-1 DO NOT ENTER SIGN (30"X30")
  - (4) R3-5 RIGHT TURN ONLY SIGN (30"X36")
  - (5) R3-2 NO LEFT TURN SIGN (36"X36")
  - (6) R4-7A KEEP RIGHT SIGN (24"X30")
  - (A) HANDICAP SYMBOL PER LATEST ADA STANDARDS
  - (B) 4" SOLID YELLOW AT 24" C-C AT 45 DEGREES
  - (C) 4" SOLID YELLOW STRIPE (TYP)
  - (D) 24" WIDE WHITE STOP BAR
  - (E) CROSSWALK - STAMPED CONCRETE BRICK PATTERN  
SEE ARCHITECTURAL PLANS FOR PATTERN AND COLOR
  - (F) HANDICAP RAMP WITH DEPRESSED CURB AND DETECTABLE WARNINGS (IF REQUIRED) IN ACCORDANCE WITH CURRENT ADA REQUIREMENTS

REVISIONS

1	08-30-21	REVISED PER VILLAGE REVIEW
2	07-02-21	REVISED PER VILLAGE REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

**CIVWORKS**  
Consulting, LLC

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www.civworks.com

**SITE DIMENSIONAL AND PAVING PLAN**  
**HOTEL AND SIT DOWN RESTAURANT**

**PROPOSED WOODSPRING SUITES HOTEL**  
**2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515**

SHEET TITLE: PROJECT: SHEET NO. **C3.0**

PROJ. MGR.: OP  
DRAWN BY: OP  
FIRST ISSUE DATE: 01-04-2021  
SCALE: 1"=30'

PROJ. NUMBER: 19006

# ITE Trip Generation Worksheets

## Land Use: 312 Business Hotel

### Description

A business hotel is a place of lodging aimed toward the business traveler but also accommodates a growing number of recreational travelers. These hotels provide sleeping accommodations and other limited facilities, such as a breakfast buffet bar and afternoon beverage bar (no lunch or dinner is served and limited meeting facilities are provided). Each unit is a large single room. Business hotels provide very few or none of the supporting facilities provided at hotels or suite hotels and are usually smaller in size. Hotel (Land Use 310), all suites hotel (Land Use 311), motel (Land Use 320), and resort hotel (Land Use 330) are related uses.

### Additional Data

Ten studies provided information on room occupancy at the time of data collection. The average occupancy rate for these sites was approximately 86 percent.

Time-of-day distribution data for this land use are presented in Appendix A. For the eight general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:30 and 8:30 a.m. and 7:00 and 8:00 p.m., respectively. Vehicle counts at three general urban/suburban sites determined their Saturday and Sunday peak hours to be between 4:15 and 5:15 p.m. and 10:00 and 11:00 a.m., respectively.

For one center city core site with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:30 and 8:30 a.m. and 9:00 and 10:00 p.m., respectively. Vehicle counts at the one center city core site determined its Saturday and Sunday peak hours to be between 9:15 and 10:15 p.m. and 10:15 and 11:15 a.m., respectively.

The average numbers of person trips per vehicle trip at the three general urban/suburban sites at which both person trip and vehicle trip data were collected, were as follows:

- 1.31 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.34 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.

The sites were surveyed in the 1980s, the 1990s, and the 2010s in the District of Columbia, Florida, Georgia, Indiana, Minnesota, Texas, Vermont, Washington, and Wisconsin.

***For all lodging uses, it is important to collect data on occupied rooms as well as total rooms in order to accurately predict trip generation characteristics for the site.***

### Source Numbers

216, 301, 306, 436, 507, 867, 870, 872, 877, 925, 945, 951, 959

## Business Hotel (312)

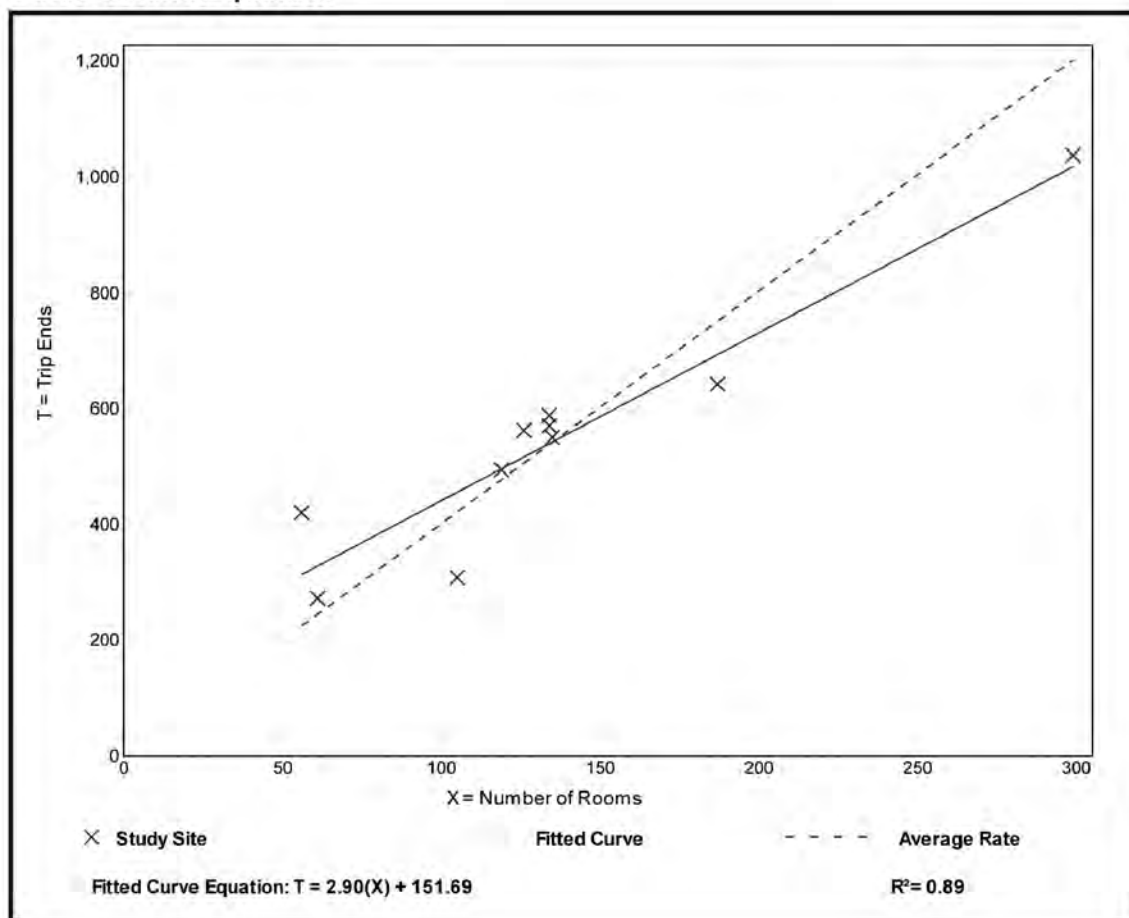
**Vehicle Trip Ends vs: Rooms**  
**On a: Weekday**

**Setting/Location: General Urban/Suburban**  
Number of Studies: 10  
Avg. Num. of Rooms: 136  
Directional Distribution: 50% entering, 50% exiting

### Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
4.02	2.94 - 7.52	0.92

### Data Plot and Equation



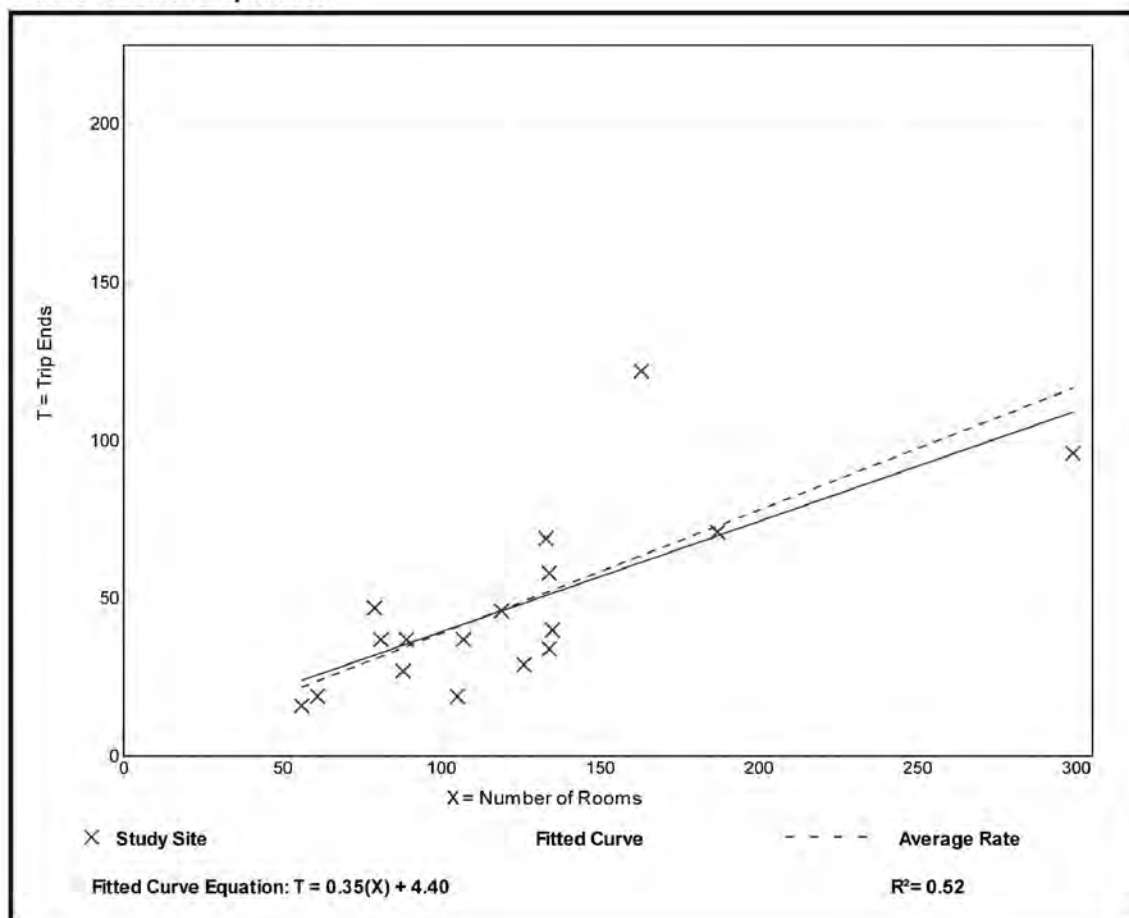
## Business Hotel (312)

**Vehicle Trip Ends vs:** Rooms  
**On a:** Weekday,  
 Peak Hour of Adjacent Street Traffic,  
 One Hour Between 7 and 9 a.m.  
**Setting/Location:** General Urban/Suburban  
 Number of Studies: 18  
 Avg. Num. of Rooms: 121  
 Directional Distribution: 42% entering, 58% exiting

### Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.39	0.18 - 0.75	0.15

### Data Plot and Equation



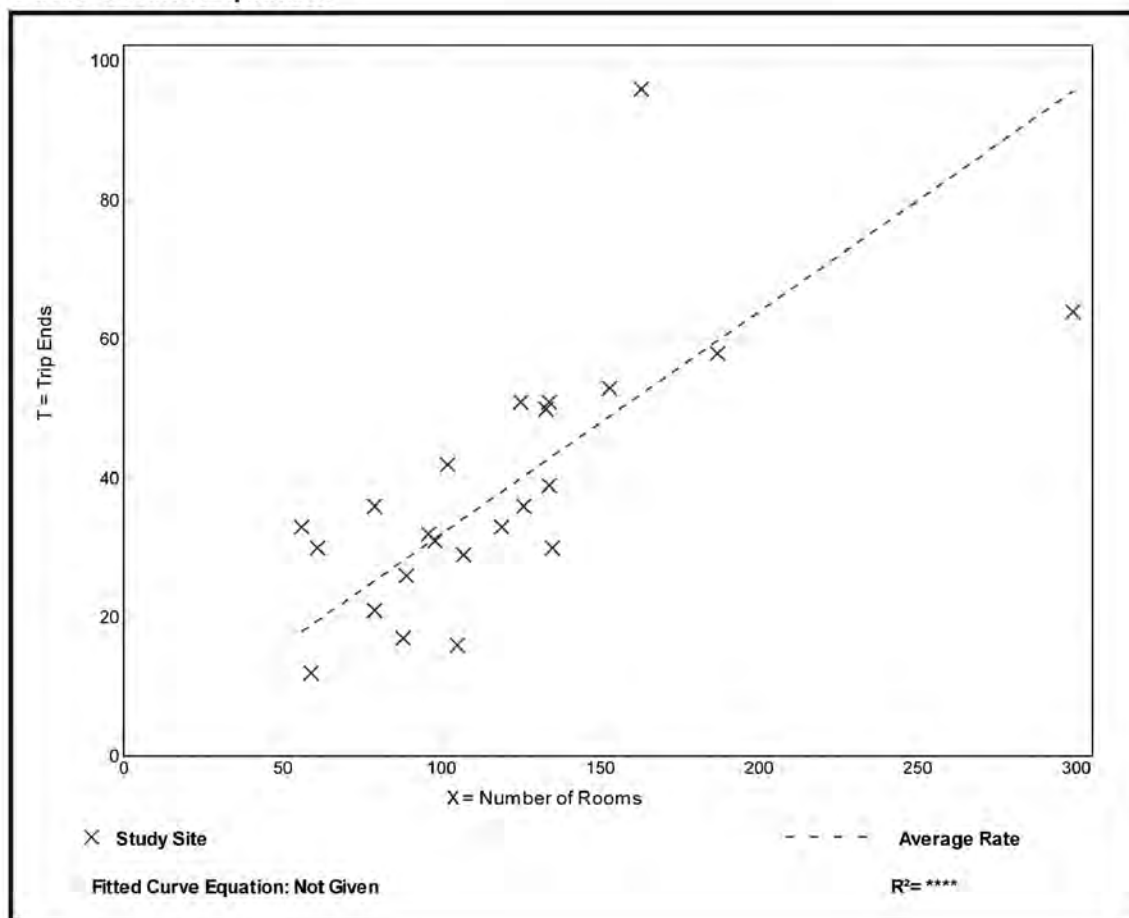
## Business Hotel (312)

**Vehicle Trip Ends vs:** Rooms  
**On a:** Weekday,  
 Peak Hour of Adjacent Street Traffic,  
 One Hour Between 4 and 6 p.m.  
**Setting/Location:** General Urban/Suburban  
 Number of Studies: 23  
 Avg. Num. of Rooms: 119  
 Directional Distribution: 55% entering, 45% exiting

### Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.32	0.15 - 0.59	0.11

### Data Plot and Equation



## Land Use: 931 Quality Restaurant

### Description

This land use consists of high quality, full-service eating establishments with a typical duration of stay of at least one hour. Quality restaurants generally do not serve breakfast; some do not serve lunch; all serve dinner. This type of restaurant often requests and sometimes requires reservations and is generally not part of a chain. Patrons commonly wait to be seated, are served by a waiter/waitress, order from menus and pay for meals after they eat. While some of the study sites have lounge or bar facilities (serving alcoholic beverages), they are ancillary to the restaurant. Fast casual restaurant (Land Use 930) and high-turnover (sit-down) restaurant (Land Use 932) are related uses.

### Additional Data

The outdoor seating area is not included in the overall gross floor area. Therefore, the number of seats may be a more reliable independent variable on which to establish trip generation rates for facilities having significant outdoor seating.

The sites were surveyed in the 1980s and the 1990s in Alberta (CAN), California, Colorado, Florida, Indiana, Kentucky, New Jersey, and Utah.

### Source Numbers

126, 260, 291, 301, 338, 339, 368, 437, 440, 976

## Quality Restaurant (931)

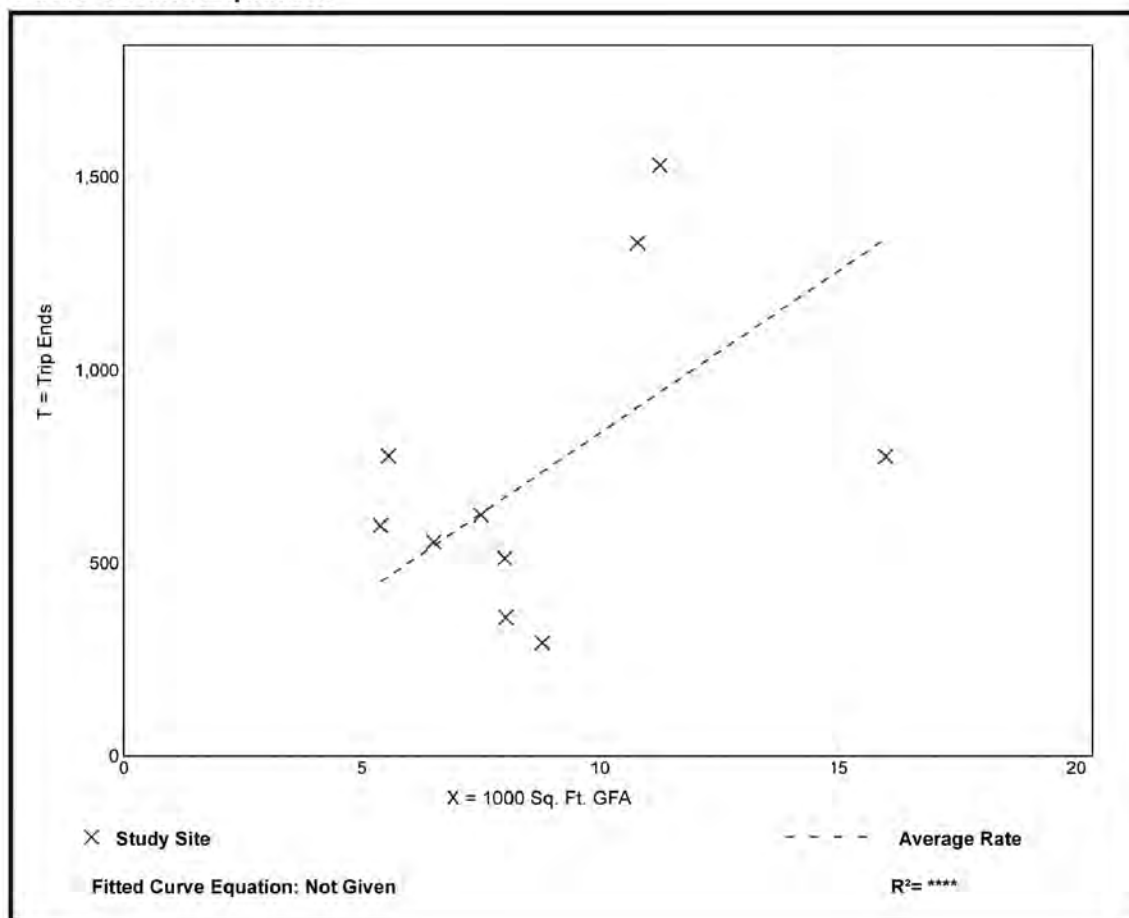
**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**  
**On a: Weekday**

**Setting/Location: General Urban/Suburban**  
Number of Studies: 10  
1000 Sq. Ft. GFA: 9  
Directional Distribution: 50% entering, 50% exiting

### Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
83.84	33.45 - 139.93	40.01

### Data Plot and Equation



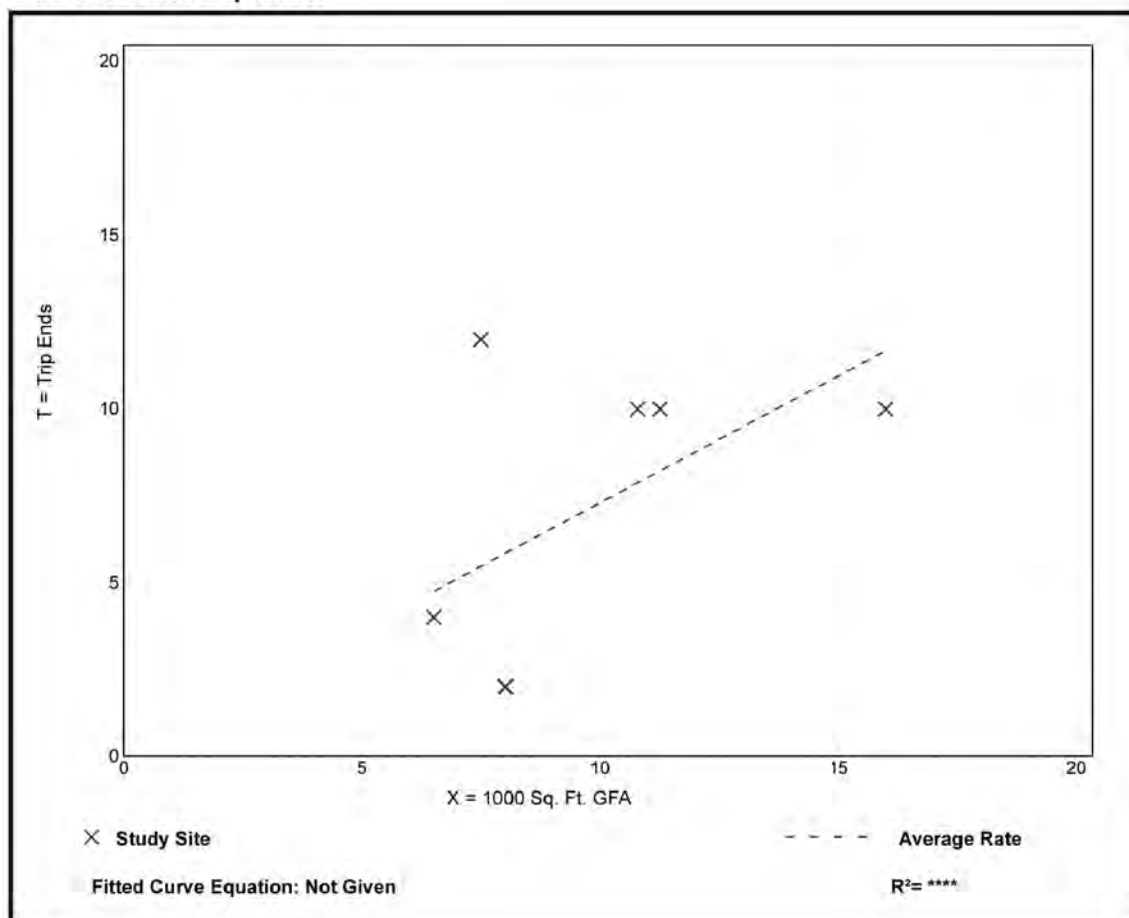
## Quality Restaurant (931)

**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 7 and 9 a.m.**  
**Setting/Location: General Urban/Suburban**  
 Number of Studies: 7  
 1000 Sq. Ft. GFA: 10  
 Directional Distribution: Not Available

### Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.73	0.25 - 1.60	0.42

### Data Plot and Equation



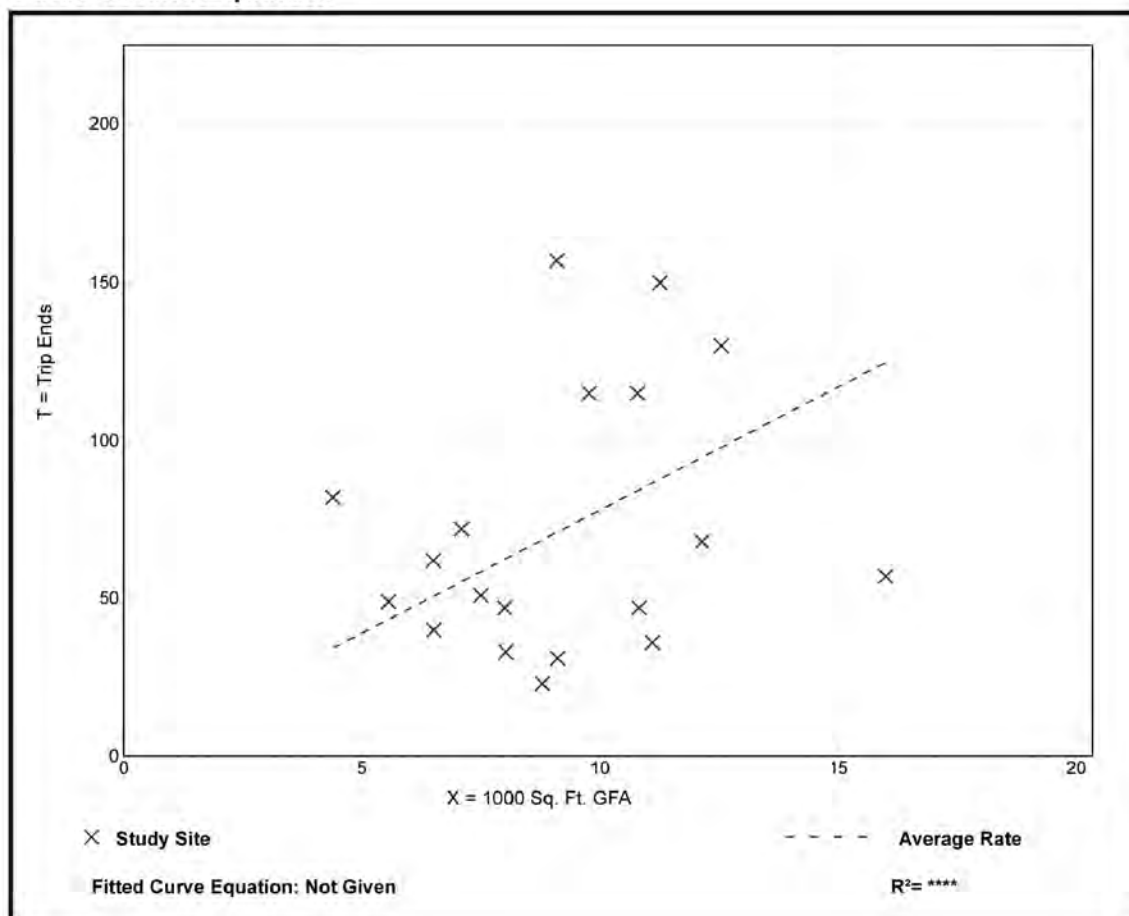
## Quality Restaurant (931)

**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 4 and 6 p.m.**  
**Setting/Location: General Urban/Suburban**  
 Number of Studies: 19  
 1000 Sq. Ft. GFA: 9  
 Directional Distribution: 67% entering, 33% exiting

### Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
7.80	2.62 - 18.68	4.49

### Data Plot and Equation



## Land Use: 934

### Fast-Food Restaurant with Drive-Through Window

#### Description

This category includes fast-food restaurants with drive-through windows. This type of restaurant is characterized by a large drive-through clientele, long hours of service (some are open for breakfast, all are open for lunch and dinner, some are open late at night or 24 hours a day) and high turnover rates for eat-in customers. These limited-service eating establishments do not provide table service. Non-drive-through patrons generally order at a cash register and pay before they eat. Fast casual restaurant (Land Use 930), high-turnover (sit-down) restaurant (Land Use 932), fast-food restaurant without drive-through window (Land Use 933), and fast-food restaurant with drive-through window and no indoor seating (Land Use 935) are related uses.

#### Additional Data

***Users should exercise caution when applying statistics during the AM peak periods, as the sites contained in the database for this land use may or may not be open for breakfast. In cases where it was confirmed that the sites were not open for breakfast, data for the AM peak hour of the adjacent street traffic were removed from the database.***

The outdoor seating area is not included in the overall gross floor area. Therefore, the number of seats may be a more reliable independent variable on which to establish trip generation rates for facilities having significant outdoor seating.

Time-of-day distribution data for this land use for a weekday, Saturday, and Sunday are presented in Appendix A. For the 46 general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 11:45 a.m. and 12:45 p.m. and 12:00 and 1:00 p.m., respectively. For the one dense multi-use urban site with data, the same AM and PM peak hours were observed.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alaska, Alberta (CAN), California, Colorado, Florida, Indiana, Kentucky, Maryland, Massachusetts, Minnesota, Montana, New Hampshire, New Jersey, New York, North Carolina, Ohio, Pennsylvania, South Dakota, Texas, Vermont, Virginia, Washington, and Wisconsin.

#### Source Numbers

163, 164, 168, 180, 181, 241, 245, 278, 294, 300, 301, 319, 338, 340, 342, 358, 389, 438, 502, 552, 577, 583, 584, 617, 640, 641, 704, 715, 728, 810, 866, 867, 869, 885, 886, 927, 935, 962, 977

## Fast-Food Restaurant with Drive-Through Window (934)

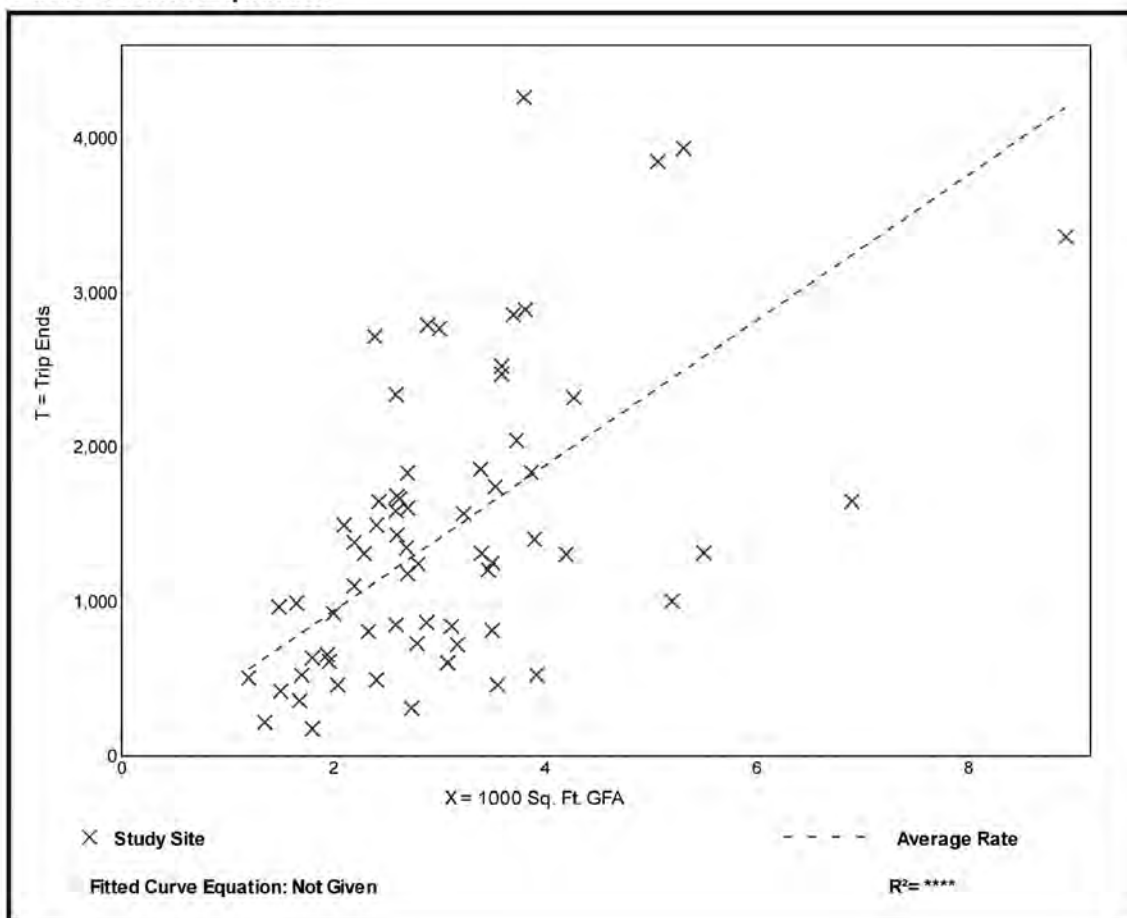
**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**  
**On a: Weekday**

**Setting/Location: General Urban/Suburban**  
Number of Studies: 67  
1000 Sq. Ft. GFA: 3  
Directional Distribution: 50% entering, 50% exiting

### Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
470.95	98.89 - 1137.66	244.44

### Data Plot and Equation



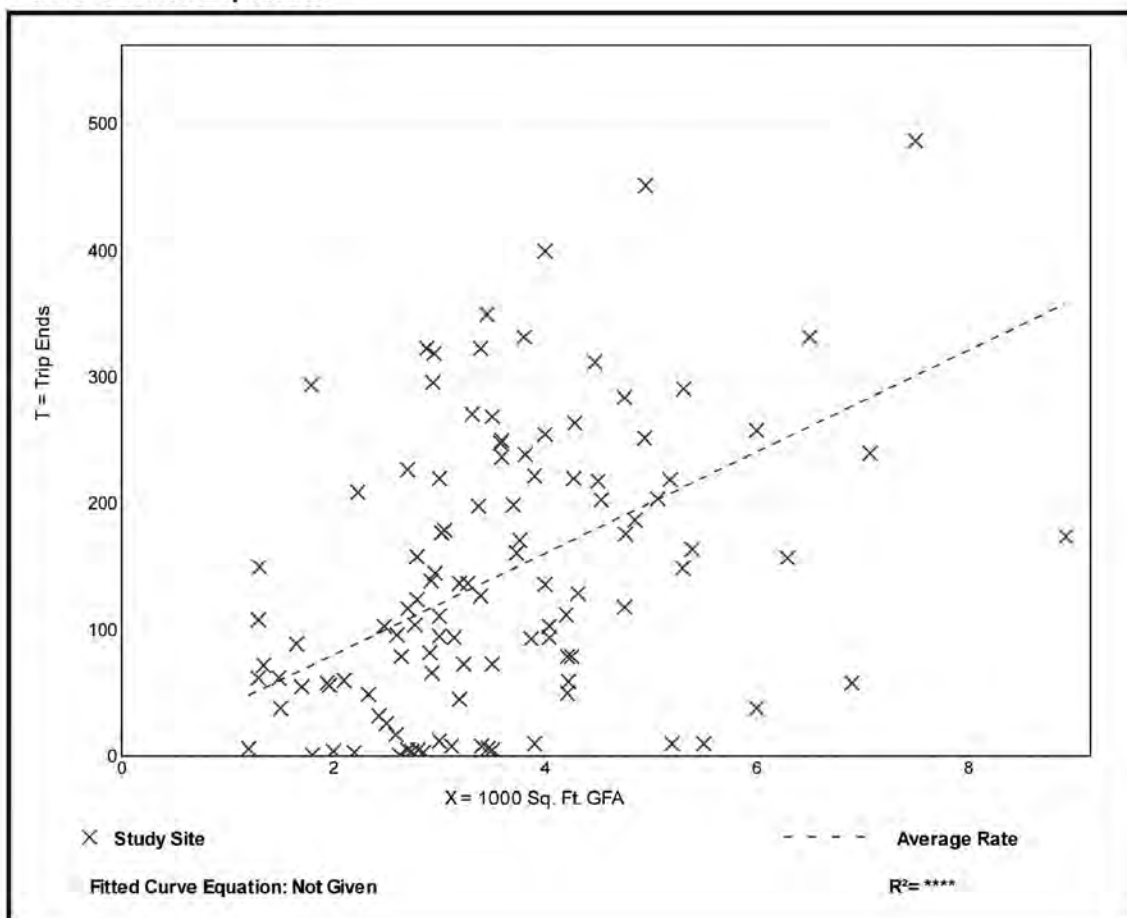
## Fast-Food Restaurant with Drive-Through Window (934)

**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 7 and 9 a.m.**  
**Setting/Location: General Urban/Suburban**  
 Number of Studies: 111  
 1000 Sq. Ft. GFA: 4  
 Directional Distribution: 51% entering, 49% exiting

### Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
40.19	0.38 - 164.25	28.78

### Data Plot and Equation



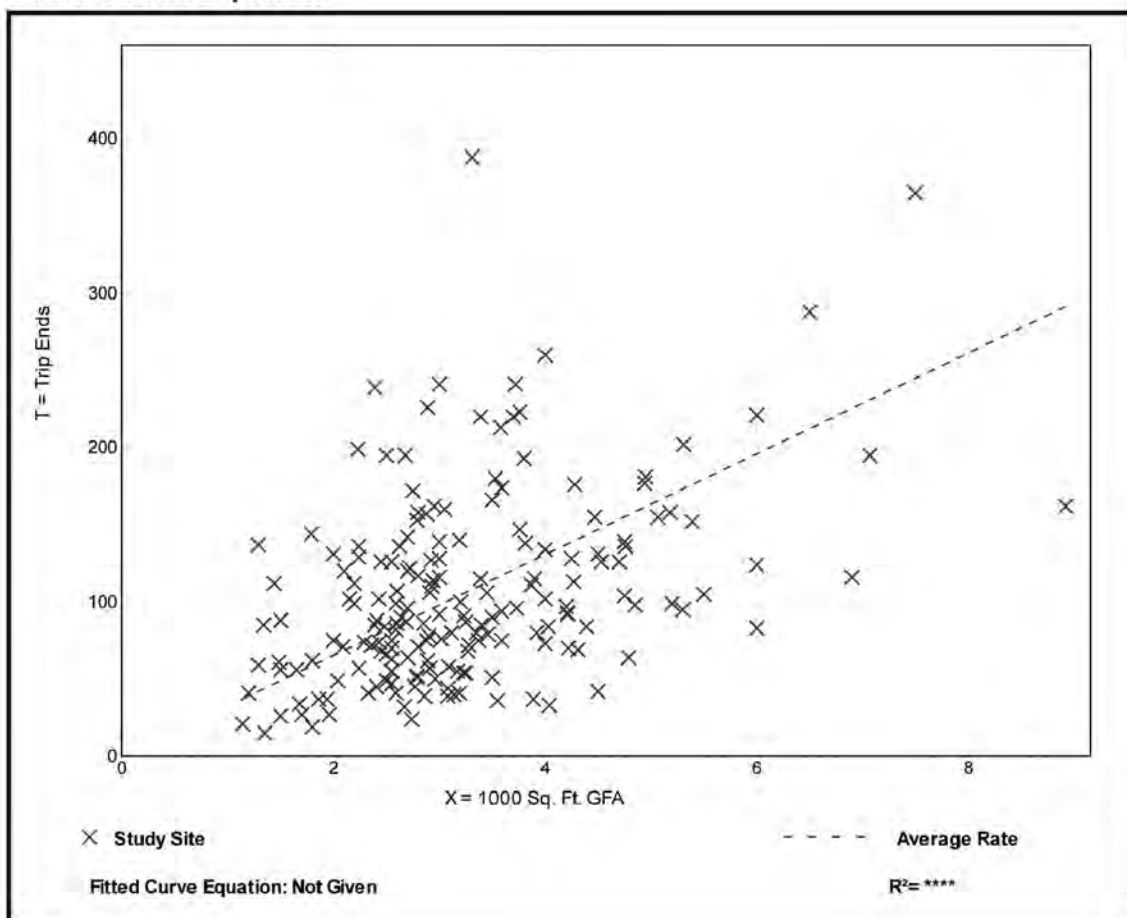
## Fast-Food Restaurant with Drive-Through Window (934)

**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 4 and 6 p.m.**  
**Setting/Location: General Urban/Suburban**  
 Number of Studies: 185  
 1000 Sq. Ft. GFA: 3  
 Directional Distribution: 52% entering, 48% exiting

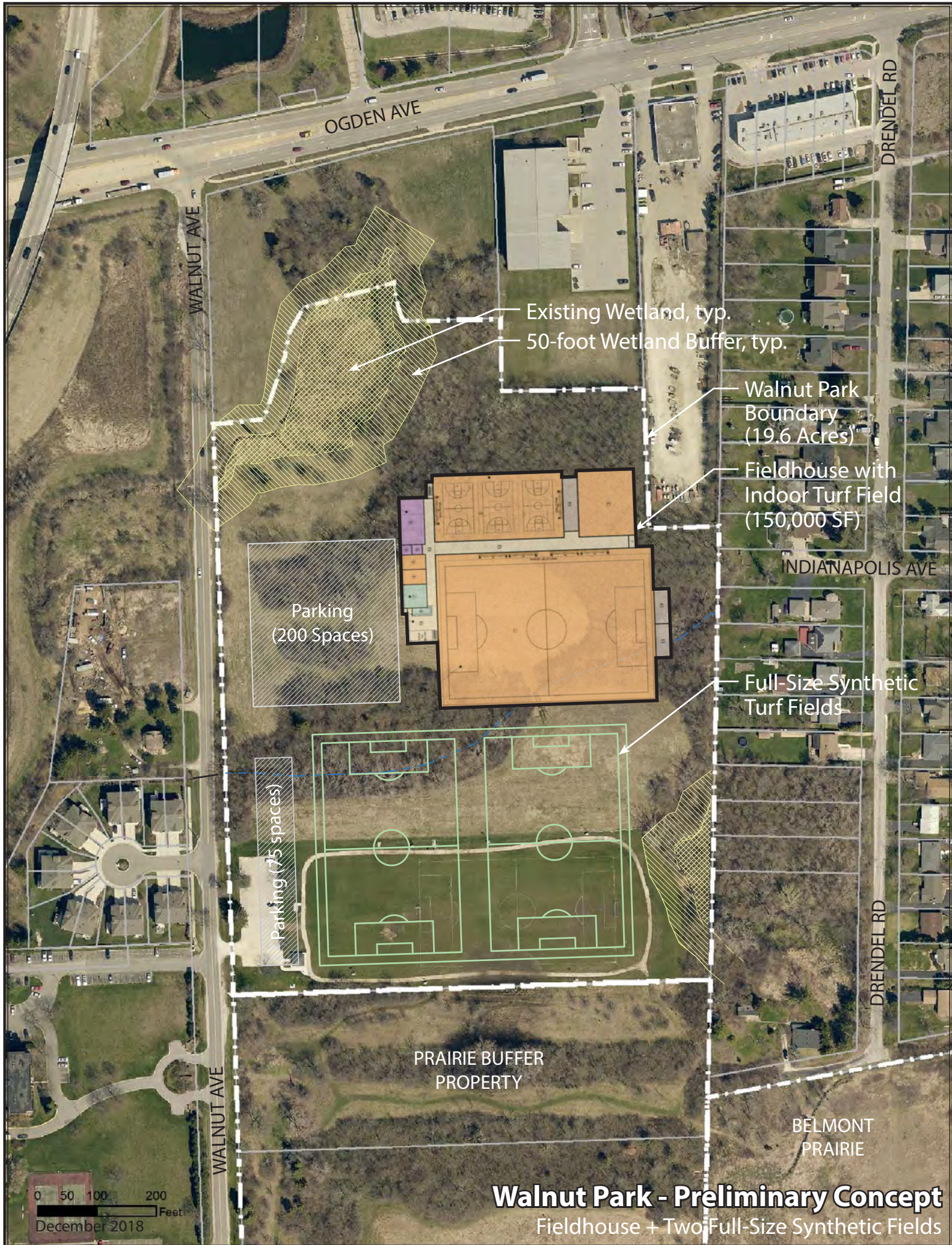
### Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
32.67	8.17 - 117.22	17.87

### Data Plot and Equation



# Walnut Park Development Plan



**Walnut Park - Preliminary Concept**  
 Fieldhouse + Two Full-Size Synthetic Fields

## Level of Service Criteria

## LEVEL OF SERVICE CRITERIA

Signalized Intersections		
Level of Service	Interpretation	Average Control Delay (seconds per vehicle)
A	Favorable progression. Most vehicles arrive during the green indication and travel through the intersection without stopping.	≤10
B	Good progression, with more vehicles stopping than for Level of Service A.	>10 - 20
C	Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear. Number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.	>20 - 35
D	The volume-to-capacity ratio is high and either progression is ineffective or the cycle length is too long. Many vehicles stop and individual cycle failures are noticeable.	>35 - 55
E	Progression is unfavorable. The volume-to-capacity ratio is high and the cycle length is long. Individual cycle failures are frequent.	>55 - 80
F	The volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue.	>80.0
Unsignalized Intersections		
Level of Service	Average Total Delay (SEC/VEH)	
A	0 - 10	
B	> 10 - 15	
C	> 15 - 25	
D	> 25 - 35	
E	> 35 - 50	
F	> 50	

Source: *Highway Capacity Manual*, 2010.

Capacity Analysis Summary Sheets  
Existing Weekday Morning Peak Hour Conditions

# HCM 6th TWSC

## 1: Walnut Avenue & Ogden Avenue

10/26/2020

Intersection						
Int Delay, s/veh	8					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↑	↑↑	↑↑	
Traffic Vol, veh/h	1937	62	14	987	53	29
Future Vol, veh/h	1937	62	14	987	53	29
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	0	-
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	4	0	0	4	3	0
Mvmt Flow	2176	70	16	1109	60	33
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	2247	0	2799	1124
Stage 1	-	-	-	-	2212	-
Stage 2	-	-	-	-	587	-
Critical Hdwy	-	-	4.1	-	6.86	6.9
Critical Hdwy Stg 1	-	-	-	-	5.86	-
Critical Hdwy Stg 2	-	-	-	-	5.86	-
Follow-up Hdwy	-	-	2.2	-	3.53	3.3
Pot Cap-1 Maneuver	-	-	233	-	~ 14	203
Stage 1	-	-	-	-	68	-
Stage 2	-	-	-	-	516	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	233	-	~ 13	203
Mov Cap-2 Maneuver	-	-	-	-	~ 53	-
Stage 1	-	-	-	-	63	-
Stage 2	-	-	-	-	516	-
Approach	EB	WB	NB			
HCM Control Delay, s	0	0.3	298.9			
HCM LOS			F			
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	72	-	-	233	-	
HCM Lane V/C Ratio	1.28	-	-	0.068	-	
HCM Control Delay (s)	298.9	-	-	21.6	-	
HCM Lane LOS	F	-	-	C	-	
HCM 95th %tile Q(veh)	7.3	-	-	0.2	-	
Notes						
-: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    *: All major volume in platoon						

Capacity Analysis Summary Sheets  
Existing Weekday Evening Peak Hour Conditions

# HCM 6th TWSC

## 1: Walnut Avenue & Ogden Avenue

10/26/2020

Intersection						
Int Delay, s/veh	0.7					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↖	↑↑	↗	
Traffic Vol, veh/h	1260	89	29	1753	28	23
Future Vol, veh/h	1260	89	29	1753	28	23
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	0	-
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	2	4	0	1	4	0
Mvmt Flow	1326	94	31	1845	29	24
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	1421	0	2359	711
Stage 1	-	-	-	-	1374	-
Stage 2	-	-	-	-	985	-
Critical Hdwy	-	-	4.1	-	6.88	6.9
Critical Hdwy Stg 1	-	-	-	-	5.88	-
Critical Hdwy Stg 2	-	-	-	-	5.88	-
Follow-up Hdwy	-	-	2.2	-	3.54	3.3
Pot Cap-1 Maneuver	-	-	485	-	~ 29	380
Stage 1	-	-	-	-	197	-
Stage 2	-	-	-	-	318	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	485	-	~ 27	380
Mov Cap-2 Maneuver	-	-	-	-	114	-
Stage 1	-	-	-	-	184	-
Stage 2	-	-	-	-	318	-
Approach	EB	WB	NB			
HCM Control Delay, s	0	0.2	36.4			
HCM LOS			E			
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	167	-	-	485	-	
HCM Lane V/C Ratio	0.321	-	-	0.063	-	
HCM Control Delay (s)	36.4	-	-	12.9	-	
HCM Lane LOS	E	-	-	B	-	
HCM 95th %tile Q(veh)	1.3	-	-	0.2	-	
Notes						
-: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    *: All major volume in platoon						

Capacity Analysis Summary Sheets  
No Build Weekday Morning Peak Hour Conditions

# HCM 6th TWSC

## 1: Walnut Avenue & Ogden Avenue

11/09/2020

Intersection						
Int Delay, s/veh	13.3					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↖	↑↑	↘	
Traffic Vol, veh/h	1995	66	18	1017	60	36
Future Vol, veh/h	1995	66	18	1017	60	36
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	0	-
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	4	0	0	4	3	0
Mvmt Flow	2242	74	20	1143	67	40

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	2317	0	2892
Stage 1	-	-	-	-	2280
Stage 2	-	-	-	-	612
Critical Hdwy	-	-	4.1	-	6.86
Critical Hdwy Stg 1	-	-	-	-	5.86
Critical Hdwy Stg 2	-	-	-	-	5.86
Follow-up Hdwy	-	-	2.2	-	3.53
Pot Cap-1 Maneuver	-	-	219	-	~ 12
Stage 1	-	-	-	-	~ 62
Stage 2	-	-	-	-	501
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	219	-	~ 11
Mov Cap-2 Maneuver	-	-	-	-	~ 48
Stage 1	-	-	-	-	~ 56
Stage 2	-	-	-	-	501

Approach	EB	WB	NB
HCM Control Delay, s	0	0.4	\$ 436.3
HCM LOS			F

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	67	-	-	219	-
HCM Lane V/C Ratio	1.61	-	-	0.092	-
HCM Control Delay (s)	\$ 436.3	-	-	23.1	-
HCM Lane LOS	F	-	-	C	-
HCM 95th %tile Q(veh)	9.4	-	-	0.3	-

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

Capacity Analysis Summary Sheets  
No Build Weekday Evening Peak Hour Conditions

## HCM 6th TWSC

### 1: Walnut Avenue & Ogden Avenue

11/09/2020

Intersection						
Int Delay, s/veh	1.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↑	↑↑	↑	
Traffic Vol, veh/h	1298	97	37	1806	37	32
Future Vol, veh/h	1298	97	37	1806	37	32
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	0	-
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	2	4	0	1	4	0
Mvmt Flow	1366	102	39	1901	39	34
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	1469	0	2447	735
Stage 1	-	-	-	-	1418	-
Stage 2	-	-	-	-	1029	-
Critical Hdwy	-	-	4.1	-	6.88	6.9
Critical Hdwy Stg 1	-	-	-	-	5.88	-
Critical Hdwy Stg 2	-	-	-	-	5.88	-
Follow-up Hdwy	-	-	2.2	-	3.54	3.3
Pot Cap-1 Maneuver	-	-	465	-	~ 25	367
Stage 1	-	-	-	-	186	-
Stage 2	-	-	-	-	301	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	465	-	~ 23	367
Mov Cap-2 Maneuver	-	-	-	-	104	-
Stage 1	-	-	-	-	170	-
Stage 2	-	-	-	-	301	-
Approach	EB	WB	NB			
HCM Control Delay, s	0	0.3	46.7			
HCM LOS			E			
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	156	-	-	465	-	
HCM Lane V/C Ratio	0.466	-	-	0.084	-	
HCM Control Delay (s)	46.7	-	-	13.4	-	
HCM Lane LOS	E	-	-	B	-	
HCM 95th %tile Q(veh)	2.2	-	-	0.3	-	
Notes						
-: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    *: All major volume in platoon						

Capacity Analysis Summary Sheets  
Total Projected Weekday Morning Peak Hour Conditions

# HCM 6th TWSC

## 1: Walnut Avenue & Ogden Avenue

08/30/2021

Intersection						
Int Delay, s/veh	2					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↖	↑↑	↖	↖
Traffic Vol, veh/h	2005	70	24	1017	79	38
Future Vol, veh/h	2005	70	24	1017	79	38
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	65	0
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	4	0	0	4	3	0
Mvmt Flow	2253	79	27	1143	89	43

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	2333	0	2920
Stage 1	-	-	-	-	2294
Stage 2	-	-	-	-	626
Critical Hdwy	-	-	4.1	-	6.86
Critical Hdwy Stg 1	-	-	-	-	5.86
Critical Hdwy Stg 2	-	-	-	-	5.86
Follow-up Hdwy	-	-	2.2	-	3.53
Pot Cap-1 Maneuver	-	-	324	-	~ 4 *263
Stage 1	-	-	-	-	226
Stage 2	-	-	-	-	493
Platoon blocked, %	-	-	1	-	1
Mov Cap-1 Maneuver	-	-	323	-	~ 4 *263
Mov Cap-2 Maneuver	-	-	-	-	139
Stage 1	-	-	-	-	226
Stage 2	-	-	-	-	452

Approach	EB	WB	NB
HCM Control Delay, s	0	0.4	52.9
HCM LOS			F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	139	263	-	-	323	-
HCM Lane V/C Ratio	0.639	0.162	-	-	0.083	-
HCM Control Delay (s)	68.1	21.3	-	-	17.2	-
HCM Lane LOS	F	C	-	-	C	-
HCM 95th %tile Q(veh)	3.4	0.6	-	-	0.3	-

**Notes**  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

# HCM 6th TWSC

## 2: Proposed Access Drive & Ogden Avenue

11/09/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	2033	10	0	1041	0	6
Future Vol, veh/h	2033	10	0	1041	0	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	Free	-	None	-	Stop
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	98	95	95
Heavy Vehicles, %	4	0	0	4	0	0
Mvmt Flow	2140	11	0	1062	0	6

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	21.8
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	220	-	-
HCM Lane V/C Ratio	0.029	-	-
HCM Control Delay (s)	21.8	-	-
HCM Lane LOS	C	-	-
HCM 95th %tile Q(veh)	0.1	-	-

### HCM 6th TWSC 3: Walnut Avenue & Proposed Access Drive

11/09/2020

Intersection						
Int Delay, s/veh	1.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	0	21	96	0	10	84
Future Vol, veh/h	0	21	96	0	10	84
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	0	0	2	0	0	0
Mvmt Flow	0	22	101	0	11	88

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	211	101	0	0	101	0
Stage 1	101	-	-	-	-	-
Stage 2	110	-	-	-	-	-
Critical Hdwy	6.4	6.2	-	-	4.1	-
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	-	-	2.2	-
Pot Cap-1 Maneuver	782	960	-	-	1504	-
Stage 1	928	-	-	-	-	-
Stage 2	920	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	776	960	-	-	1504	-
Mov Cap-2 Maneuver	776	-	-	-	-	-
Stage 1	921	-	-	-	-	-
Stage 2	920	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.8	0	0.8
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	960	1504
HCM Lane V/C Ratio	-	-	0.023	0.007
HCM Control Delay (s)	-	-	8.8	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0

Capacity Analysis Summary Sheets  
Total Projected Weekday Evening Peak Hour Conditions

# HCM 6th TWSC

## 1: Walnut Avenue & Ogden Avenue

08/30/2021

Intersection						
Int Delay, s/veh	0.9					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↖	↑↑	↖	↖
Traffic Vol, veh/h	1318	105	55	1806	57	35
Future Vol, veh/h	1318	105	55	1806	57	35
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	65	0
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	2	4	0	1	4	0
Mvmt Flow	1387	111	58	1901	60	37

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	1499	0	2511
Stage 1	-	-	-	-	1444
Stage 2	-	-	-	-	1067
Critical Hdwy	-	-	4.1	-	6.88
Critical Hdwy Stg 1	-	-	-	-	5.88
Critical Hdwy Stg 2	-	-	-	-	5.88
Follow-up Hdwy	-	-	2.2	-	3.54
Pot Cap-1 Maneuver	-	-	819	-	*~ 25
Stage 1	-	-	-	-	*508
Stage 2	-	-	-	-	*287
Platoon blocked, %	-	-	1	-	1
Mov Cap-1 Maneuver	-	-	818	-	*~ 23
Mov Cap-2 Maneuver	-	-	-	-	*169
Stage 1	-	-	-	-	*508
Stage 2	-	-	-	-	*267

Approach	EB	WB	NB
HCM Control Delay, s	0	0.3	27.9
HCM LOS			D

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	169	544	-	-	818	-
HCM Lane V/C Ratio	0.355	0.068	-	-	0.071	-
HCM Control Delay (s)	37.6	12.1	-	-	9.7	-
HCM Lane LOS	E	B	-	-	A	-
HCM 95th %tile Q(veh)	1.5	0.2	-	-	0.2	-

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

## HCM 6th TWSC

### 2: Proposed Access Drive & Ogden Avenue

11/09/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	1333	20	0	1861	0	8
Future Vol, veh/h	1333	20	0	1861	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	Free	-	None	-	Stop
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	2	0	0	1	0	0
Mvmt Flow	1403	21	0	1959	0	8
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	-	-	-	-	702
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	-	-	-	-	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	-	3.3
Pot Cap-1 Maneuver	-	0	0	-	0	385
Stage 1	-	0	0	-	0	-
Stage 2	-	0	0	-	0	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	-	385
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB	WB	NB			
HCM Control Delay, s	0	0	14.6			
HCM LOS						B
Minor Lane/Major Mvmt	NBLn1	EBT	WBT			
Capacity (veh/h)	385	-	-			
HCM Lane V/C Ratio	0.022	-	-			
HCM Control Delay (s)	14.6	-	-			
HCM Lane LOS	B	-	-			
HCM 95th %tile Q(veh)	0.1	-	-			

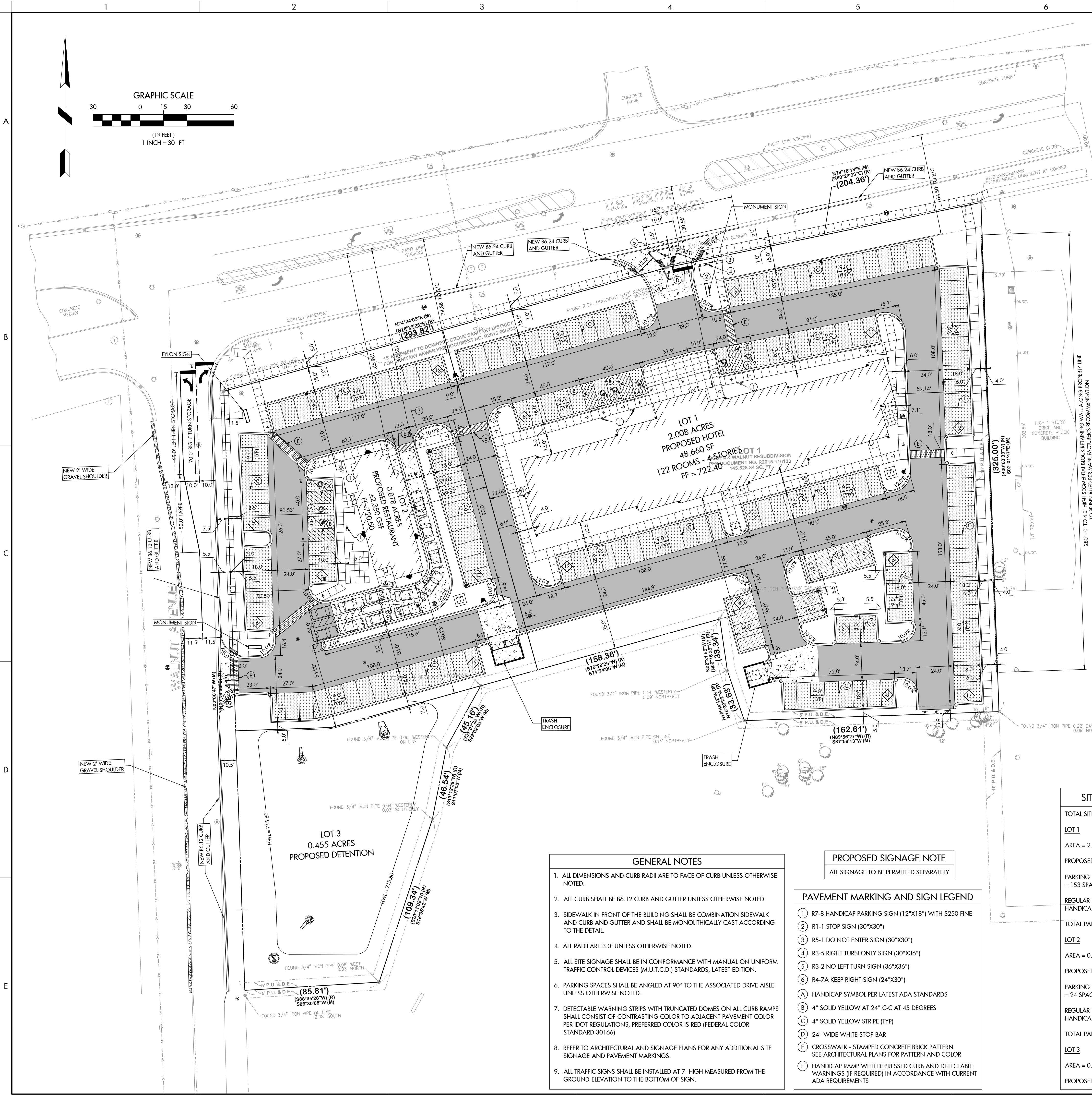
## HCM 6th TWSC

### 3: Walnut Avenue & Proposed Access Drive

11/09/2020

Intersection						
Int Delay, s/veh	1.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	0	23	69	0	26	134
Future Vol, veh/h	0	23	69	0	26	134
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	0	0	2	0	0	3
Mvmt Flow	0	24	73	0	27	141
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	268	73	0	0	73	0
Stage 1	73	-	-	-	-	-
Stage 2	195	-	-	-	-	-
Critical Hdwy	6.4	6.2	-	-	4.1	-
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	-	-	2.2	-
Pot Cap-1 Maneuver	726	995	-	-	1540	-
Stage 1	955	-	-	-	-	-
Stage 2	843	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	712	995	-	-	1540	-
Mov Cap-2 Maneuver	712	-	-	-	-	-
Stage 1	937	-	-	-	-	-
Stage 2	843	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	8.7	0		1.2		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	995	1540	-	
HCM Lane V/C Ratio	-	-	0.024	0.018	-	
HCM Control Delay (s)	-	-	8.7	7.4	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0.1	-	

# Alternate Development Plan Site Plan



### SHARED PARKING SPACE CALCULATIONS

LAND USE (REQUIRED)	WEEKDAY			WEEKEND		
	MIDNIGHT-7:00 AM	7:00 AM - 6:00 PM	6:00 PM - MIDNIGHT	MIDNIGHT-7:00 AM	7:00 AM - 6:00 PM	6:00 PM - MIDNIGHT
LODGING (153)	100% = 153	60% = 92	90% = 138	100% = 153	65% = 99	80% = 122
EATING AND DRINKING (24)	50% = 12	70% = 17	100% = 24	45% = 11	70% = 17	100% = 24
<b>TOTAL REQUIRED</b>	<b>165</b>	<b>109</b>	<b>162</b>	<b>164</b>	<b>116</b>	<b>146</b>

TOTAL PARKING PROVIDED = 182 SPACES. THEREFORE THE OVERALL SITE MEETS THE SHARED PARKING REQUIREMENTS

### ZONING ANALYSIS

PROJECT NAME	PROPOSED WOODSPRING SUITES HOTEL
ADDRESS	2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515
PIN(S)	08-01-304-031
ZONING DISTRICT	EXISTING: O-R-M ; OFFICE - RESEARCH - MANUFACTURING PROPOSED: B-3 ; GENERAL SERVICES AND HIGHWAY BUSINESS
MINIMUM DISTRICT AREA	MIN. 4 ACRES ; PARCEL AREA = 4.31 ACRES
EXISTING USE	VACANT LAND
PROPOSED USE	WOODSPRING SUITES HOTEL AND FUTURE DRIVE-THROUGH RESTAURANT
PETITION TYPE	P.U.D.
DEVIATIONS	LOT 1 : 1. REDUCED NUMBER OF ON-SITE PARKING SPACES LOT 2 : 1. NONE OVERALL DEVELOPMENT : 1. NONE

#### LOT 1 - PROPOSED WOODSPRING SUITES HOTEL

REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	87,465 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	47 FEET	YES	-13 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.56	YES	-0.19
BLDG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	130.66 FEET	YES	+55.66 FEET
BLDG. SIDE SETBACK (EAST)	MINIMUM	0.0 FEET	59.14 FEET	YES	+59.14 FEET
BLDG. SIDE SETBACK (WEST, INTERIOR)	MINIMUM	0.0 FEET	22.00 FEET	YES	+22.00 FEET
BLDG. SETBACK (SOUTH)	MINIMUM	10.0 FEET	77.99 FEET	YES	+67.99 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	64.50 FEET TO B/C	YES	+14.50 FEET
PARKING SETBACK (EAST)	MINIMUM	5.0 FEET	10.00 FEET	YES	+5.00 FEET
PARKING SETBACK (WEST, INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (SOUTH)	MINIMUM	5.0 FEET	5.0 FEET	YES	0.0 FEET
TOTAL PARKING REQUIRED	MINIMUM	1.25/ROOM (153)	125	NO	-28 SPACES
TWO-WAY DRIVE AISLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
OPEN SPACE (10%)	MINIMUM	8,747 S.F.	19,165 S.F.	YES	+10,418 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	4,373 S.F.	4,973 S.F.	YES	+600 S.F.

#### LOT 2 - FUTURE DRIVE-THROUGH RESTAURANT (USER TO BE DETERMINED)

REQUIREMENT	FACTOR	REQUIRED	PROPOSED	MEETS REQUIREMENT	DIFFERENCE
LOT AREA	N/A	N/A	38,304 S.F.	N/A	N/A
MAX. BUILDING HEIGHT	MAXIMUM	60 FEET	25 FEET (PARAPET)	YES	-35 FEET
MAX. FLOOR AREA RATIO (FAR)	MAXIMUM	0.75	0.06	YES	-0.69
BLDG. STREET SETBACK (NORTH - OGDEN)	MINIMUM	75 FEET FROM CL	126.92 FEET	YES	+51.92 FEET
BLDG. SETBACK (SOUTH)	MINIMUM	10.0 FEET	80.23 FEET	YES	+70.23 FEET
PARKING SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	74.88 FEET TO B/C	YES	+24.88 FEET
PARKING SETBACK (EAST - INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
PARKING SETBACK (WEST - WALNUT)	MINIMUM	8.0 FEET	8.5 FEET	YES	0.5 FEET
TOTAL PARKING REQUIRED	MINIMUM	10/1K SF (24)	57	YES	+33 SPACES
TWO-WAY DRIVE AISLE WIDTH	MINIMUM	24 FEET	24 FEET	YES	NONE
PARKING SPACE SIZE	MINIMUM	9' X 18'	9' X 18'	YES	NONE
DRIVE-THRU SETBACK (NORTH - OGDEN)	MINIMUM	50.0 FEET FROM CL	122.24 FEET	YES	+72.24 FEET
DRIVE-THRU SETBACK (EAST - INTERIOR)	MINIMUM	N/A	N/A	N/A	N/A
DRIVE-THRU SETBACK (WEST - WALNUT)	MINIMUM	8.0 FEET	50.50 FEET	YES	+42.50 FEET
DRIVE-THRU SETBACK (SOUTH)	MINIMUM	5.0 FEET	54.00 FEET	YES	+49.00 FEET
STACKING SPACES	MINIMUM	12 CARS	12 CARS	YES	0 CARS
DRIVE-THROUGH LANE WIDTH	MINIMUM	10 FEET	12 FEET	YES	+2 FEET
BICYCLE PARKING	MINIMUM	2 SPACES	4 SPACES	YES	+2 SPACES
OPEN SPACE	MINIMUM	3,830 S.F.	7,783 S.F.	YES	+3,953 S.F.
50% STREET YARD OPEN SPACE	MINIMUM	1,915 S.F.	3,878 S.F.	YES	+1,963 S.F.

- ### GENERAL NOTES
- ALL DIMENSIONS AND CURB RADII ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - ALL CURB SHALL BE 86.12 CURB AND GUTTER UNLESS OTHERWISE NOTED.
  - SIDEWALK IN FRONT OF THE BUILDING SHALL BE COMBINATION SIDEWALK AND CURB AND GUTTER AND SHALL BE MONOLITHICALLY CAST ACCORDING TO THE DETAIL.
  - ALL RADII ARE 3.0' UNLESS OTHERWISE NOTED.
  - ALL SITE SIGNAGE SHALL BE IN CONFORMANCE WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) STANDARDS, LATEST EDITION.
  - PARKING SPACES SHALL BE ANGLED AT 90° TO THE ASSOCIATED DRIVE AISLE UNLESS OTHERWISE NOTED.
  - DETECTABLE WARNING STRIPS WITH TRUNCATED DOMES ON ALL CURB RAMPS SHALL CONSIST OF CONTRASTING COLOR TO ADJACENT PAVEMENT COLOR PER IDOT REGULATIONS, PREFERRED COLOR IS RED (FEDERAL COLOR STANDARD 30166)
  - REFER TO ARCHITECTURAL AND SIGNAGE PLANS FOR ANY ADDITIONAL SITE SIGNAGE AND PAVEMENT MARKINGS.
  - ALL TRAFFIC SIGNS SHALL BE INSTALLED AT 7' HIGH MEASURED FROM THE GROUND ELEVATION TO THE BOTTOM OF SIGN.

- ### PROPOSED SIGNAGE NOTE
- ALL SIGNAGE TO BE PERMITTED SEPARATELY
- ### PAVEMENT MARKING AND SIGN LEGEND
- (1) R7-8 HANDICAP PARKING SIGN (12"X18") WITH \$250 FINE
  - (2) R1-1 STOP SIGN (30"X30")
  - (3) R5-1 DO NOT ENTER SIGN (30"X30")
  - (4) R3-5 RIGHT TURN ONLY SIGN (30"X36")
  - (5) R3-2 NO LEFT TURN SIGN (36"X36")
  - (6) R4-7A KEEP RIGHT SIGN (24"X30")
  - (A) HANDICAP SYMBOL PER LATEST ADA STANDARDS
  - (B) 4" SOLID YELLOW AT 24° C-C AT 45 DEGREES
  - (C) 4" SOLID YELLOW STRIPE (TYP)
  - (D) 24" WIDE WHITE STOP BAR
  - (E) CROSSWALK - STAMPED CONCRETE BRICK PATTERN SEE ARCHITECTURAL PLANS FOR PATTERN AND COLOR
  - (F) HANDICAP RAMP WITH DEPRESSED CURB AND DETECTABLE WARNING (IF REQUIRED) IN ACCORDANCE WITH CURRENT ADA REQUIREMENTS

### SITE DEVELOPMENT SUMMARY

TOTAL SITE AREA = 3.341± ACRES
<b>LOT 1</b>
AREA = 2,008 ACRES
PROPOSED HOTEL = 48,660 GSF = 122 ROOMS
PARKING REQUIRED = 1.25 SPACES / ROOM = 153 SPACES
REGULAR PARKING PROVIDED = 120 SPACES
HANDICAP PARKING PROVIDED = 5 SPACES
TOTAL PARKING PROVIDED = 125 SPACES
<b>LOT 2</b>
AREA = 0.878 ACRES
PROPOSED RESTAURANT = 2,350 GSF
PARKING REQUIRED = 10 SPACES / 1,000 SF = 24 SPACES
REGULAR PARKING PROVIDED = 54 SPACES
HANDICAP PARKING PROVIDED = 3 SPACES
TOTAL PARKING PROVIDED = 57 SPACES
<b>LOT 3</b>
AREA = 0.455 ACRES
PROPOSED WET BOTTOM DETENTION BASIN

### SITE PAVING LEGEND

	HEAVY DUTY ASPHALT PAVING 2.0" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, N50 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, IL-19, N50 8" AGGREGATE BASE COURSE, TYPE B (IDOT GRAD CA-6)
	LIGHT DUTY ASPHALT PAVING 2.0" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, N50 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, IL-19, N50 10" AGGREGATE BASE COURSE, TYPE B (IDOT GRAD CA-6)
	CONCRETE PAVEMENT 8" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	STAMPED CONCRETE PAVEMENT 8" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	SIDEWALK PAVEMENT 5" P.C. CONCRETE PAVEMENT (THICKEN TO 8" AT DRIVEWAYS) 4" AGGREGATE BASE (CA-6)

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS  
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SHEET TITLE:  
**SITE DIMENSIONAL AND PAVING PLAN  
HOTEL AND DRIVE-THROUGH RESTAURANT**

PROJECT:  
**PROPOSED WOODSPRING SUITES HOTEL  
2751 & 2761 OGDEN AVENUE, DOWNERS GROVE, IL 60515**

PROJ. MGR.: OP  
 DRAWN BY: OP  
 FIRST ISSUE DATE: 01-04-2021  
 SCALE: 1"=30'  
**SHEET NO.  
C3.1**

PROJ. NUMBER:  
 19006

Capacity Analysis Summary Sheets  
Alternate Plan Projected Weekday Morning Peak Hour Conditions

HCM 6th TWSC  
1: Walnut Avenue & Ogden Avenue

08/30/2021

Intersection						
Int Delay, s/veh	4.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↖	↑↑	↖	↖
Traffic Vol, veh/h	2008	78	47	1005	101	45
Future Vol, veh/h	2008	78	47	1005	101	45
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	65	0
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	4	0	0	4	3	0
Mvmt Flow	2256	88	53	1129	113	51

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	2345	0	2972
Stage 1	-	-	-	-	2301
Stage 2	-	-	-	-	671
Critical Hdwy	-	-	4.1	-	6.86
Critical Hdwy Stg 1	-	-	-	-	5.86
Critical Hdwy Stg 2	-	-	-	-	5.86
Follow-up Hdwy	-	-	2.2	-	3.53
Pot Cap-1 Maneuver	-	-	311	-	~ 3 *263
Stage 1	-	-	-	-	219
Stage 2	-	-	-	-	467
Platoon blocked, %	-	-	1	-	1
Mov Cap-1 Maneuver	-	-	310	-	~ 3 *263
Mov Cap-2 Maneuver	-	-	-	-	129
Stage 1	-	-	-	-	218
Stage 2	-	-	-	-	387

Approach	EB	WB	NB
HCM Control Delay, s	0	0.8	85.9
HCM LOS			F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	129	263	-	-	310	-
HCM Lane V/C Ratio	0.88	0.192	-	-	0.17	-
HCM Control Delay (s)	114.4	21.9	-	-	19	-
HCM Lane LOS	F	C	-	-	C	-
HCM 95th %tile Q(veh)	5.6	0.7	-	-	0.6	-

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

# HCM 6th TWSC

## 2: Proposed Access Drive & Ogden Avenue

11/09/2020

Intersection						
Int Delay, s/veh	0.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	2028	25	0	1052	0	17
Future Vol, veh/h	2028	25	0	1052	0	17
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	Free	-	None	-	Stop
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	98	95	95
Heavy Vehicles, %	4	0	0	4	0	0
Mvmt Flow	2135	26	0	1073	0	18

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	22.7
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	221	-	-
HCM Lane V/C Ratio	0.081	-	-
HCM Control Delay (s)	22.7	-	-
HCM Lane LOS	C	-	-
HCM 95th %tile Q(veh)	0.3	-	-

### HCM 6th TWSC 3: Walnut Avenue & Proposed Access Drive

11/09/2020

Intersection						
Int Delay, s/veh	2.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	0	50	96	0	41	84
Future Vol, veh/h	0	50	96	0	41	84
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	0	0	2	0	0	0
Mvmt Flow	0	53	101	0	43	88

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	275	101	0	0	101	0
Stage 1	101	-	-	-	-	-
Stage 2	174	-	-	-	-	-
Critical Hdwy	6.4	6.2	-	-	4.1	-
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	-	-	2.2	-
Pot Cap-1 Maneuver	719	960	-	-	1504	-
Stage 1	928	-	-	-	-	-
Stage 2	861	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	697	960	-	-	1504	-
Mov Cap-2 Maneuver	697	-	-	-	-	-
Stage 1	900	-	-	-	-	-
Stage 2	861	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9	0	2.4
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	960	1504
HCM Lane V/C Ratio	-	-	0.055	0.029
HCM Control Delay (s)	-	-	9	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1

Capacity Analysis Summary Sheets  
Alternate Plan Projected Weekday Evening Peak Hour Conditions

# HCM 6th TWSC

## 1: Walnut Avenue & Ogden Avenue

08/30/2021

Intersection						
Int Delay, s/veh	1.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑		↖	↑↑	↖	↖
Traffic Vol, veh/h	1314	107	62	1797	68	38
Future Vol, veh/h	1314	107	62	1797	68	38
Conflicting Peds, #/hr	0	1	1	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	125	-	65	0
Veh in Median Storage, #	0	-	-	0	1	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	2	4	0	1	4	0
Mvmt Flow	1383	113	65	1892	72	40

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	1497	0	2517
Stage 1	-	-	-	-	1441
Stage 2	-	-	-	-	1076
Critical Hdwy	-	-	4.1	-	6.88
Critical Hdwy Stg 1	-	-	-	-	5.88
Critical Hdwy Stg 2	-	-	-	-	5.88
Follow-up Hdwy	-	-	2.2	-	3.54
Pot Cap-1 Maneuver	-	-	*818	-	*~ 24
Stage 1	-	-	-	-	*508
Stage 2	-	-	-	-	*284
Platoon blocked, %	-	-	1	-	1
Mov Cap-1 Maneuver	-	-	*817	-	*~ 22
Mov Cap-2 Maneuver	-	-	-	-	*166
Stage 1	-	-	-	-	*508
Stage 2	-	-	-	-	*261

Approach	EB	WB	NB
HCM Control Delay, s	0	0.3	31.4
HCM LOS			D

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	166	544	-	-	* 817	-
HCM Lane V/C Ratio	0.431	0.074	-	-	0.08	-
HCM Control Delay (s)	42.2	12.1	-	-	9.8	-
HCM Lane LOS	E	B	-	-	A	-
HCM 95th %tile Q(veh)	2	0.2	-	-	0.3	-

**Notes**  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

HCM 6th TWSC  
2: Proposed Access Drive & Ogden Avenue

11/09/2020

Intersection						
Int Delay, s/veh	0.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑		↑
Traffic Vol, veh/h	1326	26	0	1859	0	17
Future Vol, veh/h	1326	26	0	1859	0	17
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	Free	-	None	-	Stop
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	2	0	0	1	0	0
Mvmt Flow	1396	27	0	1957	0	18

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	14.7
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	388	-	-
HCM Lane V/C Ratio	0.046	-	-
HCM Control Delay (s)	14.7	-	-
HCM Lane LOS	B	-	-
HCM 95th %tile Q(veh)	0.1	-	-

### HCM 6th TWSC 3: Walnut Avenue & Proposed Access Drive

11/09/2020

Intersection						
Int Delay, s/veh	2.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	0	37	69	0	35	134
Future Vol, veh/h	0	37	69	0	35	134
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	0	0	2	0	0	3
Mvmt Flow	0	39	73	0	37	141

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	288	73	0	0	73	0
Stage 1	73	-	-	-	-	-
Stage 2	215	-	-	-	-	-
Critical Hdwy	6.4	6.2	-	-	4.1	-
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	-	-	2.2	-
Pot Cap-1 Maneuver	707	995	-	-	1540	-
Stage 1	955	-	-	-	-	-
Stage 2	826	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	689	995	-	-	1540	-
Mov Cap-2 Maneuver	689	-	-	-	-	-
Stage 1	930	-	-	-	-	-
Stage 2	826	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.8	0	1.5
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	995	1540
HCM Lane V/C Ratio	-	-	0.039	0.024
HCM Control Delay (s)	-	-	8.8	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0.1

DRAFT

VILLAGE OF DOWNERS GROVE  
PLAN COMMISSION MEETING

September 13, 2021, 7:00 P.M.

Chairman Pro tem Maurer called the September 13, 2021 meeting of the Downers Grove Plan Commission to order at 7:00 p.m. and led the Plan Commissioners and public in the recital of the Pledge of Allegiance.

**ROLL CALL:**

**PRESENT:** Chairman Pro tem Maurer, Commissioners Boyle, Majauskas, Patel, Rector, Toth

**ABSENT:** Chairman Rickard, Commissioners Dmytryszyn, Johnson

**STAFF:** Planning Manager Jason Zawila and Development Planner Gabby Baldassari

**OTHERS**

**PRESENT:** Petitioner, Mr. Duffey Phelps, One Walker Avenue, Clarendon Hills; Traffic Consultant Mr. Michael Werthmann, with KLOA, Civil engineering consultant, Mr. Oz Pastrona with CivWorks; Mr. Scott Richards, 1130 Warren Avenue, Downers Grove; Michael Cassa, Downers Grove Economic Development Corporation.

**APPROVAL OF MINUTES**

Chairman Pro tem Maurer entertained a motion to approve the minutes.

**MINUTES OF THE JUNE 7, 2021 PLAN COMMISSION MEETING WERE APPROVED ON MOTION BY COMMISSIONER RECTOR. SECOND BY COMMISSIONER BOYLE. MOTION PASSED BY VOICE VOTE OF 6-0.**

**MINUTES OF THE JUNE 14, 2021 PLAN COMMISSION MEETING WERE APPROVED ON MOTION BY COMMISSIONER BOYLE. SECOND BY COMMISSIONER TOTH. MOTION PASSED BY VOICE VOTE OF 5-0. (COMMISSIONER MAJAUSKAS ABSTAINS)**

**PUBLIC HEARING**

Chairman Pro tem Maurer explained the protocol for the public hearing process and swore in those individuals that would be speaking during the public hearing.

**FILE 21-PLC-0015: A petition seeking approval of a Planned Unit Development, map amendment from O-R-M, Office-Research Manufacturing to B-3, General Services and Highway Business, a Special-Use for a drive-through, and a Final Plat of Subdivision to construct a restaurant and a hotel. The property is zoned O-R-M, Office Research-Manufacturing. The property is located on the southeast corner of Walnut Avenue and**

DRAFT

**Ogden Avenue (PIN 08-01-304-031). Chicago WSS Developer II, Petitioner and Tag Land DGC, LLC Owner.**

Petitioner, Mr. Duffey Phelps, One Walker Avenue, Clarendon Hills, Suite 300, shared that he is Vice President of Development with Holladay Properties, and was the developer for the Burlington Station project. A background of Holladay Properties followed. The proposed 3.4-acre site is planned to be subdivided into three separate lots – the eastern portion will be for the proposed hotel, the west portion for the restaurant and the southwest property will have the detention pond. Mr. Phelps discussed that the necessary parking requirements and cross access easements were considered in order to meet the Village’s ordinance for this development. Phase 1 will include the development of the 122 room Wood Springs Hotel and necessary infrastructure, traffic lanes off Walnut Street, stormwater facilities, and public improvements. He did meet with the Downers Grove Park District to review the plans and to have connectivity to the park district site.

Mr. Phelps stated that Phase 2 will be the development of a restaurant but two plans exist: the first option is for a sit-down restaurant with a deviation request of 184 parking spaces, when the Zoning Ordinance requires 187 spaces. The reason for the reduced number of parking spaces was due to the nearby south wetland which encroaches onto the property and he was trying to limit the impact to it, yet keep the drive aisle for emergency vehicle access. The second option calls for a drive-through restaurant with a double drive-through lane. A special use is being requested for it. For this plan there will be 182 parking spaces while the ordinance states 165 spaces are needed; so a surplus exists.

Mr. Phelps discussed that the Wood Springs Hotel is a fast growing chain in the U.S. Users will be extended stay travelers, business travelers, and (traveling) sports teams. BRR Architects is the architect design firm for the project. Materials on the hotel’s facade will include stone “bump outs” and a canopy. The site is also a catalyst site in the village’s Comprehensive Plan. Renderings of the building and floor plans followed. Mr. Phelps elaborated that there are interested parties for the restaurant and the site is located in a good location near the expressways and park district. A list of how the development met the requirements of the village’s Comprehensive Plan followed. He believes the project fits well as a planned unit development site and makes the site cohesive.

Traffic Consultant, Mr. Michael Whitman, with KLOA and civil engineering consultant Mr. Oz Pastrona with CivWorks were introduced.

Commissioners confirmed with the civil engineer that the utilities from Phase 2 will be delivered to the future restaurant pad, the 184 shared parking spaces will accommodate the full hotel, the hotel’s room rates (differ seasonally), the “typical” guest, and how the sports park will play into the hotel, its amenities, etc. Mr. Phelps explained the brand can provide a market needs study, if necessary. Per Commissioner Majauskas’ question, the hotel would accommodate traveling youth sports clubs. It would not be providing housing for the homeless or evicted, etc. Commissioner Majauskas stated she would have preferred to see some studies provided and for the petitioner to explain how the development was a gateway that included retail, when she did not see any retail being planned. Per a question, Mr. Phelps explained that Holladay Properties does hire third-party management companies to manage their hotel – JMP Asset Management and Nationwide Management. He confirmed that the company does have corporate relationships to allow for flexibility per week.

Chairman Pro tem Maurer invited public comment.

## DRAFT

Mr. Michael Cassa, with the Downers Grove Economic Development Corporation, explained to commissioners that the village is known as an office community in the Chicagoland area with three office parks – Esplanade on Butterfield, Highland Landmark on Butterfield, and the Corridors, which is directly across the street from the proposed site. Mr. Cassa believed the applicant would draw its business from the Corridors, since there is no close hotel in the vicinity. Regarding a future sports park, Mr. Cassa shared that sports tourism is the fastest growing tourism segment in the U.S. and the proposed hotel/restaurant is to attract sports tournament to the area. Details followed.

Mr. Scott Richards, 1130 Warren Avenue, inquired if such facility would attract truck traffic and whether it would impact Ogden Avenue; whether the proposed retention pond would be dry or wet; whether a coffee/breakfast bar would be offered; and how many staff would be on-site at night. Lastly, Mr. Richards commented that retail for the site would not be a good fit since the industry was struggling. The proposed use was a good choice.

Ms. Gabby Baldassari, Development Planner for the village, summarized the request before the commissioners, presented the existing conditions of the area, and said the entire property would be split into three lots containing the hotel, restaurant, and stormwater retention, respectively. Two options for the restaurant were being proposed: Option one involved a fast casual dine-in, or option 2 would involve a drive-through (requiring a special use). Elevations for the hotel followed. Parking for both options would be shared across Lots 1 and 2, but Option 2 would be short by three parking spaces. A petition for a deviation from the zoning ordinance was being sought by the applicant to provide 184 spaces where 187 would be required. A review of the easements followed as well as the recommended site objectives under the village's Comprehensive Plan (a Catalyst D-1). Staff believed the criteria for the proposal was met and referred to the motion in staff's report.

Commissioner questions centered on the purpose of subdividing the third lot and that the commission was not voting on two options but was providing one vote, leaving the applicant the option that works best for them.

Addressing the public questions above, Mr. Phelps explained that semi-trucks will not allowed to be parked in the hotel lot and was not designed to allow that type of parking, three staff work overnight, and the detention point will be wet with wetland plantings on the bottom with surrounded landscaping. A coffee bar and grab and grow breakfast will be offered to guests. Mr. Phelps extended appreciation to the commissioners for their consideration and was looking forward to growing the hotel's presence in the community.

Chairman Pro tem Maurer closed public comments.

Chairman Pro tem Maurer voiced support for the project, believed hotels were great contributors for a community, and he had confidence in the Wood Springs brand. He believed the use was appropriate for the site. Other commissioner comments included support for the project. The village could benefit from the proposal. Commissioner Majauskas voiced the fact that the commission was not following the village's Comprehensive Plan and questioned why have the plan, citing Page 118 of the Comprehensive Plan and its recommendation for the site. Planning Manger Zawila stated that for the record Commissioner Majauskas's reference was incorrect.

## DRAFT

Commissioner Majauskas pointed out that after some phone calls, the Lisle Hotels were at 10% capacity and struggling. The Red Roof Inn was selling the pads as a transient apartment complex and she did not see the need for another hotel. The site was not zoned for it, it was in a wetlands area, there was no marketing study produced by the applicant. The proposal did not make sense for the site.

Chairman Pro tem Maurer explained that the hotels will rebound and the need will always be there. Another commissioner suggested that the applicant produce the marketing study before he appears before the village council, as it would provide good feedback. Another commissioner understood the proposed hotel was a premium mid-tier product which was different from a Red Roof Inn and the customer base was a very specific use type that would draw customers into such type of hotel. Examples followed on the type of customer that would use such a hotel.

Another commissioner, in reviewing the Village's Comprehensive Plan, believed the proposal fit the site and offered an ancillary service to the businesses to the north. Inn Town Suites was booked which conveyed to him that a market does exist. The petitioner was a long-term holder and did not flip properties. For those reasons, he supported the proposal. It was also stated that there was a need for such hotels because when he travels he prefers a room with a kitchenette, takes his family to sports tournaments, and agrees that the village is missing a significant revenue stream.

**WITH RESPECT TO FILE 21-PLC-0015 AND BASED ON THE PETITIONER'S SUBMITTAL, THE STAFF REPORT, THE TESTIMONY PRESENTED, AND HAVING MET THE STANDARDS OF APPROVAL FOR A PLANNED UNIT DEVELOPMENT, ACCOMPANYING MAP AMENDMENT, SPECIAL USE AND PLAT OF SUBDIVISION AS REQUIRED BY THE VILLAGE OF DOWNERS GROVE ZONING ORDINANCE AND IT BEING IN THE PUBLIC INTEREST, COMMISSIONER BOYLE MADE A MOTION THAT THE PLAN COMMISSION RECOMMEND TO THE VILLAGE COUNCIL APPROVAL OF FILE 21-PLC-0015, SUBJECT TO THE FOLLOWING CONDITIONS:**

- 1. THE SPECIAL USE, PLANNED UNIT DEVELOPMENT AND MAP AMENDMENT SHALL SUBSTANTIALLY CONFORM TO THE STAFF REPORT, RENDERINGS, ARCHITECTURE PLANS PREPARED BY HOLLADAY PROPERTIES, DATED DECEMBER 14, 2021, ENGINEERING PLANS PREPARED BY CIVWORKS CONSULTING LLC, DATED AUGUST 30, 2021, LANDSCAPE PLANS PREPARED BY LG WORKSHOP LLC, DATED JULY 2, 2021, AND TRAFFIC PLANS PREPARED BY KLOA DATED AUGUST 30, 2021 EXCEPT AS SUCH PLANS MAY BE MODIFIED TO CONFORM TO THE VILLAGE CODES AND ORDINANCES;**
- 2. THE PETITIONER SHALL SUBDIVIDE THE LOT INTO THE THREE LOTS OF RECORD PURSUANT TO SECTION 20.5 OF THE SUBDIVISION ORDINANCE PRIOR TO THE ISSUANCE OF ANY SITE DEVELOPMENT OR BUILDING PERMITS;**
- 3. THE BUILDING ELEVATIONS FOR LOT 2 SHALL BE CONSISTENT AND COMPATIBLE WITH THE HOTEL BUILDING DESIGN;**
- 4. A PERPETUAL CROSS ACCESS AND PARKING EASEMENT IS PROVIDED BETWEEN LOTS 1 AND 2;**

## DRAFT

5. **A PERPETUAL CROSS ACCESS EASEMENT IS PROVIDED TO THE PROPERTY IMMEDIATELY SOUTH OF THE PLANNED UNIT DEVELOPMENT;**
6. **A PEDESTRIAN EASEMENT IS PROVIDED ON LOT 1 FOR THE BENEFIT OF PUBLIC ACCESS TO THE PROPERTY IMMEDIATELY SOUTH OF THE PLANNED UNIT DEVELOPMENT;**
7. **APPROVAL OF PARTICIPATION IN THE COUNTY WETLAND BANKING PROGRAM IS REQUIRED PRIOR TO THE ISSUANCE OF ANY SITE DEVELOPMENT OR BUILDING PERMITS;**
8. **PARKING SHALL BE UTILIZED ACROSS BOTH LOTS 1 AND 2, FOR THE PLANNED UNIT DEVELOPMENT. IF AT ANY POINT THE PROPERTIES ARE UNDER SEPARATE OWNERSHIP, AN OFF-SITE PARKING AGREEMENT MUST BE COMPLETED IN A FORM APPROVED BY THE VILLAGE ATTORNEY. THE AGREEMENT MUST PROVIDE FOR THE PERPETUAL USE OF PARKING ACROSS BOTH LOTS;**
9. **PAYMENT OF A FEE-IN-LIEU OF PLACEMENT OF THE SIDEWALK AS REQUIRED BY VILLAGE ORDINANCE. THE FEE WILL BE DETERMINED BASED ON THE SQUARE FOOTAGE AREA OF THE SIDEWALK, WHICH WILL BE LOCATED IMMEDIATELY SOUTH OF THE WALNUT AVENUE ACCESS DRIVE TO THE SOUTHERN PROPERTY LINE. PAYMENT MUST BE MADE PRIOR TO THE ISSUANCE OF ANY SITE DEVELOPMENT OR BUILDING PERMITS; AND**
10. **ALL SIGNAGE SHALL CONFORM TO THE VILLAGE'S SIGN ORDINANCE.**

**SECOND BY COMMISSIONER RECTOR. ROLL CALL:**

**AYE: BOYLE, RECTOR, PATEL, TOFF, MAURER  
NAY: MAJASKAS**

**MOTION PASSED. VOTE: 5-1**

Planning Manager Zawila reported that the Maple and Washington project was approved last month by Village Council.

**THE MEETING WAS ADJOURNED AT 8:14 P.M. UPON MOTION BY COMMISSIONER MAJASKAS. SECOND BY COMMISSIONER BOYLE. A VOICE VOTE FOLLOWED AND THE MOTION PASSED UNANIMOUSLY.**

/s/ Celeste K. Weilandt  
Recording Secretary  
(As transcribed by MP-3 audio)