

**VILLAGE OF DOWNERS GROVE
REPORT FOR THE VILLAGE COUNCIL MEETING
DECEMBER 6, 2011 AGENDA**

SUBJECT:	TYPE:	SUBMITTED BY:
Special Use for 160 Ogden Avenue	<ul style="list-style-type: none"> Resolution ✓ Ordinance Motion Discussion Only 	Tom Dabareiner, AICP Community Development Director

SYNOPSIS

An ordinance has been prepared for special use approval for a drive-through addition to an existing restaurant at 160 Ogden Avenue.

STRATEGIC PLAN ALIGNMENT

The Goals for 2011 to 2018 include *Strong, Diverse Local Economy*.

FISCAL IMPACT

N/A

UPDATE & RECOMMENDATION

This item was discussed at the August 9, 2011 Village Council meeting. Staff recommends approval on the December 6, 2011 Active Agenda. During the meeting, the Village Council expressed concerns with regard to the safety of the proposed drive-through location. The petitioner has revised the site plan to address the Council’s comments. The revisions are as follows:

- The drive through lane adjacent to Ogden Avenue is now 5.18 feet from the sidewalk. The original proposal indicated a 2.5-foot setback from the sidewalk. This modification provides additional room for landscaping and buffering
- The drive through lane adjacent to Cumnor Road would be 4.2 feet east of the existing public sidewalk. The original proposal indicated a 2.5-foot setback from the sidewalk.
- Barrier curbs (approximately 250 feet) would be constructed to provide additional protection for pedestrians on the adjacent public walks.
- Additional landscaping, in the form of a continuous hedge row, is proposed for the area between the drive through lane and the adjacent sidewalks. The landscaping would be 18-24 inches at planting and would have a mature height of 3-6 feet with a spread of 3-5 feet. Typical truck, van and SUV headlight heights range from 26-41 inches.
- The proposed drive through exit point would be relocated east to eliminate potential conflicts between the existing handicap parking spaces and the exit lane.
- The proposed entry to the drive-through would be relocated slightly north to accommodate additional setback.
- The relocation of the entry and exit points would result in the elimination of eight additional parking spaces. The shopping center will have 410 parking spaces where 388 are required.
- Fire sprinkler and fire detection systems are proposed for the entire building.
- The petitioner will be required to complete an administrative lot reconfiguration to reconfigure the property lines so that all construction associated with the drive-through lane is located on a single parcel.

BACKGROUND

The petitioner is requesting a Special Use to construct a drive-through window onto an existing Panera Bread restaurant located at 160 Ogden Avenue. The property is located at the northeast corner of Ogden Avenue and Cumnor Road and is zoned B-3 General Services and Highway Business.

The existing restaurant sits at a 45-degree angle to both Ogden Avenue and Cumnor Road. The petitioners are proposing a 550 square foot one-story addition to the southwest façade which would house the drive-through window. A 12-foot wide drive-through lane would begin at the northwest corner of the site with traffic flowing counter-clockwise around the building and exiting to the east of the building. A menu board and preview board would face Cumnor Road and be equipped with automatic volume controls.

The petitioner will be removing five parking spaces but will not adversely affect the availability of parking which will still meet the Village's parking requirements per Section 28.1410. The petitioner will be removing eight trees and numerous shrubs. New landscaping with shrubs and ornamental grasses will be required around the proposed drive-through lane.

The proposal meets the bulk requirements of the Zoning Ordinance (Section 28.1100) as shown in the table below:

Zoning Requirements	Required	Provided
North Setback (Side)	0 ft	58 ft
East Setback (Rear)	0 ft	58 ft
South Setback (Front)	75 ft (from centerline of Ogden)	75 ft
West Setback (Front)	25 ft	31 ft
Building Height	60 ft	16 ft
Parking Spaces	388	418
Floor Area Ratio	0.75 (max)	0.15
Open Space (Total / Front Yard)	3,240 sq ft / 1620 sq ft	5,899 sq ft / 5,899 sq ft

Vehicular access to the Panera property is through the existing entrances to the Downers Grove Shopping Center further east along Ogden Avenue and further north along Cumnor Road. No new curb cuts are proposed. The existing utilities will not be impacted by the proposal while new stormwater inlets will be installed in conjunction with the drive-through lane. The existing sidewalks along Ogden Avenue and Cumnor Road will not be impacted.

The Plan Commission considered the petition at their July 11, 2011 meeting. The Plan Commission expressed concerns regarding the proximity of the drive-through lane to the sidewalks along Ogden Avenue and Cumnor Road and the potential for headlights to shine into the adjacent rights-of-way. The petitioner addressed these concerns as follows: the installation of a nine-inch barrier curb along portions of the drive through lane to provide an additional safety barrier and the installation of an evergreen hedge row to eliminate headlights shining into adjacent rights-of-way. Staff believes these modifications will address the Plan Commission's concerns.

The Plan Commission found the proposal meets the standards of approval for a special use as noted in Section 28.1902 of the Zoning Ordinance. The use is desirable, will not be detrimental to the health, welfare, or property values in the vicinity, will comply with the regulations of the B-3 zoning district and is

listed as an allowable special use in the B-3 zoning district. Based on their analysis, the Plan Commission recommended approval of the special use. Staff concurs.

ATTACHMENTS

Aerial Map

Ordinance

Staff Report with attachments dated July 11, 2011

Minutes of the Plan Commission Hearing dated July 11, 2011

Revised plans dated October 24, 2011

VILLAGE OF DOWNERS GROVE
COUNCIL ACTION SUMMARY

INITIATED: Applicant **DATE:** December 6, 2011
(Name)

RECOMMENDATION FROM: _____ **FILE REF:** PC-18-11
(Board or Department)

NATURE OF ACTION:

- Ordinance
- Resolution
- Motion
- Other

STEPS NEEDED TO IMPLEMENT ACTION:

Motion to Adopt "AN ORDINANCE AUTHORIZING A SPECIAL USE FOR 160 OGDEN AVENUE TO PERMIT A DRIVE-THROUGH WINDOW", as presented.

SUMMARY OF ITEM:

Adoption of the attached ordinance will authorize a special use to permit a drive through window at 160 Ogden Avenue.

RECORD OF ACTION TAKEN:

ORDINANCE NO. _____

**AN ORDINANCE AUTHORIZING A SPECIAL USE FOR 160 OGDEN AVENUE
TO PERMIT A DRIVE-THROUGH WINDOW**

WHEREAS, the following described property, to wit:

The south 192.00 feet of the west 221.00 feet of Lot 2, except the north 70 feet of the west 26 feet thereof, and except the south 26 feet of the east 94 feet thereof, in James A. McCormick Subdivision being a subdivision of all that part of the east ½½ of the northwest ¼¼ of Section 4, Township 38 North, Range 11, east of the Third Principal Meridian, lying south of the property line of 41st Street and north of the north property line of Ogden Avenue (except the east 40 feet and west 33 feet thereof) in DuPage County, Illinois (containing 0.88 acres, more or less).

Commonly known as 160 Ogden Avenue, Downers Grove, IL (PIN 09-04-112-038).

(hereinafter referred to as the "Property") is presently zoned in the "*B-3, General Services and Highway Business District*" under the Comprehensive Zoning Ordinance of the Village of Downers Grove; and

WHEREAS, the owner of the Property has filed with the Plan Commission, a written petition conforming to the requirements of the Zoning Ordinance, requesting that a Special Use per Section 28.609 of the Zoning Ordinance be granted to allow a drive-through window for an existing restaurant; and,

WHEREAS, such petition was referred to the Plan Commission of the Village of Downers Grove, and said Plan Commission has given the required public notice, has conducted a public hearing on July 11, 2011, respecting said petition and has made its findings and recommendations, all in accordance with the statutes of the State of Illinois and the ordinances of the Village of Downers Grove; and,

WHEREAS, the Plan Commission has recommended approval of the Special Use, subject to certain conditions; and,

WHEREAS, the Village Council finds that the evidence presented in support of said petition, as stated in the aforesaid findings and recommendations of the Plan Commission, is such as to establish the following:

1. The proposed use at that particular location requested is necessary or desirable to provide a service or a facility which is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.
2. The proposed use will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, or general welfare of persons residing or working in the vicinity or injurious to property values or improvements in the vicinity.
3. The proposed use will comply with the regulations specified in this Zoning Ordinance for the district in which the proposed use is to be located.
4. The proposed use is one of the special uses specifically listed for the district in which it is to be located and, if approved with restrictions as set forth in this ordinance, will comply with the provisions of the Downers Grove Zoning Ordinance regulating this

Special Use.

NOW, THEREFORE, BE IT ORDAINED by the Council of the Village of Downers Grove, in DuPage County, Illinois, as follows:

SECTION 1. That Special Use of the Property is hereby granted to allow a drive-through window for an existing restaurant at 160 Ogden Avenue within the B-3 zoning district.

SECTION 2. This approval is subject to the following conditions:

1. The Special Use shall substantially conform to the staff report dated July 11, 2011 and with preliminary engineering and landscape plans prepared by Core States Group, dated May 4, 2011 and revised on June 16, 2011; architectural plans prepared by Frederick J. Goglia, dated January 4, 2011; and sign drawings prepared by Mandeville Sign, dated July 15, 2010 and revised on June 20, 2011 except such plans may be modified to conform to Village Codes and Ordinances.
2. A description of best management practices for stormwater that are incorporated into the site design and how they will function shall be prepared and submitted. The applicable design criteria such as soil type, vegetation, and land cover conditions draining to the best management practice along with appropriate calculations shall be included.
3. The proposed addition shall have a manual and automatic fire detection system installed throughout in a manner acceptable to the Village. All areas of the building shall be protected.
4. The proposed addition shall have a complete automatic sprinkler system installed throughout in a manner acceptable to the Village. All areas of the building shall be protected.
5. All proposed signage shall comply with the Village's Sign Ordinance.
6. A minimum three-foot wide landscaping bed, containing shrubs or ornamental grasses, shall be installed between the drive-through lane and adjacent sidewalks for the entire length of the drive-through lane.
7. The menu board shall be equipped with automatic volume control.

SECTION 3. The above conditions are hereby made part of the terms under which the Special Use is granted. Violation of any or all of such conditions shall be deemed a violation of the Village of Downers Grove Zoning Ordinance, the penalty for which may include, but is not limited to, a fine and or revocation of the Special Use granted herein.

SECTION 4. That all ordinances or parts of ordinances in conflict with the provisions of this ordinance are hereby repealed.

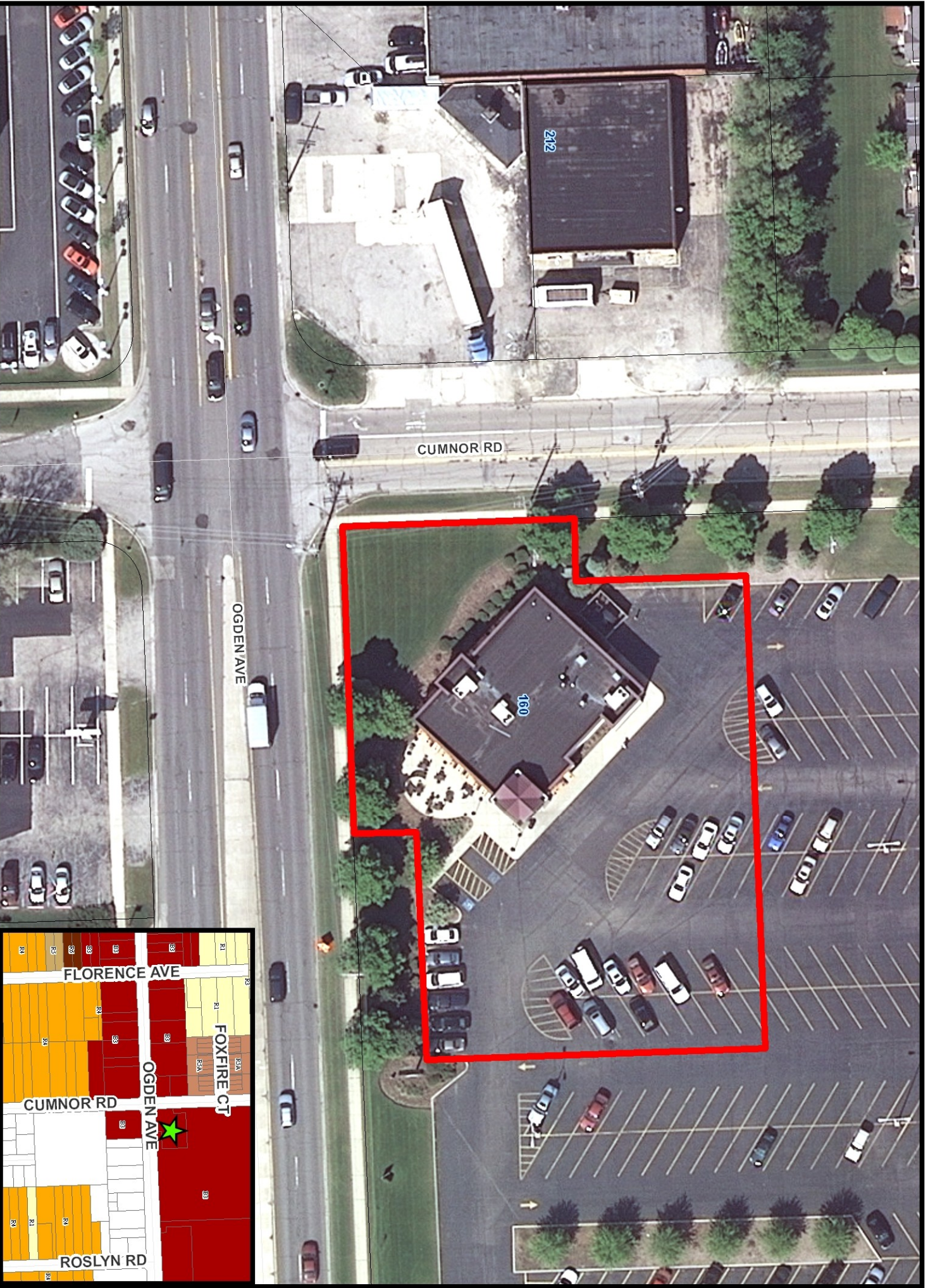
Mayor

Passed:

Published:

Attest: _____

Village Clerk

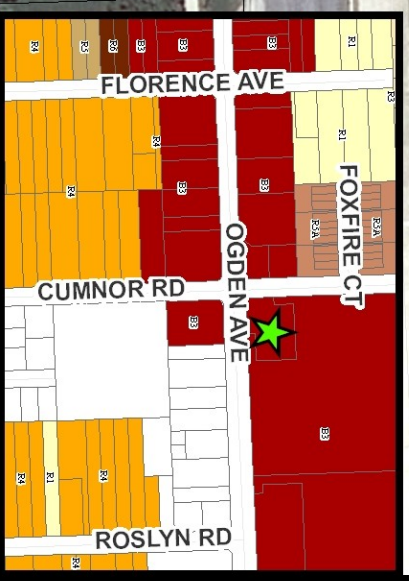


CUMNOR RD

OGDEN AVE

212

160



FLORENCE AVE

FOXFIRE CT

CUMNOR RD

OGDEN AVE

ROSLYN RD

0 10 20 30 40 Feet

160 Ogden Avenue Location Map





**VILLAGE OF DOWNERS GROVE
REPORT FOR THE PLAN COMMISSION
JULY 11, 2011 AGENDA**

SUBJECT:	TYPE:	SUBMITTED BY:
PC-18-11 160 Ogden Avenue	Special Use for a drive-through window	Stan Popovich, AICP Planner

REQUEST

The petitioner is requesting approval of a Special Use for a drive-through window at an existing restaurant located at 160 Ogden Avenue.

NOTICE

The application has been filed in conformance with applicable procedural and public notice requirements.

GENERAL INFORMATION

OWNER: J.J. Brothers, LLC
146 A Ogden Avenue
Downers Grove, IL 60515

APPLICANT: Panera, LLC
3630 South Geyer Road, Suite 100
Sunset Hills, MO 63127

PROPERTY INFORMATION

EXISTING ZONING: B-3, General Services and Highway Business
EXISTING LAND USE: Restaurant
PROPERTY SIZE: 0.74 acres (32,404 square feet)
PINS: 09-04-112-038

SURROUNDING ZONING AND LAND USES

	ZONING	FUTURE LAND USE
NORTH:	B-3, General Services and Highway Business	Commercial
SOUTH:	B-3, General Services and Highway Business	Commercial
EAST:	B-3, General Services and Highway Business	Commercial
WEST:	B-3, General Services and Highway Business	Commercial

ANALYSIS

SUBMITTALS

This report is based on the following documents, which are on file with the Department of Community Development:

1. Application/Petition for Public Hearing
2. Project Summary
3. Plat of Survey
4. Site Plan
5. Engineering Plans
6. Architectural Floor Plan and Elevations
7. Landscape Plan

PROJECT DESCRIPTION

The petitioner is requesting a special use for the addition of a drive-through window to the existing Panera Bread restaurant at 160 Ogden Avenue. The 0.74 acre parcel is located at the northeast corner of Ogden Avenue and Cumnor Road and is zoned B-3, General Services and Highway Business. The property consists of a one-story building that houses the restaurant. Access to the site is through the existing Downers Grove Shopping Center's parking lot with entrances further to the east along Ogden Avenue and further to the north along Cumnor Road.

The existing building sits at a 45-degree angle to both Ogden Avenue and Cumnor Road. The petitioners are proposing a 550 square foot one-story addition to the southwest façade which would house the drive-through window. The food preparation line will now extend to the drive-through window and new restrooms will be provided within the addition. The exterior of the building will be clad with EIFS (Exterior Insulation and Finishing System) and brick to match the existing building.

The 12-foot wide drive-through lane will begin at the northwest corner of the site and traffic will flow counter-clockwise around the building and exit in the parking lot east of the building. There are a total of eight stacking spaces from the drive-through window to the beginning of the drive-through lane. The preview and menu boards will be located between the drive-through lane and the building and face Cumnor Road.

The drive-through's hours of operation will be same as the existing restaurant, which are 6:30 A.M. through 9:30 P.M. The menu board will be equipped with automatic volume control that will adjust the outbound volume based on the outdoor, ambient noise level. Thus, in the evenings when the ambient noise decreases, the outbound volume on the menu board will decrease as well.

The proposed drive-through lane will eliminate five parking spaces. There will be no reduction in the number of handicap parking spaces provided. Currently, the shopping center and Panera Bread restaurant share a total of 423 parking spaces where 388 are required. The reduction of five spaces will not adversely affect the availability of parking.

The petitioner will be removing eight trees and numerous shrubs from the area that will be converted to a drive-through window and lane. As such, staff is requiring additional landscaping to be installed to offset the loss of this vegetation. In particular, the area to the west of the drive-through lane entrance will require additional landscaping. Additionally, ornamental grasses and shrubs will be required between the drive-through lane and adjacent sidewalks for the entire length of the drive through lane.

The proposal complies with the bulk requirements of the Zoning Ordinance as outlined in the table below:

Zoning Requirements	Required	Provided
North Setback (Side)	0 ft	58 ft
East Setback (Rear)	0 ft	58 ft
South Setback (Front)	75 ft (from centerline of Ogden)	75 ft
West Setback (Front)	25 ft	31 ft
Building Height	60 ft	16 ft
Parking Spaces	388	418
Floor Area Ratio	0.75 (max)	0.15
Open Space (Total / Front Yard)	3,240 sq ft / 1620 sq ft	5,899 sq ft / 5,899 sq ft

COMPLIANCE WITH FUTURE LAND USE PLAN

The Future Land Use Plan designates the property as Commercial. Staff believes the drive-through is consistent with the future land use designation of Commercial. As such, staff believes the proposal is consistent with the Future Land Use Plan.

COMPLIANCE WITH ZONING ORDINANCE

The property is zoned B-3, General Services and Highway Business. The proposed drive-through is a permitted Special Use in the district per Section 28.609 of the Zoning Ordinance. The proposed development meets all bulk zoning requirements including setbacks, open space, building height, parking and floor area ratio as shown in the table above. The proposed development also complies with the Special Use Standards of Approval. Staff believes the proposal is consistent with the Zoning Ordinance.

ENGINEERING/PUBLIC IMPROVEMENTS

Vehicular access to the Panera Bread restaurant is via the existing shopping center curb cuts on Cumnor Road and Ogden Avenue. The petitioner is not proposing to make any changes to the current access. Interior circulation will not be modified either as all circulation aisles will remain intact. The only change to the parking aisles will be the removal of five parking spaces from the lot. Two spaces will be removed from the parking area adjacent to Ogden Avenue where the drive-through exits. One space will be removed where the drive through entrance will be located. Two spaces will be removed in the westernmost double-wide parking row to provide for a clear entrance to the drive-through lane.

The existing utilities that service the building, including electric, gas, sanitary sewer and water, will not be impacted by the proposed improvements. Each service will remain in their same location. The Downers Grove Sanitary District has provided their conceptual approval to the proposed improvement.

The petitioner is proposing to install two stormwater inlets within the drive-through lane and associated piping to tie into the existing stormwater system. The petitioner will be required to comply with the Village's Stormwater Management Ordinance and the Village's Stormwater Best Management Practices.

The petitioner will not be impacting the existing sidewalks along Cumnor Road and Ogden Avenue. The existing dumpster and trash enclosure will remain in its current location and will not be modified.

PUBLIC SAFETY REQUIREMENTS

The Fire Prevention Division of the Fire Department has reviewed the proposed plans. Based upon the submittal, the Fire Prevention Division believes they have sufficient access to the site and building. As a result of the building addition, the entire building will be required to have manual and automatic detection

system installed throughout. A complete and automatic sprinkler system will also be required in the building.

NEIGHBORHOOD COMMENT

Notice was provided to all property owners 250 feet or less from the property line in addition to posting the public hearing sign and publishing the legal notice. Staff received no public comments.

FINDINGS OF FACT

Staff believes the standards for a Special Use, as shown below, have been met. The proposed drive-through use is a desirable use to provide a service to the community. The use will contribute to the general welfare of the community. The development will not be detrimental to the health, safety, morals, general welfare or property values in the vicinity. The proposed development complies with the regulations of the B-3 zoning district. The requested Special Use is listed as an allowable Special Uses in Section 28.609(a) of the Zoning Ordinance.

Section 28.1902 Standards for Approval of Special Uses

The Village Council may authorize a special use by ordinance provided that the proposed Special Use is consistent and in substantial compliance with all Village Council policies and land use plans, including but not limited to the Comprehensive Plan, the Future Land Use Plan and Master Plans and the evidence presented is such as to establish the following:

- (a) That the proposed use at that particular location requested is necessary or desirable to provide a service or a facility which is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.*
- (b) That such use will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, or general welfare of persons residing or working in the vicinity or injurious to property values or improvements in the vicinity.*
- (c) That the proposed use will comply with the regulations specified in this Zoning Ordinance for the district in which the proposed use is to be located or will comply with any variation(s) authorized pursuant to Section 28-1802.*
- (d) That it is one of the special uses specifically listed for the district in which it is to be located.*

RECOMMENDATIONS

The proposed special use is consistent and compatible with surrounding zoning and land use classifications. Based on the findings listed above, staff recommends the Plan Commission make a positive recommendation to the Village Council regarding this petition subject to the following conditions:

1. The Special Use shall substantially conform to the staff report dated July 11, 2011 and with preliminary engineering and landscape plans prepared by Core States Group, dated May 4, 2011 and revised on June 16, 2011; architectural plans prepared by Frederick J. Goglia, dated January 4, 2011; and sign drawings prepared by Mandeville Sign, dated July 15, 2010 and revised on June 20, 2011 except such plans may be modified to conform to Village Codes and Ordinances.
2. A description of best management practices for stormwater that are incorporated into the site design and how they will function shall be prepared and submitted. The applicable design criteria such as soil type, vegetation, and land cover conditions draining to the best management practice along with appropriate calculations shall be included.
3. The proposed addition shall have a manual and automatic fire detection system installed throughout in a manner acceptable to the Village. All areas of the building shall be protected.

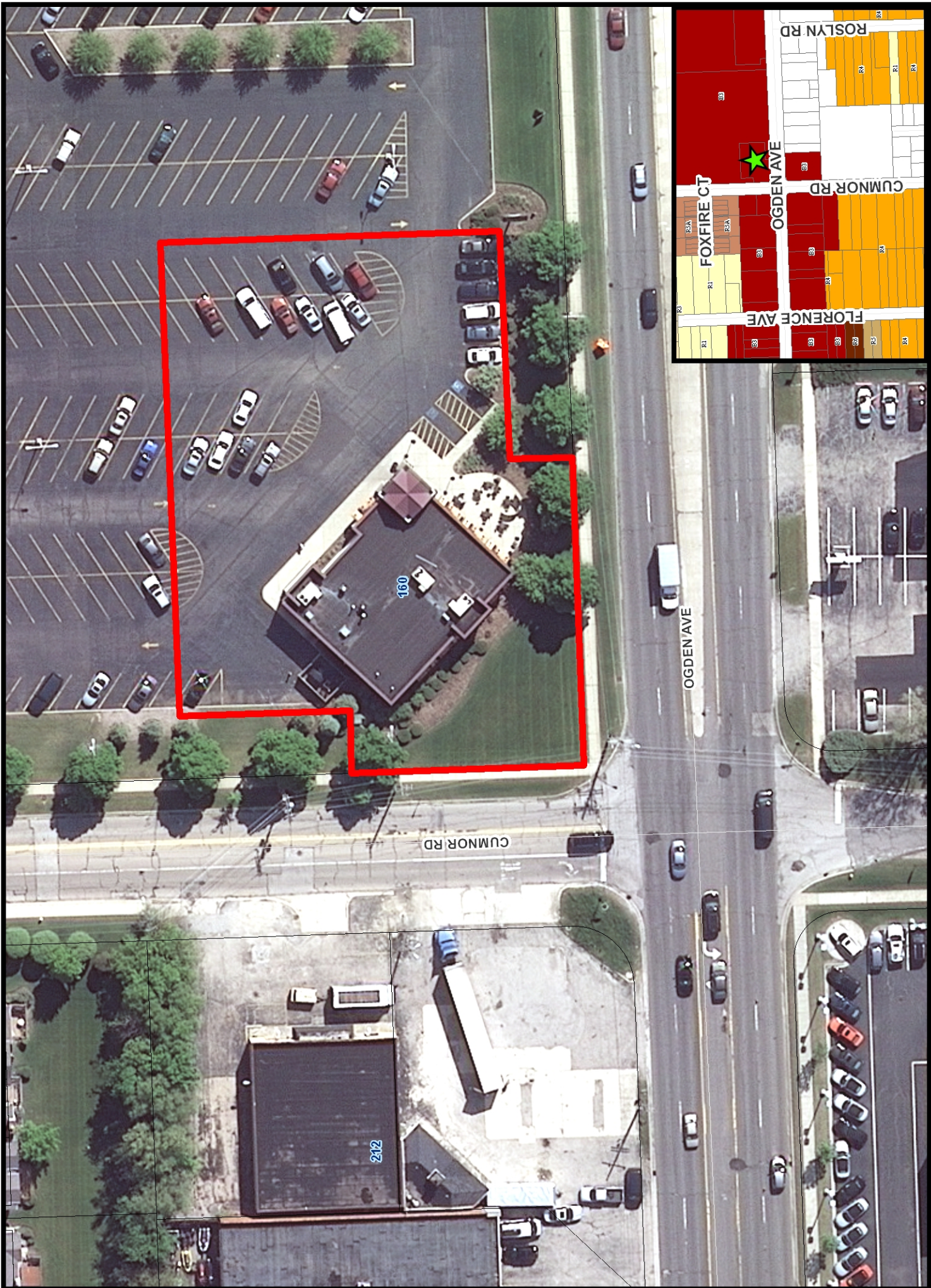
4. The proposed addition shall have a complete automatic sprinkler system installed throughout in a manner acceptable to the Village. All areas of the building shall be protected.
5. All proposed signage shall comply with the Village's Sign Ordinance.
6. A minimum three-foot wide landscaping bed, containing shrubs or ornamental grasses, shall be installed between the drive-through lane and adjacent sidewalks for the entire length of the drive-through lane.
7. The menu board shall be equipped with automatic volume control.

Staff Report Approved By:

Tom Dabareiner, AICP
Director of Community Development

TD:sjp
-att

P:\P&CD\PROJECTS\PLAN COMMISSION\2011 PC Petition Files\PC-18-11 160 Ogden - Panera - Special Use for a drive through\Staff Report PC-18-11.doc



0 10 20 30 40 Feet

160 Ogdenville Avenue Location Map



1. *The proposed use at that particular location requested is necessary or desirable to provide a service or a facility which is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community;*

The Petitioner has served the community through the operation of a Panera Bread restaurant from the Property since 1999. The restaurant provides fast-casual dining for residents of the Village and the many employees who work in the Village. Panera is a destination restaurant that complements the many service and retail uses located through the Ogden Avenue commercial corridor. The proposed special use will permit the construction of a drive-thru facility and associated addition to the existing facility. The drive-thru facility is in the interest of public convenience and will contribute to the general welfare through; 1) increased store sales & sales tax revenues, 2) quick and convenient access for patrons, and 3) facilitating additional parking for dine-in patrons.

2. *That such use will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, or general welfare of persons residing or working in the vicinity or injurious to property values or improvements to the vicinity;*

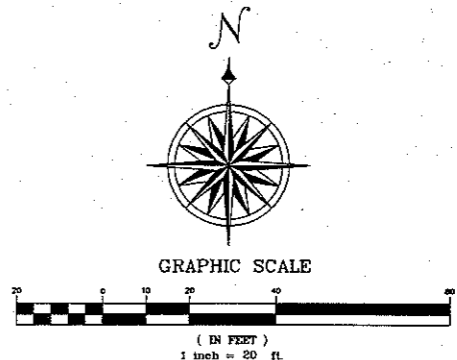
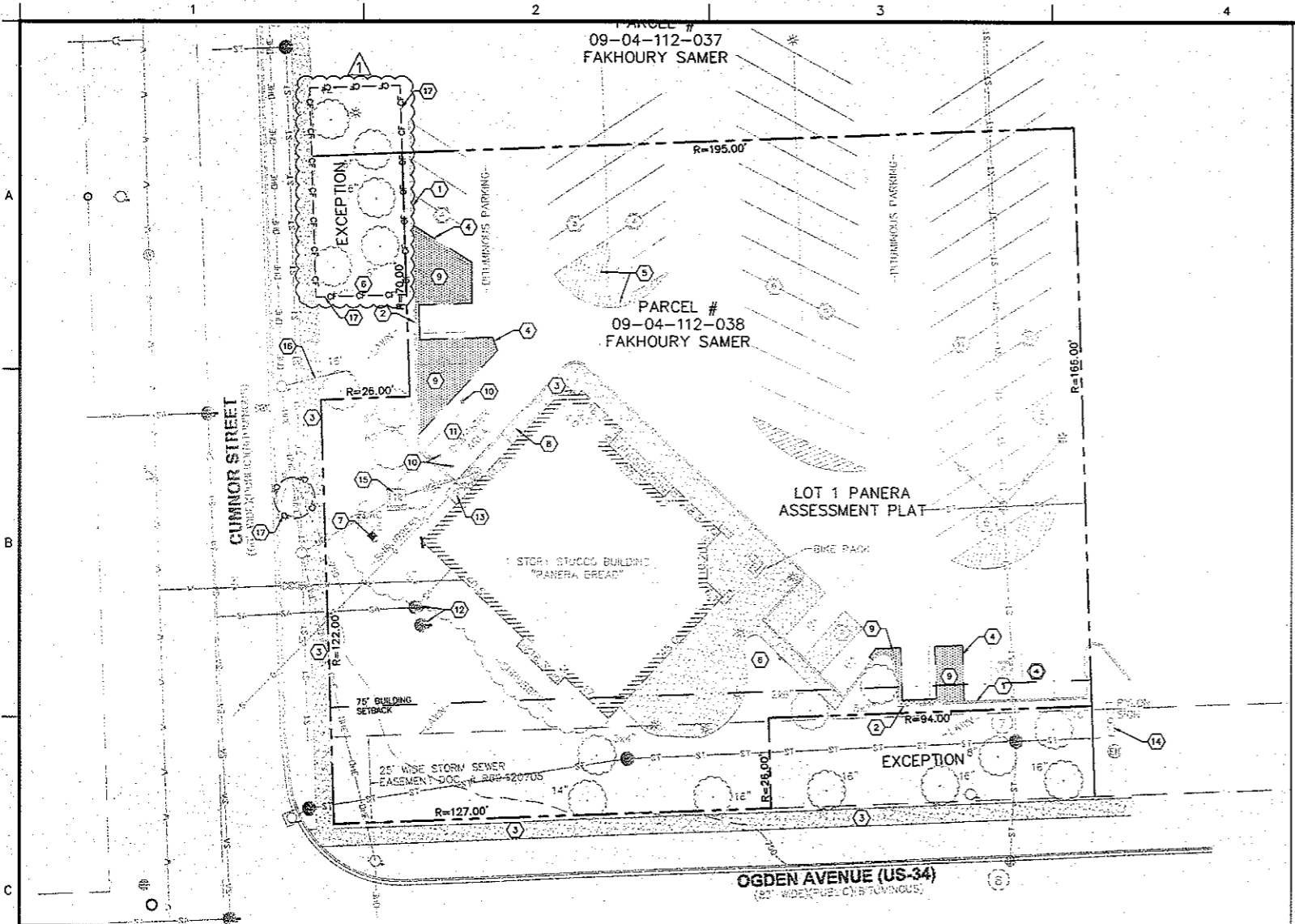
The construction of the proposed special use drive-thru addition will not be detrimental to the general welfare nor will it be injurious to property values. The Property is part of a larger commercial shopping center located along the Ogden Avenue commercial corridor. The property is developed with a free-standing building that has been utilized as a Panera Bread restaurant for approximately twelve years. The drive-thru lane and associated building improvements comply with all Village ordinances. Moreover, the drive-thru addition is oriented toward the commercial corridor and is appropriately treated with architectural improvements and landscape enhancements to address the streetscape appearance. The proposed addition is essential to the continued use and operation of the Property as a Panera Bread restaurant. The continued use of the Property preserves the general welfare of the community and will help sustain property values as the restaurant serves residents and employees in the general vicinity and serves as a complementary use to the many service and retail businesses throughout the corridor.

3. *That the proposed use will comply with the regulations specified in this Zoning Ordinance for the district in which the proposed use is to be located or will comply with any variation(s) authorized pursuant to Section 28-1802;*

The proposed drive-thru complies with the regulations specified in this Zoning Ordinance for the district in which the proposed is located and no variances are requested. The proposed drive-thru addition complies with the required seventy five foot (75') setback from Ogden Avenue. All proposed signage does or will comply with the regulations of Section 28.1500 (Sign Ordinance). The necessary eight car stacking lane and associated improvements are detailed on the Plans and have been reviewed by Village staff without objection.

4. *That it is one of the special uses specifically listed for the district in which it is to be located.*

The proposed special use is specifically listed for the district in which it is to be located. The Panera lot is located in a B-3 District and as such any special uses of the B-2 General Retail Business District are permitted as special uses in the B-e District. The drive-thru uses proposed by Panera is specifically enumerated as a special use in the B-2 District.



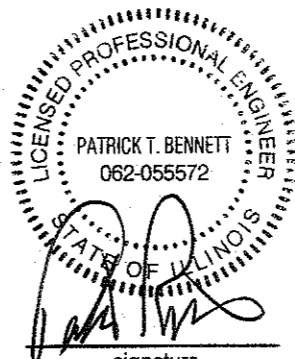
DEMOLITION NOTES

- A. REFER TO SHEET C-2 FOR DIMENSIONS AND IDENTIFICATION OF PROPOSED IMPROVEMENTS.
- B. REFER TO SHEET C-3 FOR GRADING AND DRAINAGE.
- C. REFER TO SHEET C-3 FOR GENERAL LEGEND.
- D. ALL DIMENSIONS ARE TO GROUND LEVEL IMPROVEMENTS (FACE OF CURB, CONCRETE SLAB, ETC.) UNLESS NOTED OTHERWISE.
- E. CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR DIMENSIONS AND DIMENSIONS OF SIDEWALKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.

KEYED NOTES

- 1. EXISTING CURB TO REMAIN.
- 2. EXISTING CURB TO BE REMOVED.
- 3. EXISTING CONCRETE WALK TO REMAIN.
- 4. NEW SAWCUT LINE.
- 5. EXISTING STRIPING TO BE OBLITERATED.
- 6. EXISTING LANDSCAPE AREA. REFER TO LANDSCAPE PLAN ON SHEET L-1.
- 7. EXISTING LIGHT STANDARD TO BE REMOVED AND RELOCATED.
- 8. EXISTING RAMP TO REMAIN.
- 9. EXISTING PAVEMENT TO BE REMOVED.
- 10. EXISTING BOLLARD(S) TO REMAIN.
- 11. EXISTING TRASH ENCLOSURE TO REMAIN.
- 12. EXISTING GREASE INTERCEPTOR TO BE CLEANED OUT AND REMOVED FOR RE-USE. CONTRACTOR TO COORDINATE TIMING AND NEED FOR TEMPORARY FACILITIES WITH OWNER.
- 13. EXISTING GAS METER TO REMAIN.
- 14. EXISTING PYLON SIGN TO BE REMOVED.
- 15. EXISTING TRANSFORMER TO REMAIN.
- 16. EXISTING GUY WIRE TO BE RELOCATED. CONTRACTOR TO COORDINATE WITH POWER COMPANY.
- 17. PARKWAY TREE PROTECTION 6" CHAINLINK FENCE WITH POSTS 10' APART OR LESS. REFER TO PARKWAY TREE PROTECTION REQUIREMENTS DETAIL ON SHEET C-5.

DEMOLITION LEGEND



signature
JUN 17 2011
 date

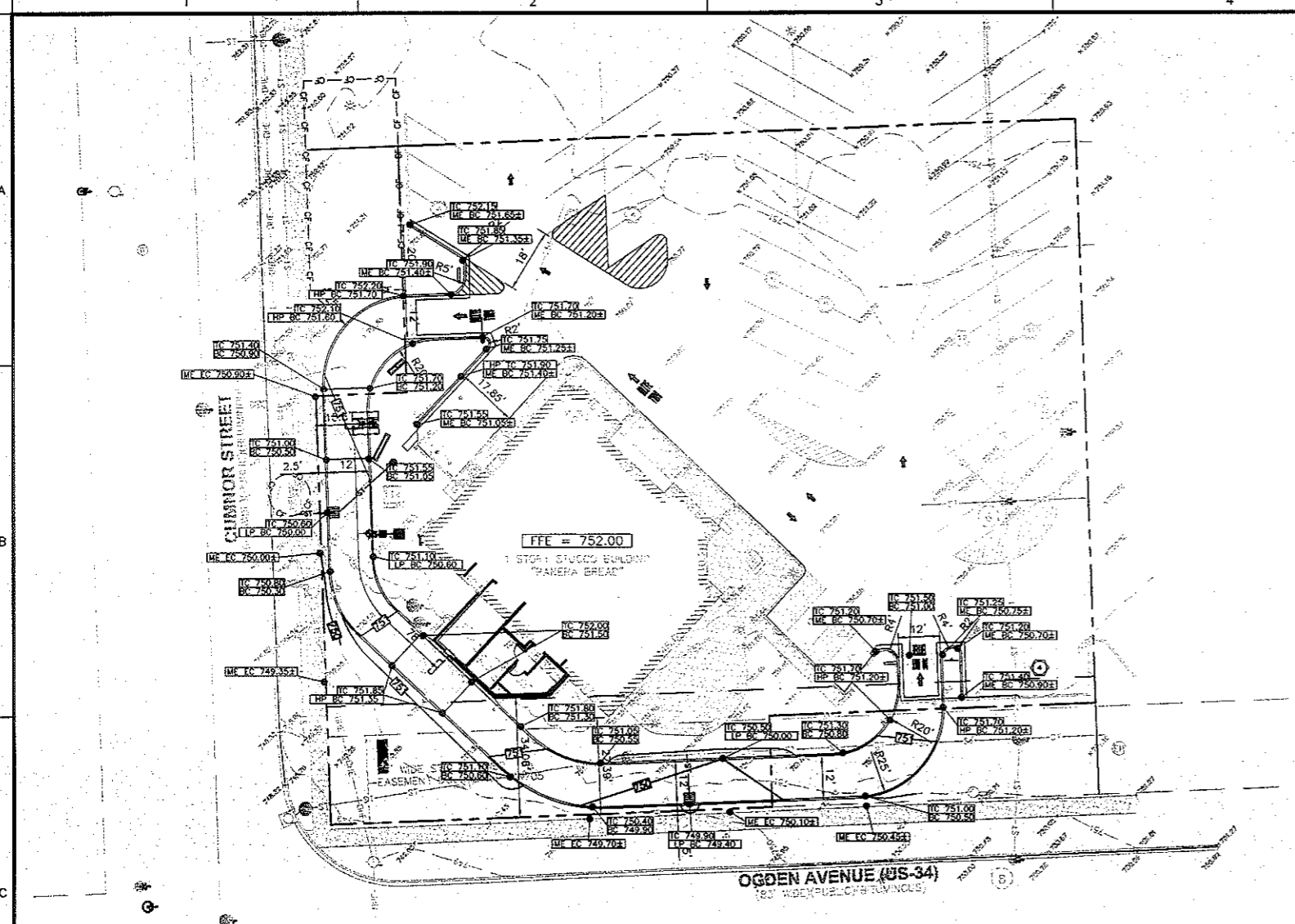
CONSTRUCTION ENTRANCE NOTE:
 GENERAL CONTRACTOR SHALL ONLY USE THE PARKING LOT AS A CONSTRUCTION ENTRANCE. NO OTHER ACCESS ACROSS THE VILLAGE RIGHT-OF-WAY WILL BE ALLOWED. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.

NOTE:
 GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND UNCOVERING EXISTING UTILITIES IN THE VICINITY OF THE PROPOSED DRIVE THRU PRIOR TO THE START OF CONSTRUCTION TO ASCERTAIN EXACT MATERIALS, LOCATIONS, ELEVATIONS, ETC. AND THEIR POTENTIAL CONFLICT WITH PROPOSED IMPROVEMENTS. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.

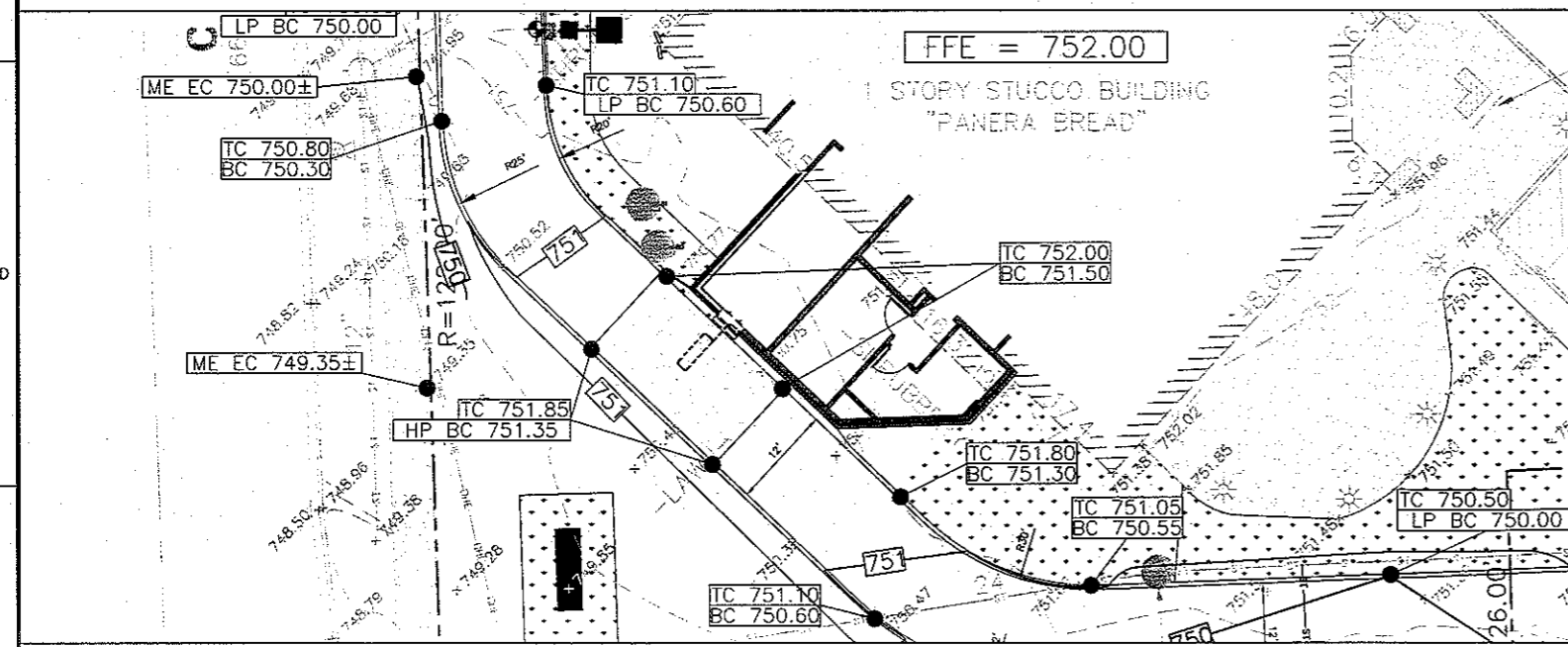


THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON DESIGN DRAWINGS, RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. CORE STATES, INC. DOES NOT GUARANTEE THAT LOCATIONS SHOWN ARE EXACT. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF UTILITIES.

Job#: ARC-11587 Scale: 1"=20' Date: 05-04-11 Drawn By: MAB Checked By: PTB	Description 05/15/2011 REVISIONS 01
	Panera, LLC 3630 South Geyer Rd., Suite 100 St. Louis, MO 63127 Cafe # 734 Drive-Thru Addition 180 Ogden Ave., Downer's Grove, IL 60515 Demolition Plan
Documents prepared by Core States, Inc. including this document, are to be used for the project only and are not to be used for any other project, by owner or by any other party. Any extension of use in any form without the written consent of Core States, Inc. is prohibited. Core States, Inc. is not responsible for any errors or omissions that may appear hereon. Core States, Inc. hereby disclaims all claims and losses.	
C-1 1 OF 6	



GRADING PLAN:
SCALE: 1" = 20'



ENLARGED GRADING PLAN:
SCALE: 1" = 10'

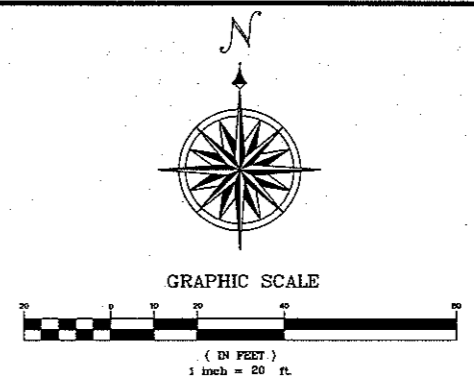
GRADING NOTES

A. REFER TO SHEET C-2 FOR DIMENSIONS AND IDENTIFICATION OF PROPOSED IMPROVEMENTS.

B. REFER TO SHEET C-2 FOR ABOVE AND BELOW GROUND SEWERS AND UTILITIES.

ABBREVIATIONS

BC BOTTOM OF CURB
 EC EDGE OF CONCRETE
 EX EXISTING
 GS GROUND SHOT
 HP HIGH POINT
 LP LOW POINT
 ME MATCH EXISTING
 TC TOP OF CURB
 TP TOP OF PAVEMENT



	EXISTING	PROPOSED
ASPHALT PAVEMENT (HEAVY DUTY)		
ASPHALT PAVEMENT (STANDARD DUTY)		
BENCHMARK		
BOLLARD (PIPE)		
BOLLARD (PIPE GUARD)		
BUSH/SHRUB		
CONCRETE PAVEMENT		
CONTOUR - MAJOR		
CONTOUR - MINOR		
ELECTRIC - GUY WIRE		
ELECTRIC - LIGHT		
ELECTRIC - METER		
ELECTRIC - OVERHEAD LINE		
ELECTRIC - POWER POLE		
ELECTRIC - TRANSFORMER		
ELECTRIC - UNDERGROUND LINE		
FENCE-BARBWIRE		
FENCE-CHAINLINK		
FENCE-CONSTRUCTION		
FENCE-SILT		
FENCE-WOOD		
FLOWLINE OF STREAM		
GAS - GAS LINE		
GAS - METER		
GAS - VALVE		
PARKING STALL NUMBER		
SANITARY SEWER - CLEANOUT		
SANITARY SEWER - MANHOLE		
SANITARY SEWER - SANITARY LINE		
SANITARY SEWER - STRUCTURE LABEL		
SIGN		
SPOT ELEVATION		
STORM SEWER - CLEANOUT		
STORM SEWER - CURB INLET		
STORM SEWER - DOUBLE CURB INLET		
STORM SEWER - FLARED END SECTION		
STORM SEWER - GRATE INLET		
STORM SEWER - DOUBLE GRATE INLET		
STORM SEWER - MANHOLE		
STORM SEWER - MANHOLE (GRADED)		
STORM SEWER - STORM LINE		
STORM SEWER - STRUCTURE LABEL		
TELEPHONE - BOX		
TELEPHONE - TELEPHONE LINE		
TREE - GENERAL		

LICENSED PROFESSIONAL ENGINEER
 PATRICK T. BENNETT
 062-055572
 STATE OF ILLINOIS
 signature
 JUN 17 2011
 date

CONSTRUCTION ENTRANCE NOTE:
 GENERAL CONTRACTOR SHALL ONLY USE THE PARKING LOT AS A CONSTRUCTION ENTRANCE. NO OTHER ACCESS ACROSS THE VILLAGE RIGHT-OF-WAY WILL BE ALLOWED. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.

NOTE:
 GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND UNCOVERING EXISTING UTILITIES IN THE VICINITY OF THE PROPOSED DRIVE THRU PRIOR TO THE START OF CONSTRUCTION TO ASCERTAIN EXACT MATERIALS, LOCATIONS, ELEVATIONS, ETC. AND THEIR POTENTIAL CONFLICT WITH PROPOSED IMPROVEMENTS. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.



THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON DESIGN DRAWINGS, RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. CORE STATES, INC. DOES NOT GUARANTEE THAT LOCATIONS SHOWN ARE EXACT. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF UTILITIES.

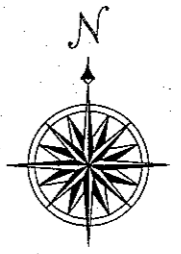
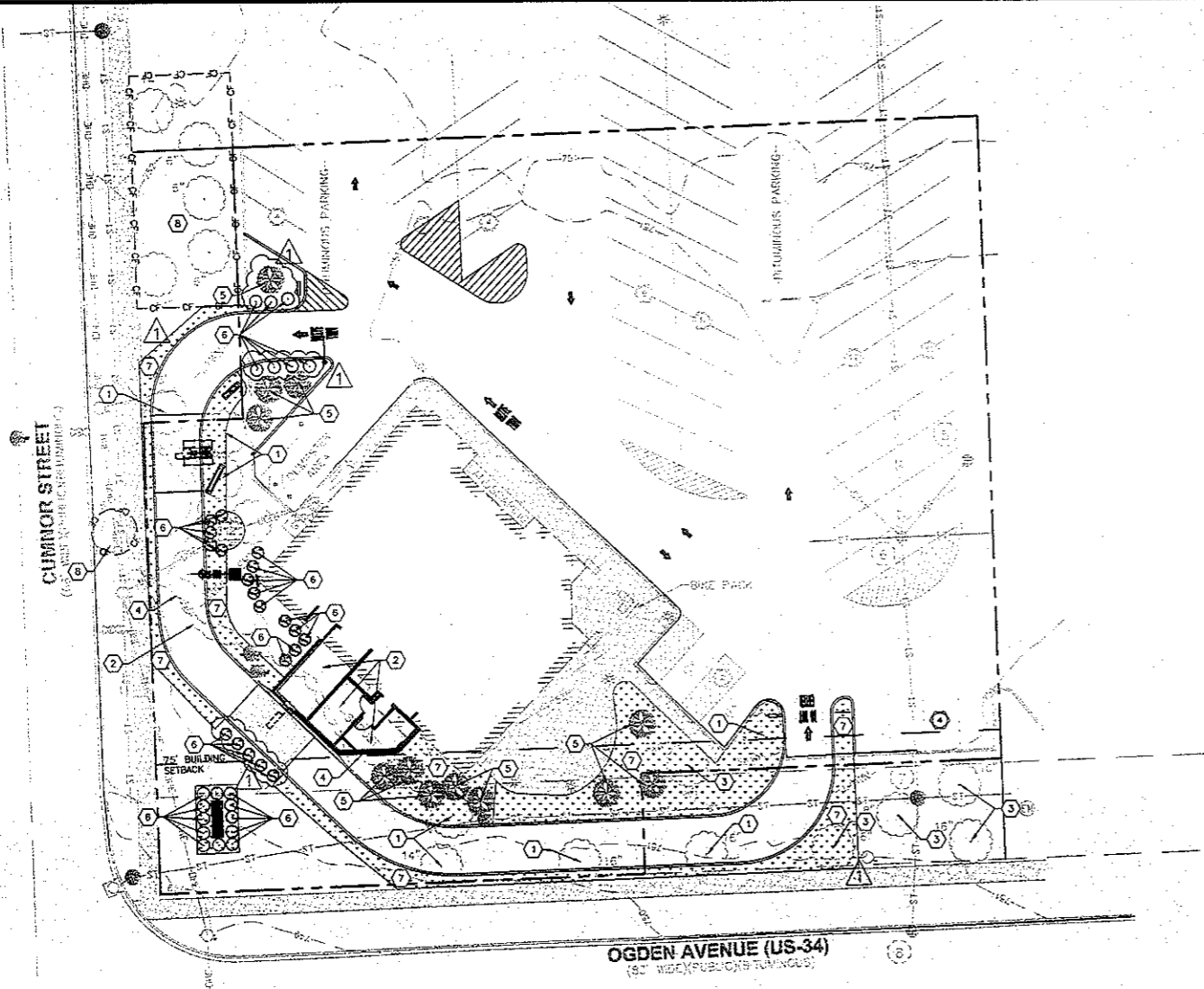
Job#: ARC-11587
 Scale: 1"=20'
 Date: 05-04-11
 Drawn By: MAB
 Checked By: PTB

Core States Group
 2000 Eastwood Executive Center, Suite 800
 Peoria, IL 61611
 Phone: (312) 414-4330
 Fax: (312) 414-4323
 corestates.com

Panera BREAD

Panera LLC
 3630 South Geyer Rd., Suite 100 St. Louis, MO 63127
 Cafe # 734
 Drive-Thru Addition
 160 Ogden Ave., Downer's Grove, IL 60515
 Grading Plan

C-3
 3 OF 6



GRAPHIC SCALE
(IN FEET)
1 inch = 20 ft

LANDSCAPE NOTES

1. EXISTING LANDSCAPING SHOWN IS BASED ON AVAILABLE INFORMATION. CONTRACTOR TO VERIFY EXISTING LANDSCAPING.
2. IF EXISTING LANDSCAPING SHOWN TO BE RELOCATED CANNOT BE PRESERVED, CONTRACTOR SHALL PROVIDE NEW MATERIALS IN LOCATIONS SHOWN.

KEYED NOTES

1. EXISTING TREE TO BE REMOVED.
2. EXISTING LANDSCAPING TO BE REMOVED, PRESERVED AND TRANSPLANTED TO NEW LOCATION.
3. EXISTING TREE TO REMAIN.
4. EXISTING SHRUBBERY TO BE REMOVED.
5. NEW TREE.
6. NEW SHRUB.
7. NEW SODDED AREA.
8. PARKWAY TREE PROTECTION AREA.

LEGEND					
GROUND COVER					
SYMBOL	DESCRIPTION				
	SODDED AREA				
SHRUBS					
SYMBOL	QUANTITY	COMMON NAME (botanical name)	ROOT	SIZE	REMARKS
	20	DWARF RED BARBERRY (barberis thunbergii var. atropurea)	CONT.	5 GAL.	18"-24"
	19	EMERALD MOUND HONEYSUCKLE (lonicera xylosteum 'emerald mound')	CONT.	5 GAL.	18"-24"
TREES					
SYMBOL	QUANTITY	COMMON NAME	ROOT	SIZE	REMARKS
	12	FAIRVIEW JUNIPER (juniperis chinensis 'fairview')	B&B	2.5" CAL.	5' - 7'
		EXISTING TREE			

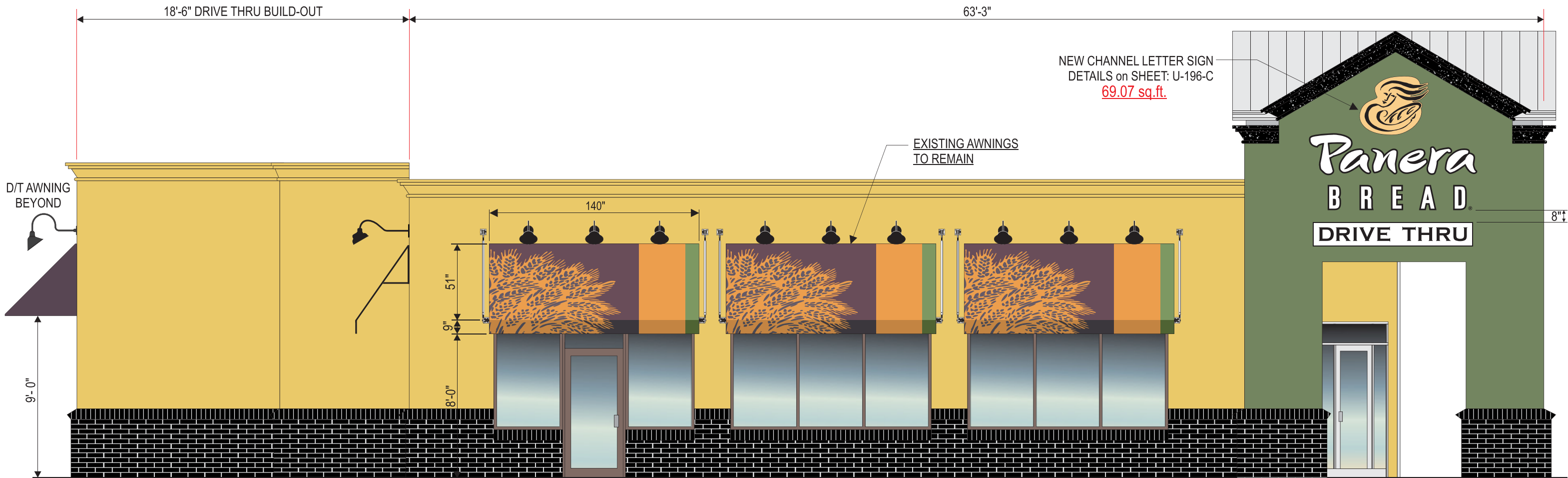
LICENSED PROFESSIONAL ENGINEER
PATRICK T. BENNETT
CE-055572
Signature
JULIE
ONE-CALL SYSTEM
1-800-492-0123
AUG 17 2011



CONSTRUCTION ENTRANCE NOTE:
GENERAL CONTRACTOR SHALL ONLY USE THE PARKING LOT AS A CONSTRUCTION ENTRANCE. NO OTHER ACCESS ACROSS THE VILLAGE RIGHT-OF-WAY WILL BE ALLOWED. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON DESIGN DRAWINGS, RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. CORE STATES, INC. DOES NOT GUARANTEE THAT LOCATIONS SHOWN ARE EXACT. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF UTILITIES.

Job#: ARC-11567	Scale: 1"=20'	Date: 05-04-11	Drawn By: MAB	Checked By: PTB
Documents prepared by Core States, Inc. are for the use of the client and are not to be used for any other purpose without the express written consent of Core States, Inc. Core States, Inc. does not warrant the accuracy of the information shown on these drawings and the user assumes all liability for any errors or omissions. Core States, Inc. reserves the right to modify these drawings at any time without notice.				
L-1 6 OF 6				



NEW CHANNEL LETTER SIGN
 DETAILS on SHEET: U-196-C
 69.07 sq.ft.

EXISTING AWNINGS
 TO REMAIN

SOUTHEAST STOREFRONT ELEVATION:
 3/16" = 1'-0"

BUILDING FRONTAGES:

SOUTHEAST ELEVATION: 63'-3" FRONTAGE
 NORTHEAST ELEVATION: 76'-3" FRONTAGE
 SOUTHWEST ELEVATION: 74'-0" FRONTAGE
 TOTAL FRONTAGE: 213'-6"

213.5' X 1.5 = 320.25 sq. ft. ALLOWED SIGN AREA

PROPOSED SIGN AREA:

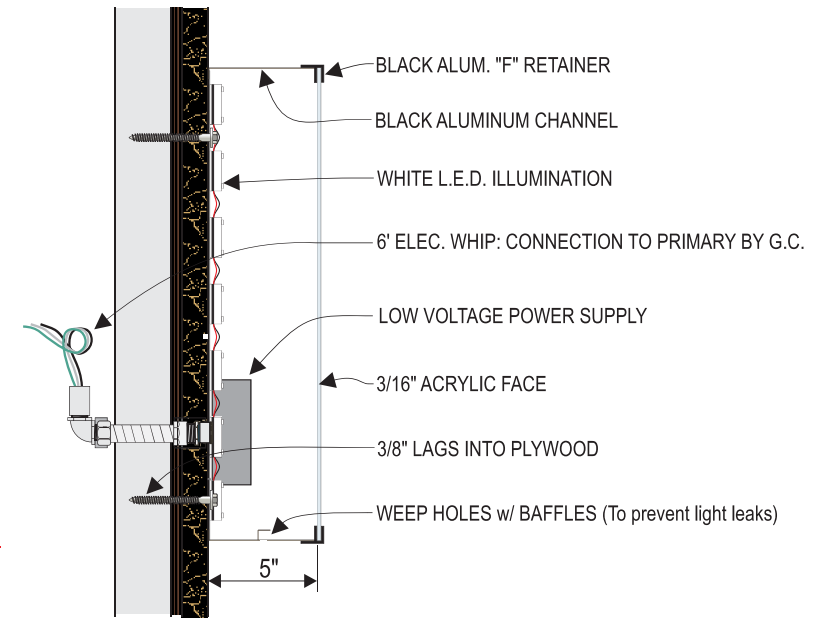
SOUTHEAST ELEV. = 69.07 + 11.89 = 80.96 sq.ft.
 NORTHEAST ELEV. = 69.07 + 11.89 = 80.96 sq.ft.
 SOUTHWEST ELEV. = 77.9 + 10.64 = 88.54 sq.ft.
 PROPOSED MONUMENT SIGN = 36.00 sq. ft.
 TOTAL PROPOSED SIGN AREA = 286.46 sq. ft.

ILLUMINATED "DRIVE THRU" SIGN CABINET:

- ALUMINUM CHANNEL LETTER TYPE CONSTRUCTION
- .040 ALUM. RETURNS, 5" DEEP, PAINTED BLACK
- 3/16" THICK WHITE ACRYLIC FACE
- 230-22 MATTE BLACK VINYL COPY
- 1" x 5/8" ALUM. "F" RETAINER PAINTED BLACK
- WHITE L.E.D. ILLUMINATION
- WEEP HOLES REQUIRED
- LOW VOLTAGE POWER SUPPLY
- FLUSH WALL MOUNT
- U.L. LABEL



D/T WALL SIGN ELEVATION: 3/8" = 1'-0" 11.89 SQ.FT.



DRIVE THRU BOX SIGN SECTION: N.T.S.

ARTWORK EXCLUSIVE PROPERTY OF

SINCE 1917

MANDEVILLE SIGN
making your mark.

676 GEORGE WASHINGTON HIGHWAY
 LINCOLN, RI 02865-4255

PHONE 401-334-9100 FAX 401-334-7799
 WEB www.mandevillesign.com

APPROVALS	
Signatures Required Before Release to Production	
Engineering	DATE
Sales	DATE
Production	DATE
Quality Control	DATE

REVISIONS			
NO.	BY	DESCRIPTION	DATE
1	JM	Updated building color to BM #2154-40 York Harbor Yellow	08/03/10
2	GL	rev.sign size, add drive thru wall sign. Add building frontage dim. Add DRIVE THRU build-out. Add sign area calcs.	01/25/11
3	WD	Updated D/T addition, and area calculations	06/20/11
4			
5			
6			
7			

CLIENT

STORE # 734

160 Ogden Ave.
 DOWNER'S GROVE, IL

LOCATION

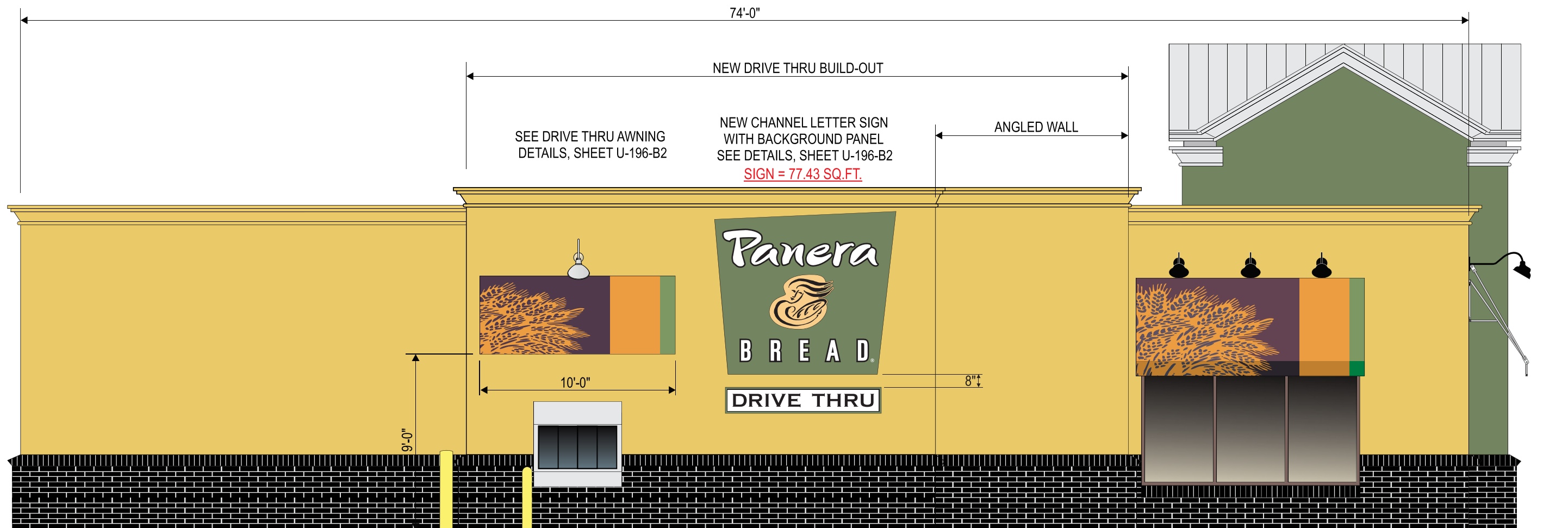
21401-1

WORK ORDER NUMBER(S)

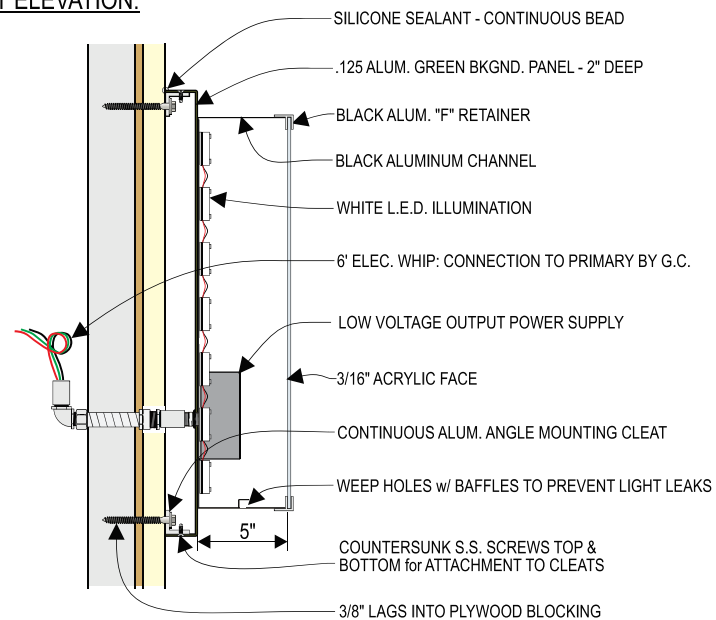
PROJECT MANAGER JIM MANDEVILLE

NOTED	JM	07/15/10
SCALE	ARTIST	DATE

DRAWING NAME U-196-A



SOUTHWEST STOREFRONT ELEVATION:
3/16" = 1'-0"



DRIVE THRU BOX SIGN SECTION: N.T.S.



D/T WALL SIGN ELEVATION: 3/8" = 1'-0"

ILLUMINATED "DRIVE THRU" SIGN CABINET:

- ALUMINUM CHANNEL LETTER TYPE CONSTRUCTION
- .040 ALUM. RETURNS, 5" DEEP, PAINTED BLACK
- 3/16" THICK WHITE ACRYLIC FACE
- 230-22 MATTE BLACK VINYL COPY
- 1" x 5/8" ALUM. "F" RETAINER PAINTED BLACK
- 2" DEEP ALUM. PAN BKGND. PANEL PAINTED PMS 5757U
- WHITE L.E.D. ILLUMINATION
- WEEP HOLES REQUIRED
- LOW VOLTAGE POWER SUPPLY
- FLUSH WALL MOUNT
- U.L. LABEL

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LINCOLN, RI 02865-4255

PHONE 401-334-9100 FAX 401-334-7799
WEB www.mandevillesign.com

APPROVALS	
Signatures Required Before Release to Production	
Engineering	DATE
Sales	DATE
Production	DATE
Quality Control	DATE

REVISIONS			
NO.	BY	DESCRIPTION	DATE
1	JM	Updated building color to BM #2154-40 York Harbor Yellow	08/03/10
2	GL	rev.sign size, add drive thru wall sign. Add building frontage dim. Add DRIVE THRU build-out.	01/25/11
3	GL	rev. buildout size and location. revise wall sign and dt sign specs.	03/02/11
4	WD	revert back to previous version	05/11/11
5	WD	Updated D/T addition and signs, remove awning tie-backs	06/20/11
6			
7			

CLIENT

STORE # 734

160 Ogden Ave.
DOWNER'S GROVE, IL

LOCATION

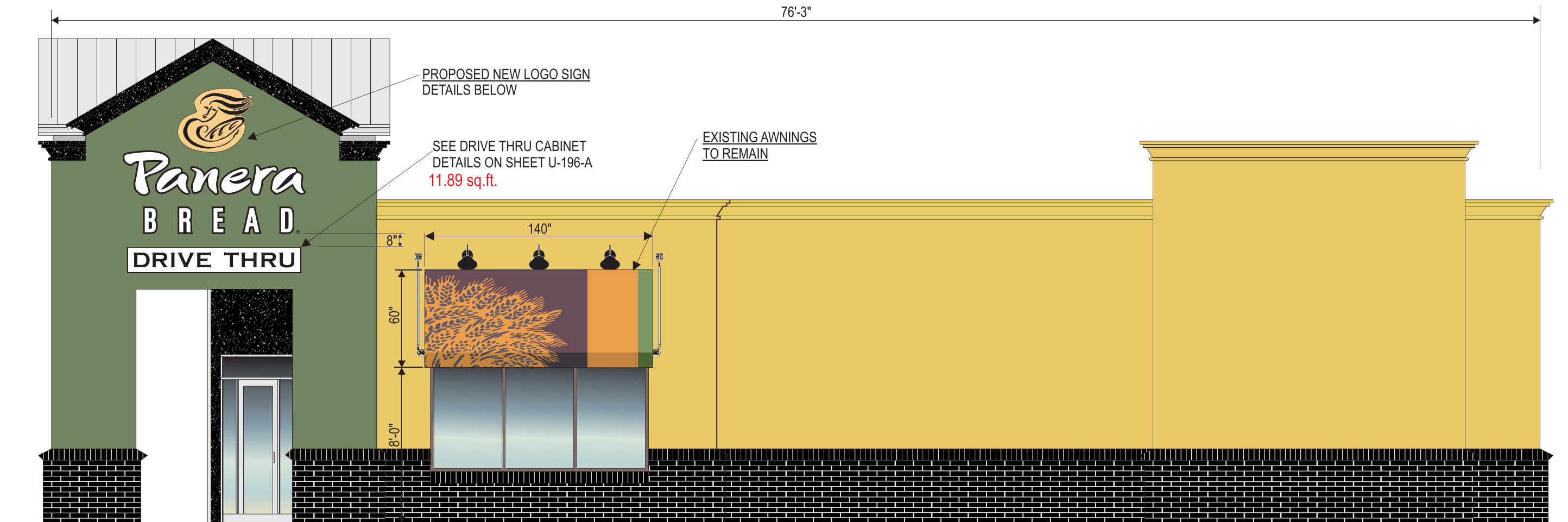
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WORK ORDER NUMBER(S)

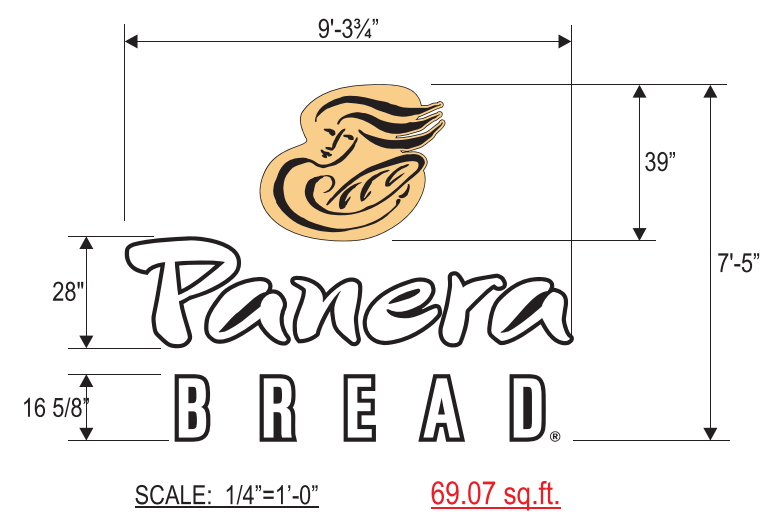
PROJECT MANAGER JIM MANDEVILLE

NOTED	JM	07/15/10
SCALE	ARTIST	DATE

DRAWING NAME U-196-B1



NORTHEAST STOREFRONT ELEVATION:
3/16" = 1'-0"

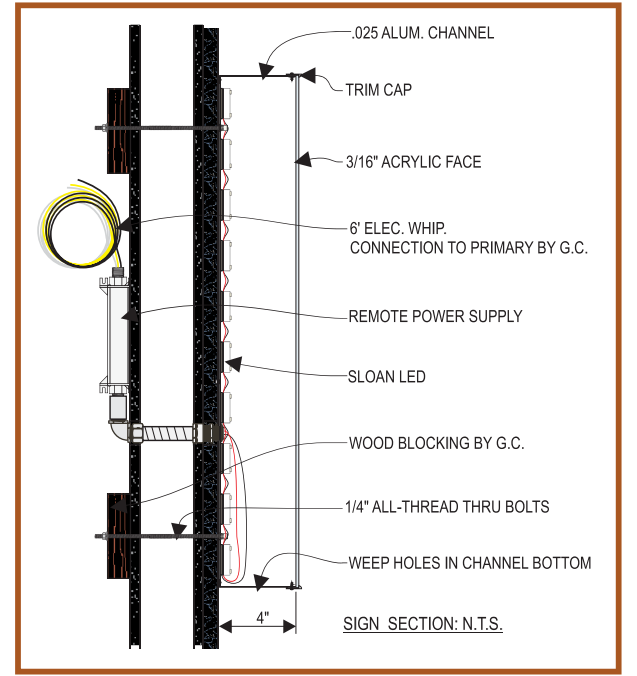


INTERNALLY ILLUMINATED CHANNEL LETTERS
w/ MOTHER BREAD LOGO

- "PANERA" & "BREAD" LETTERS:
- (#2447 WHITE) 3/16" ACRYLIC FACES WITH (230-22 MATTE BLACK) VINYL BORDERS
 - ILT WHITE LED ILLUMINATION
 - (BLACK) .040 ALUM. RETURNS - 5" DEEP
 - .040 ALUM. LETTERBACKS
 - (BLACK) 1" TRIM CAP

- MOTHER BREAD LOGO:
- (CLEAR) 3/16" ACRYLIC FACE BACKSPRAYED PMS 1355U
 - (BLACK 230-22) VINYL LOGO DETAIL.
 - (BLACK) 1" TRIM CAP
 - (BLACK) .040 ALUM. RETURNS - 4" DEEP
 - .040 ALUM. LETTER BACKS
 - ILT LED

- REMOTE POWER SUPPLIES
- U.L. AND MFG. LABELS
- WEEP HOLES



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LINCOLN, RI 02865-4255

PHONE 401-334-9100 FAX 401-334-7799

WEB www.mandevillesign.com

APPROVALS	
Signatures Required Before Release to Production	
Engineering	DATE
Sales	DATE
Production	DATE
Quality Control	DATE

REVISIONS			
NO.	BY	DESCRIPTION	DATE
1	JM	Updated building color to BM #2154-40 York Harbor Yellow	08/03/10
2	GL	rev.sign size, add drive thru wall sign. Add building frontage dim.	01/25/11
3			
4			
5			
6			
7			

CLIENT

STORE # 734

160 Ogden Ave.
DOWNER'S GROVE, IL

LOCATION

21401-1

WORK ORDER NUMBER(S)

PROJECT MANAGER **JIM MANDEVILLE**

NOTED	JM	07/15/10
SCALE	ARTIST	DATE

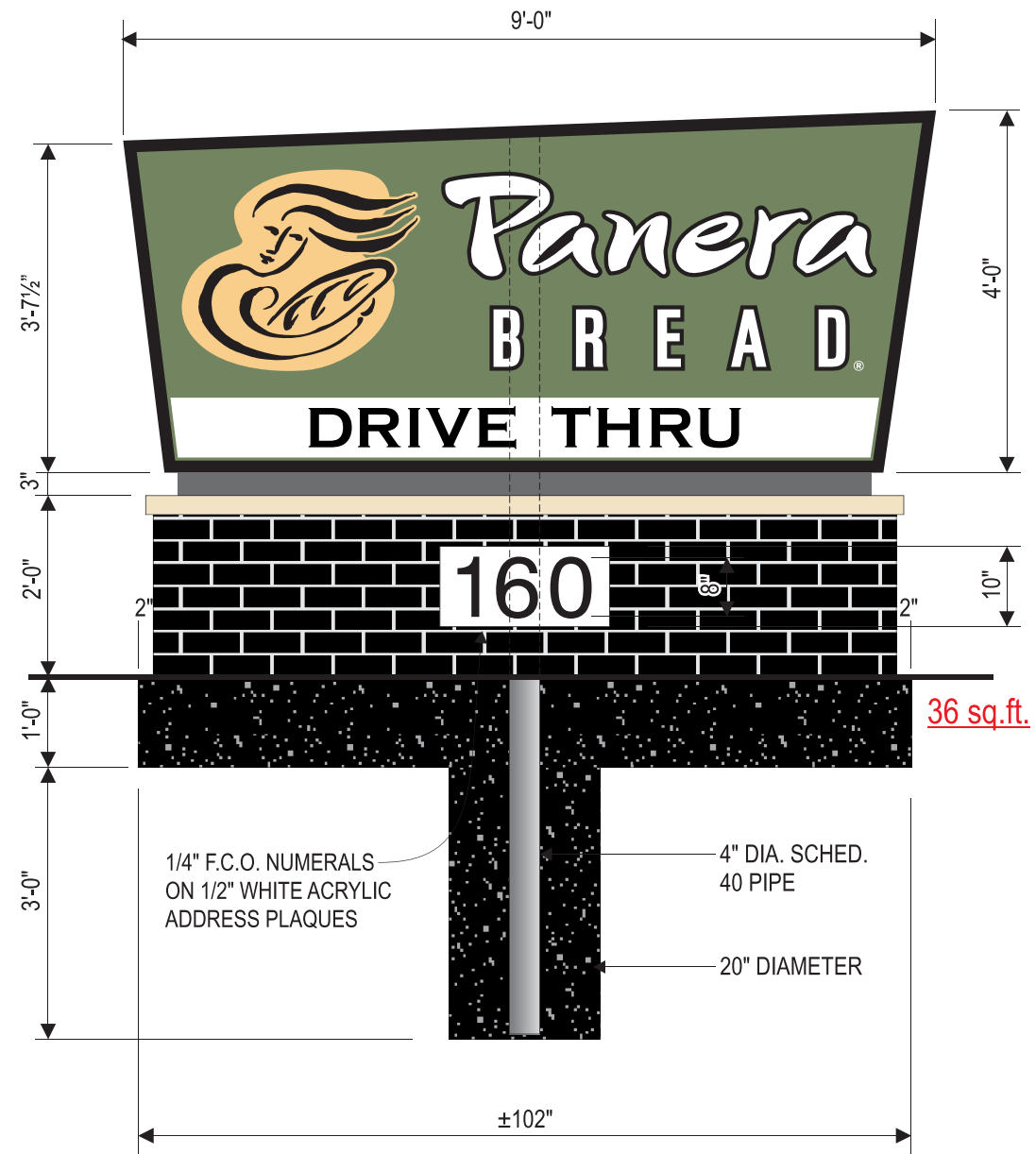
DRAWING NAME **U-196-C**



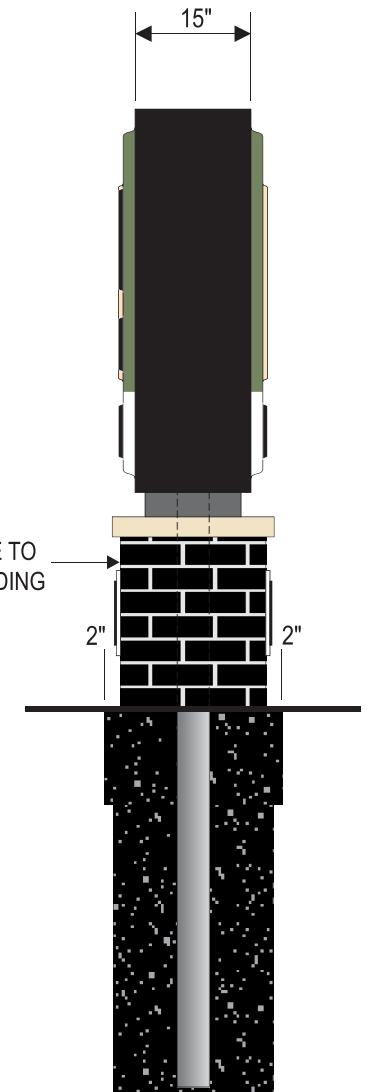
EXISTING PYLON SIGN TO BE REMOVED

D/F ILLUMINATED MONUMENT SIGN:

- (CLEAR) 3/16" CLEAR POLYCARBONATE PAN FACES w/ EMBOSSED GRAPHICS:
- BACKSPRAYED COLORS:
 - PMS-5757u GREEN (OPAQUE) FACE BACKGROUND
 - WHITE LETTER COPY
 - PMS-1355u MOTHER BREAD LOGO BACKGROUND
 - BLACK MOTHER BREAD DETAILS
 - BLACK LETTER COPY OUTLINES
 - WHITE DRIVE THRU PANEL w/ BLACK COPY
- BLACK ALUM. EXTRUSION 15" DEEP CABINET & 1 1/2" RETAINERS
- WHITE FLUORESCENT ILLUMINATION
- 3" ALUM. GRAY PAINTED REVEAL UNDER CABINET
- 4" DIA. SCHEDULE 40 STEEL TUBE SUPPORT INTO CONCRETE FOOTING
- BRICK BASE TO MATCH BUILDING
- 8" x 1/4" THICK F.C.O. NUMERALS ON 10 1/2" x 1/2" WHITE ACRYLIC ADDRESS PLAQUE (2)



MONUMENT SIGN ELEVATION: 1/2" = 1'-0"



END VIEW: 1/2" = 1'-0"

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PHONE 401-334-9100 FAX 401-334-7799
WEB www.mandevillesign.com

APPROVALS	
Signatures Required Before Release to Production	
Engineering	DATE
Sales	DATE
Production	DATE
Quality Control	DATE

REVISIONS			
NO.	BY	DESCRIPTION	DATE
1	GL	create new 60 sf sign, to be installed on existing pole	1/14/11
2	GL	delete pole sign. Add new monument sign	1/25/11
3	GL	correct circle R color to white, rev. specs	03/03/11
4	WD	Update monument sign	05/11/11
5			
6			
7			

CLIENT 

STORE # 734

160 Ogden Ave.
DOWNER'S GROVE, IL

LOCATION

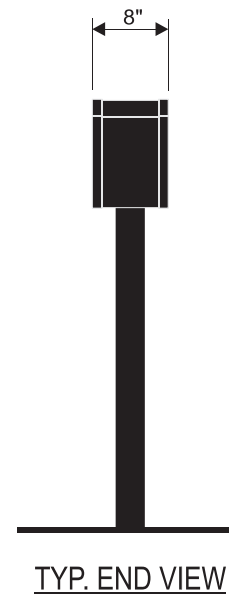
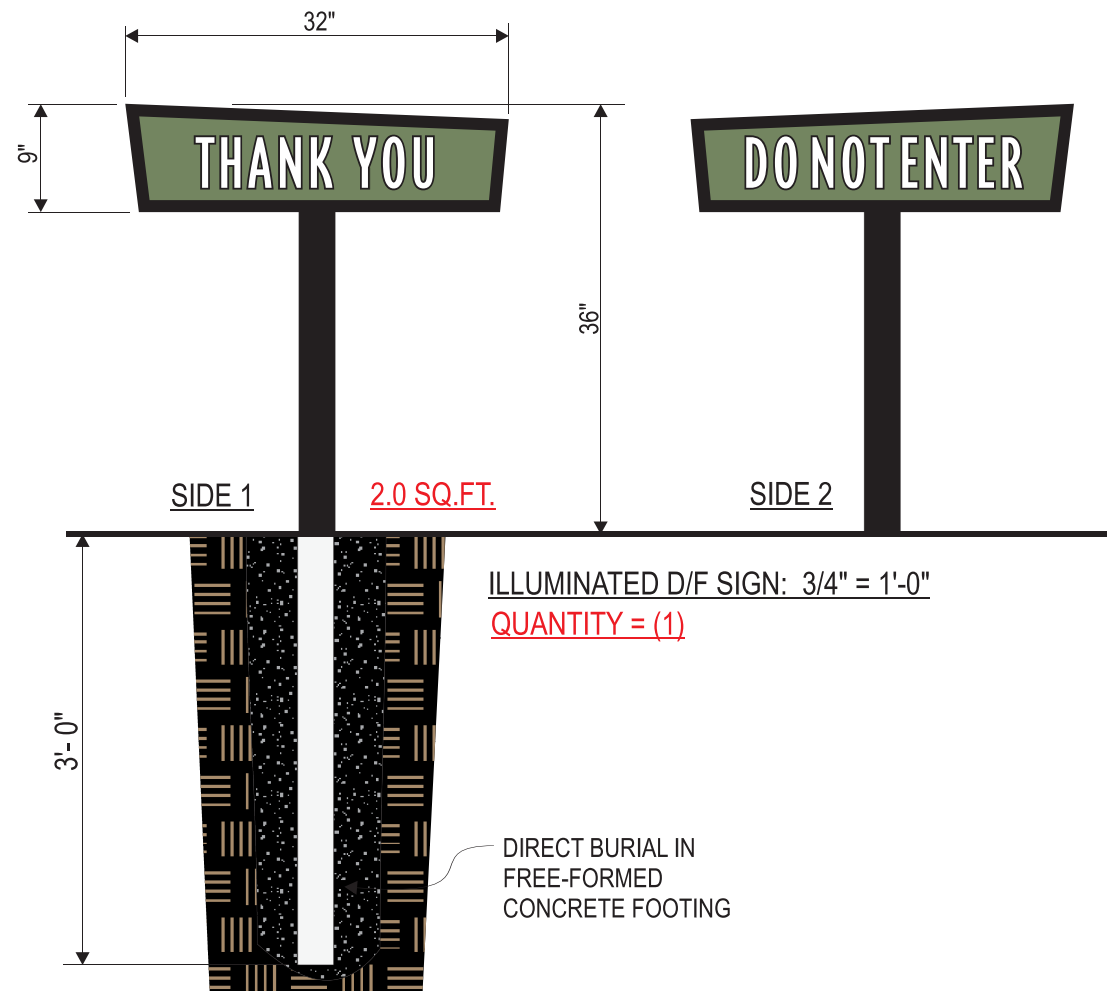
21401-1

WORK ORDER NUMBER(S)

PROJECT MANAGER JIM MANDEVILLE

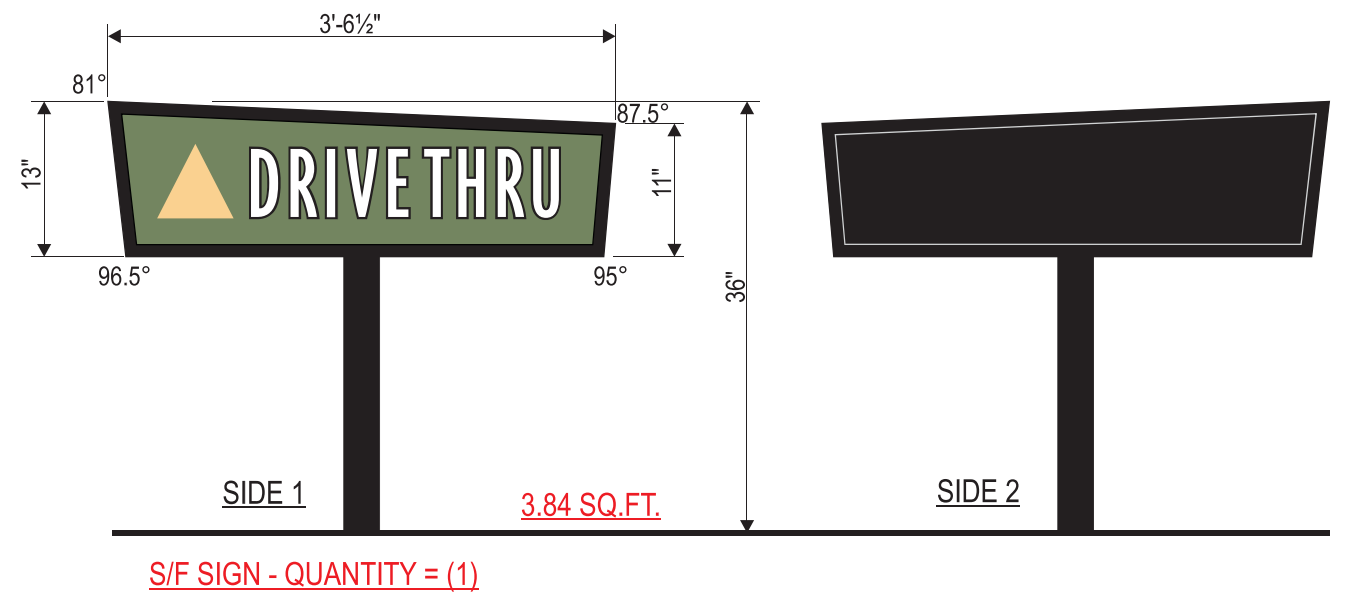
NOTED	JM	07/15/10
SCALE	ARTIST	DATE

DRAWING NAME U-196-E



ILLUMINATED DIRECTIONAL SIGNS:

- 8" DEEP CABINET w/ .063 ALUM. RETURNS & 1" ALUM. RETAINERS, PAINTED BLACK
- CLEAR LEXAN FACES w/ DIGITALLY PRINTED VINYL GRAPHICS APPLIED to 2ND SURFACE.
- OPAQUE PMS 5757U GREEN BACKGROUND, WHITE COPY w/ BLACK BORDER
- FLUORESCENT ILLUMINATION
- PMS 1355 PEACH ARROW if REQUIRED
- BLACK 3" X 3" X 1/4" ALUM. SQ. TUBE POSTS
- SINGLE FACE SIGNS HAVE FINISHED .080 ALUM. BACKS, PAINTED PMS 5757U GREEN
- DIRECT BURIAL IN FREE-FORMED CONCRETE FOOTING



DIRECTIONAL SIGNS: 3/4" = 1'-0"

ARTWORK EXCLUSIVE PROPERTY OF

SINCE  1917

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LINCOLN, RI 02865-4255

PHONE 401-334-9100 FAX 401-334-7799

WEB www.mandevillesign.com

APPROVALS	
Signatures Required Before Release to Production	
Engineering	DATE
Sales	DATE
Production	DATE
Quality Control	DATE

REVISIONS		
NO.	BY	DESCRIPTION
1		
2		
3		
4		
5		
6		
7		



STORE# 734

160 Ogden Ave.
DOWNER'S GROVE, IL

LOCATION

21401-1

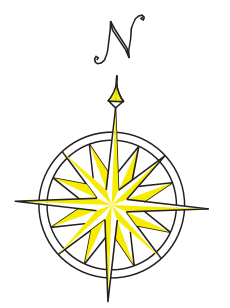
WORK ORDER NUMBER(S)

PROJECT MANAGER JIM MANDEVILLE

NOTED	GL	01/25/11
SCALE	ARTIST	DATE

DRAWING NAME U-196-F1

CUMNOR ROAD



PREVIEW BOARD

CANOPY/SPEAKER

MENU BOARD

DRIVE THRU BUILD-OUT

DRIVE THRU AWNING

PROPOSED MONUMENT SIGN
w/ 3' of Low Shrubs Around Base

10' SETBACK

10' SETBACK

NEW D/T
ADDITION

S/F DRIVE THRU SIGN

CLEARANCE BAR

THANK YOU/DO NOT ENTER SIGN

EXISTING POLE SIGN TO BE REMOVED

NEW "STOP"
SIGN

PLAZA MULTI-TENANT SIGN

OGDEN AVENUE

ARTWORK EXCLUSIVE PROPERTY OF

SINCE 1917

MANDEVILLE SIGN
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676 GEORGE WASHINGTON HIGHWAY
LINCOLN, RI 02865-4255

PHONE 401-334-9100 FAX 401-334-7799

WEB www.mandevillesign.com

APPROVALS	
Signatures Required Before Release to Production	
Engineering	DATE
Sales	DATE
Production	DATE
Quality Control	DATE

REVISIONS			
NO.	BY	DESCRIPTION	DATE
1	WD	move d/t structures	02/01/11
2	GL	update site plan	03/02/11
3	WD	update site plan	05/11/11
4	WD	update d/t addition	06/20/11
5			
6			
7			

STORE # 734

160 Ogden Ave.
DOWNER'S GROVE, IL

LOCATION

21401-1

WORK ORDER NUMBER(S)

PROJECT MANAGER JIM MANDEVILLE

NOTED	GL	01/25/11
SCALE	ARTIST	DATE

DRAWING NAME U-196-F2

DRAFT

VILLAGE OF DOWNERS GROVE
PLAN COMMISSION MEETING
PUBLIC HEARING

JULY 11, 2011, 7:00 P.M.

PC-18-11 A petition seeking a special use approval for a drive through for the property located at the northeast corner of Ogden Avenue and Cumnor Road, commonly known as 160 Ogden Avenue, Downers Grove, IL (PIN 09-04-112-038); Panera, LLC, Petitioner; J.J. Brothers, LLC, Owner

Chairman Jirik swore in those individuals who would be speaking on File PC-18-11.

Mr. Popovich discussed the petitioner was seeking approval of a special use for a drive-through window at an existing Panera Bread restaurant, zoned B-3 General Services and Highway Business, and located at the northeast corner of Cumnor Road and Ogden Avenue. The existing building sat at a 45-degree angle to the two streets. There are two access points to the commercial shopping area in which the restaurant was located -- one east on Ogden Avenue and one further north along Cumnor Road -- which would remain as is. No new access from Cumnor or Ogden is proposed.

Proposed was a 550 sq. ft. addition, clad in EIFS and brick material to match the existing building, which would be added to the existing southwest side of the building. To accommodate the new window (southwest facade), a new food preparation line will extend out from the existing food preparation line and new restrooms will be provided within the new addition. A 12-ft. drive-through lane was proposed with access to start at the northwest corner of the property and run counter-clockwise around the building, allowing for eight stacked cars. Cars would continue to the east and then exit east of the building. Staff was requiring that the petitioner include an automatic voice control on the menu board to reduce noise levels. Operational hours remained the same, i.e., 6:30 a.m. to 9:30 p.m.

Mr. Popovich reported that five parking stalls would be eliminated with the creation of the new drive-through lane but parking for the entire commercial shopping center would not be affected by the removal of the parking spaces. Eight existing trees and various shrubs would be removed and be replaced with new landscaping. Staff explained where the required landscaping would be located around the drive-through, i.e., the northwest corner and the southeast corner to provide buffering for the vehicles entering/exiting the drive-through lane.

Staff believed the proposal complied with and was consistent with the Zoning Ordinance and met the requirements of the Future Land Use Plan. No new curb cuts were planned for this site and internal circulation would basically remain the same. Utilities and sidewalks would also remain in their locations. The Downers Grove Sanitary District provided conceptual approval of the petition and there would be installation of some new stormwater drains.

The Fire Prevention District had no concerns but the building would be required to have sprinklers and an alarm system. A couple of general phone calls were received on this project. One resident asked whether new curb cuts would be installed and there was concern for traffic congestion at the intersection of Cumnor Road and Ogden Avenue.

DRAFT

Regarding the criteria for meeting the standards for special use, staff believed the use was a desirable service to the area and the proposal would not be detrimental to the health, safety and general welfare of the community. The use complied with the regulations of the Zoning Ordinance and a drive-through was a permitted special use in the B-3 Zoning District. Staff recommended the Plan Commission forward a positive recommendation subject to the conditions listed on page 4-5 in staff's report.

Discussion followed about the menu volume control and whether it was new, wherein Mr. Popovich explained that menu volume has been a concern in the past and staff was trying to be proactive by asking the petitioner to include it. Mr. Matejczyk supported the feature. Asked what staff was trying to accomplish by adding landscaping near the drive-through lane, Mr. Popovich said it was to provide additional screening and buffering. The landscaping along the sidewalk would provide a better delineation between the sidewalk and the drive-through to create a buffer between the pedestrians on the sidewalk and the vehicles. Mr. Beggs voiced concern that the landscaped buffering and a six-inch curb between a pedestrian and a vehicle was not enough protection for a pedestrian. He thought a physical barrier should be considered.

Regarding the sweep of the drive-through lane and the 15-foot distance from the sidewalk along Ogden Avenue to the nearest lane of traffic on Ogden, Chairman Jirik stated there would be headlights splashing right into the oncoming traffic. He suggested accomplishing this safety issue, along with the above issue, and avoiding the conflict simultaneously. Mr. Beggs concurred. Another concern raised was snow removal, wherein Mr. Popovich stated the owner would have to plow onto the corner green space or where other piles were located within the shopping center.

Mr. Russ Whitaker, III, attorney with Rosanova & Whitaker, Ltd., 23 W. Jefferson St., Naperville, IL, on behalf of Panera, LLC, introduced himself and stated that Panera LLC operated on the site since 1999. A brief history of the company followed. He discussed that due to the region's climate a drive-through was a "must have" for many operators in the northern climate. He appreciated working with staff on the proposal and staff's thorough presentation. He was open to answering questions.

Per Mr. Beggs' concern about protecting the pedestrian, Mr. Whitaker stated it could be something to consider and he was amenable to the suggestion. Chairman Jirik reminded the petitioner what the commission was trying to accomplish with the Ogden Avenue Master Plan, i.e., aesthetics, physical barriers to provide control/safety, and to come up with something tasteful and practical.

Mr. Waechtler asked if there were issues with vehicle stacking.

Mr. Tony Disanza, Panera LLC, 3630 S. Geyer Road, Sunset Hills, MO, responded that in previous drive-throughs there has not been more than 7 or 8 vehicles stacking at peak times (11:00 a.m. to 2:00 p.m.) and the total time from taking an order to picking it up was about 3.5 minutes. Mr. Waechtler noted that 7 vehicles were depicted on the diagram but having 8 or 9 vehicles would place them in the parking lot.

Chairman Jirik opened up the meeting to public comment. No comments followed. Public comment was closed. Petitioner also had no closing statement.

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WITH RESPECT TO FILE PC 18-11, MRS. RABATAH MADE A MOTION THAT THE PLAN COMMISSION MAKE A POSITIVE RECOMMENDATION TO THE VILLAGE COUNCIL REGARDING THE ABOVE PETITION SUBJECT TO THE FOLLOWING CONDITIONS:

1. THE SPECIAL USE SHALL SUBSTANTIALLY CONFORM TO THE STAFF REPORT DATED JULY 11, 2011 AND WITH PRELIMINARY ENGINEERING AND LANDSCAPE PLANS PREPARED BY CORE STATES GROUP, DATED MAY 4, 2011 AND REVISED ON JUNE 16, 2011; ARCHITECTURAL PLANS PREPARED BY FREDERICK J. GOGLIA, DATED JANUARY 4, 2011; AND SIGN DRAWINGS PREPARED BY MANDEVILLE SIGN, DATED JULY 15, 2010 AND REVISED ON JUNE 20, 2011 EXCEPT SUCH PLANS MAY BE MODIFIED TO CONFORM TO VILLAGE CODES AND ORDINANCES.
2. A DESCRIPTION OF BEST MANAGEMENT PRACTICES FOR STORMWATER THAT ARE INCORPORATED INTO THE SITE DESIGN AND HOW THEY WILL FUNCTION SHALL BE PREPARED AND SUBMITTED. THE APPLICABLE DESIGN CRITERIA SUCH AS SOIL TYPE, VEGETATION, AND LAND COVER CONDITIONS DRAINING TO THE BEST MANAGEMENT PRACTICE ALONG WITH APPROPRIATE CALCULATIONS SHALL BE INCLUDED.
3. THE PROPOSED ADDITION SHALL HAVE A MANUAL AND AUTOMATIC FIRE DETECTION SYSTEM INSTALLED THROUGHOUT IN A MANNER ACCEPTABLE TO THE VILLAGE. ALL AREAS OF THE BUILDING SHALL BE PROTECTED.
4. THE PROPOSED ADDITION SHALL HAVE A COMPLETE AUTOMATIC SPRINKLER SYSTEM INSTALLED THROUGHOUT IN A MANNER ACCEPTABLE TO THE VILLAGE. ALL AREAS OF THE BUILDING SHALL BE PROTECTED.
5. ALL PROPOSED SIGNAGE SHALL COMPLY WITH THE VILLAGE'S SIGN ORDINANCE.
6. A MINIMUM THREE-FOOT WIDE LANDSCAPING BED, CONTAINING SHRUBS OR ORNAMENTAL GRASSES, SHALL BE INSTALLED BETWEEN THE DRIVE-THROUGH LANE AND ADJACENT SIDEWALKS FOR THE ENTIRE LENGTH OF THE DRIVE-THROUGH LANE.
7. THE MENU BOARD SHALL BE EQUIPPED WITH AUTOMATIC VOLUME CONTROL; AND
8. THE PETITIONER SHALL PROVIDE A PROPOSAL, BASED ON THE DRIVE-THROUGH, TO ALLEVIATE THE SAFETY CONCERNS EXPRESSED ABOVE, WITH REGARD TO PEDESTRIAN SAFETY ON THE SIDEWALK AND THE HEADLIGHTS POTENTIALLY FLASHING ONTO THE ADJACENT STREET, AND THAT SUCH CONSIDERATION SHALL BE PROVIDED PRIOR TO VILLAGE COUNCIL CONSIDERATION.

SECONDED BY MR. WEBSTER.

Mr. Beggs called attention to the fact that the standards for approving the special use must provide for the safety of the community and if the Village Council was not comfortable with what the petitioner will present, Council should not approve the proposal. Mr. O'Brien conveyed that staff understood what the Plan Commission was looking for, i.e., a plan indicating increased screening

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and a physical barrier. Commissioners concurred they were comfortable with staff's understanding.

ROLL CALL:

**AYE: MRS. RABATAH, MR. WEBSTER, MR. BEGGS, MR MATEJCZYK,
MR. WAECHTLER, CHAIRMAN JIRIK**

NAY: NONE

MOTION CARRIED. VOTE: 6-0

October 31, 2011

Stan Popovich
Village of Downers Grove
801 Burlington Ave.
Downers Grove, IL 60515

Re: Panera Special Use Application

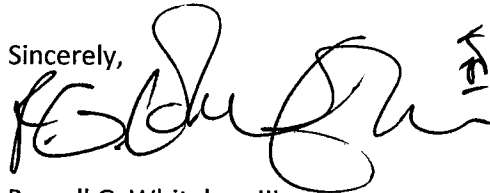
Dear Mr. Popovich,

Attached please find the final revised drawings for the proposed drive through addition for the existing Panera Bread located at 160 Ogden Avenue in Downers Grove. The revised plans include the following modifications:

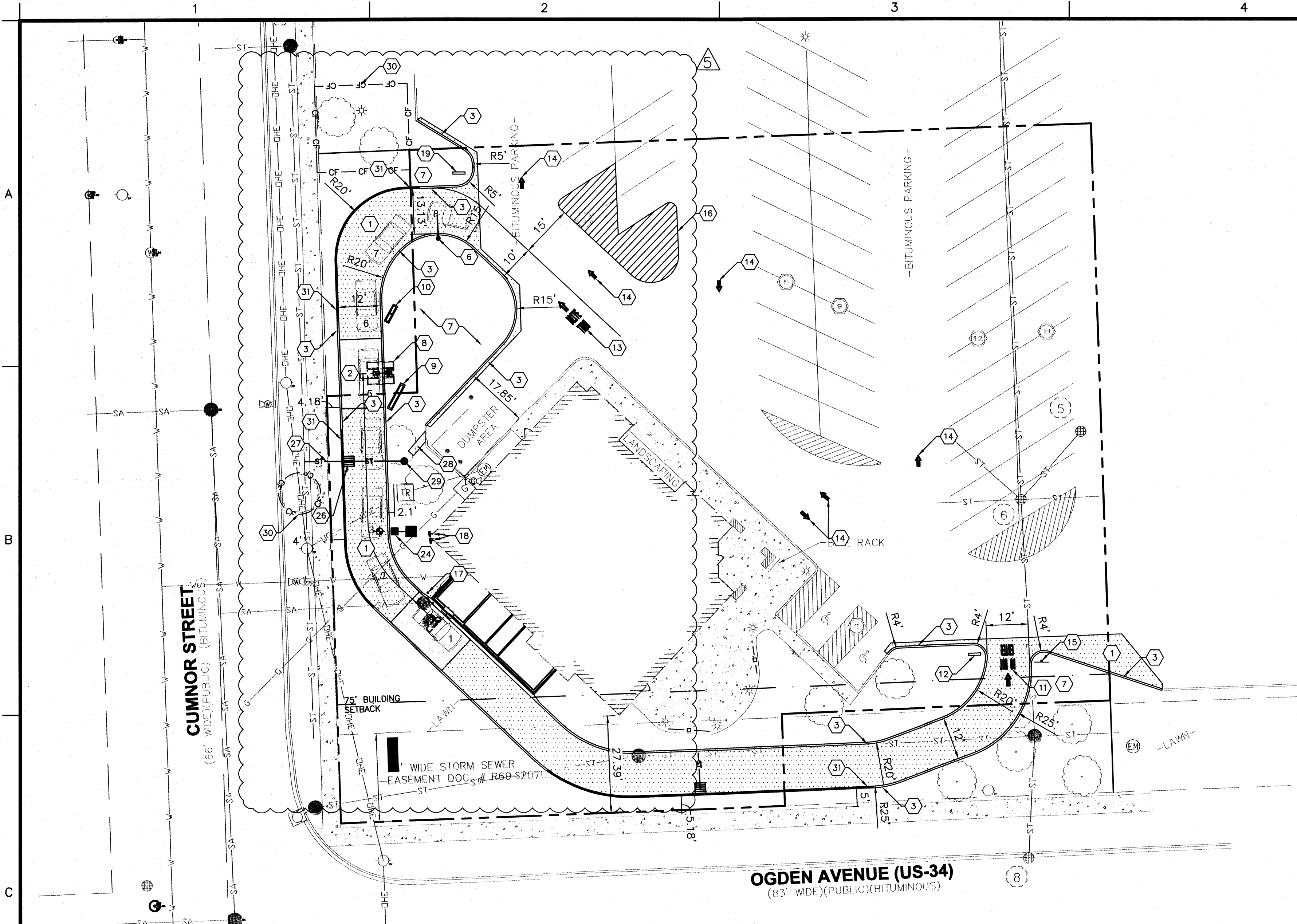
- The internal building layout has been reconfigured to facilitate automatic fire protection installations;
- The building addition has been reconfigured and shifted to the north commensurate with the new internal layout;
- The drive through entrance has been extended to the north in order to accommodate full code-required stacking;
- The front drive-aisle has been reconfigured and striped commensurate with the new drive through entrance location; and
- The distance between the drive through aisle and the property line has been maximized and a full landscape buffer has been incorporated.

These modifications address the issues raised by both staff and the Village Board in the initial review of the proposed plans. Should you have any questions or concerns please don't hesitate to call. We look forward to receiving final approvals and beginning construction of these improvements.

Sincerely,

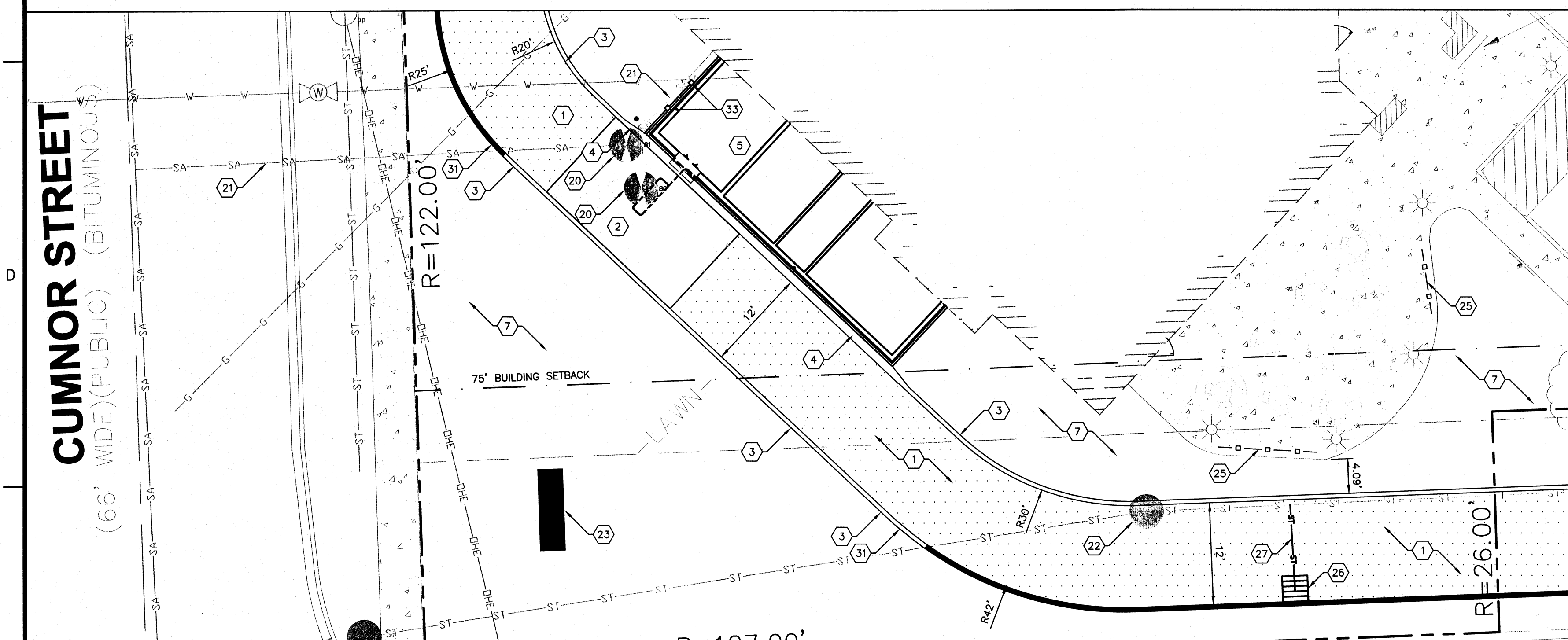


Russell G. Whitaker, III



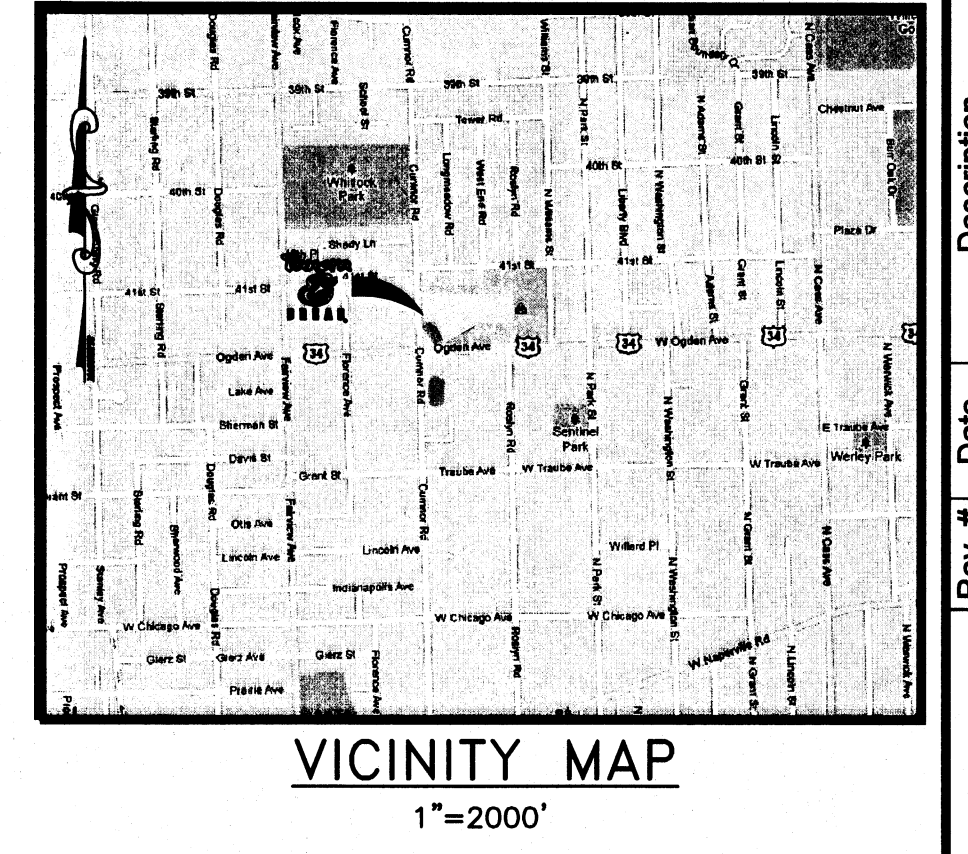
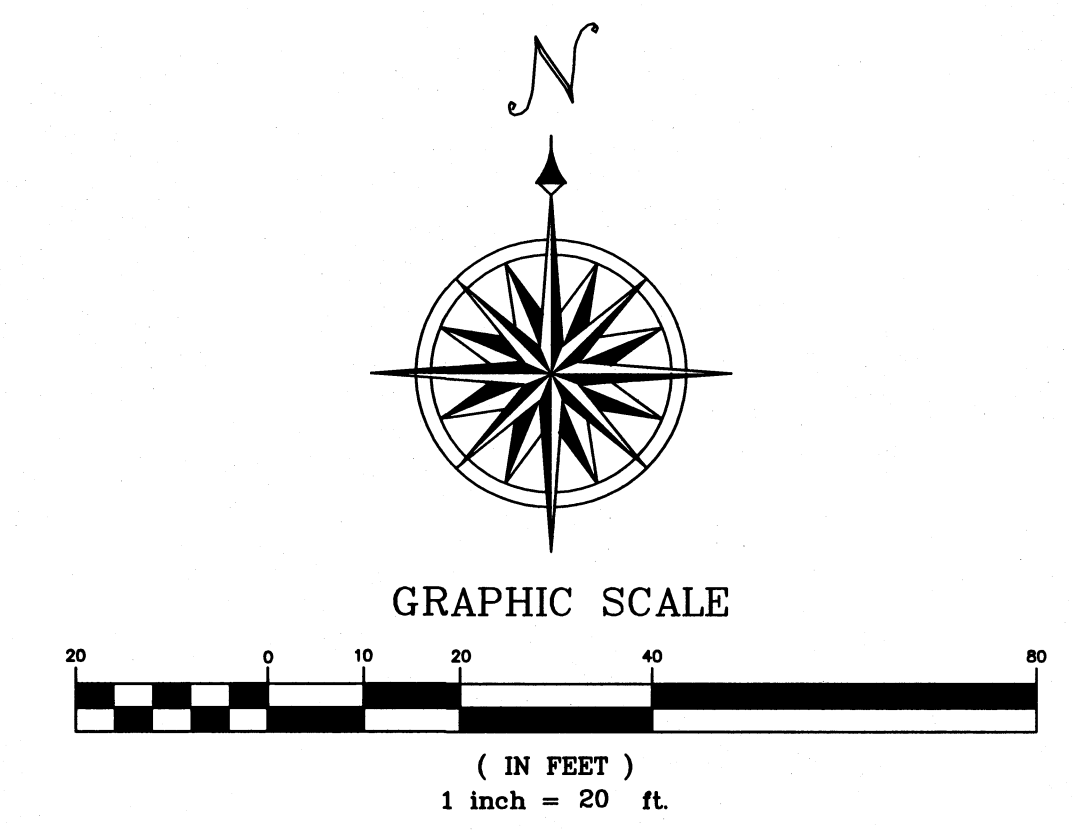
SITE / UTILITY PLAN:

SCALE: 1" = 20'



ENLARGED SITE/UTILITY PLAN:

SCALE: 1" = 10'



GENERAL NOTES

1. THESE PLANS ARE BASED ON DRAWINGS PREPARED BY FIRST AMERICAN, DATED 02/10/11. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THAT THE IMPROVEMENTS EXIST AS SHOWN ON THE SURVEY. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFYING CORE STATES GROUP OF ANY DEVIATIONS OR OMISSIONS THAT MAY EFFECT THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS.
2. ALL PHASES OF SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE OWNER / DEVELOPER SITE WORK SPECIFICATIONS.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF EXISTING STRUCTURES, RELATED UTILITIES, PAVING, UNDERGROUND STORAGE TANKS AND OTHER EXISTING IMPROVEMENTS AS NOTED.
4. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
5. THE GENERAL CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR AND SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
6. ALL CONSTRUCTION IN RIGHT-OF-WAY SHALL BE COORDINATED WITH THE APPROPRIATE AGENCY(IES) HAVING AUTHORITY.
7. **WARRANTY/DISCLAIMER:**
THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER THE ENGINEER NOR ITS PERSONNEL CAN OR DO WARRANT THESE DESIGNS OR PLANS AS CONSTRUCTED EXCEPT IN THE SPECIFIC CASES WHERE THE ENGINEER INSPECTS AND CONTROLS THE PHYSICAL CONSTRUCTION ON A CONTEMPORARY BASIS AT THE SITE.
8. **SAFETY NOTICE TO CONTRACTOR:**
IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL REQUIREMENTS THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. ANY CONSTRUCTION OBSERVATION BY THE ENGINEER OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE.

DOWNERS GROVE SANITARY DISTRICT NOTES

1. THE DOWNERS GROVE SANITARY DISTRICT STANDARDS AND ORDINANCES SHALL GOVERN ALL SANITARY SEWER CONSTRUCTION.
2. THE SEWER CONTRACTOR SHALL SCHEDULE WITH THE DISTRICT INSPECTIONS OF THE SANITARY SEWER CONSTRUCTION 48 HOURS IN ADVANCE OF THE START OF CONSTRUCTION (630-969-0664).
3. THE CONNECTION INTO THE EXISTING MANHOLE SHALL BE MADE BY CORE DRILLING THE MANHOLE AND INSTALLING A RUBBER BOOT TO INSURE A WATERTIGHT SEAL. THE MANHOLE BENCH SHALL ALSO BE REFORMED TO PROVIDE A SMOOTH FLOW INVERT.
4. THE CONSTRUCTED SEWERS SHALL PASS ALL DISTRICT REQUIREMENTS FOR AIR TESTING, TELEVISION AND MANHOLE VACUUM TESTS. IF THE GREASE TRAP IS TO BE RE-LOCATED OR RE-USED IT MUST BE BUILT TO EXISTING SANITARY DISTRICT STANDARDS AND BE ABLE TO PASS A VACUUM TEST.
5. "FLEX SEAL" NON-SHEAR COUPLINGS (WITH STAINLESS STEEL SHEAR RING) SHALL BE USED TO CONNECT PIPES OF DISSIMILAR SIZE.
6. THE FOLLOWING PIPE SPECIFICATION SHALL BE USED: PVC PIPE WITH A SDR OF 26, COMPLYING WITH ASTM D2241, 160 PSI PRESSURE PIPE PUSH ON BELL AND SRIGOT TYPE WITH RUBBER RING SEAL GASKET ASTM D3139.

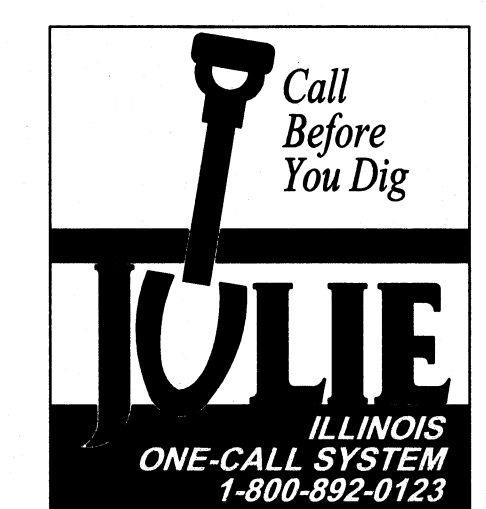
SITE NOTES

- A. REFER TO SHEET C-1 FOR DEMOLITION.
- B. REFER TO SHEET C-3 FOR GRADING AND DRAINAGE.
- C. REFER TO SHEET C-3 FOR GENERAL LEGEND.
- D. ALL DIMENSIONS ARE TO GROUND LEVEL IMPROVEMENTS (FACE OF CURB, CONCRETE SLAB, ETC.) UNLESS NOTED OTHERWISE.
- E. CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR LOCATIONS AND DIMENSIONS OF SIDEWALKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.

KEYED NOTES

1. NEW ASPHALT PAVEMENT. REFER TO DETAIL ON SHEET C-4. NOTE: GC SHALL PROVIDE ALTERNATE BID FOR CONCRETE PAVING.
2. NEW CONCRETE PAD WITH SENSOR LOOP. REFER TO DETAIL ON SHEET C-4 FOR CONCRETE PAD AND LOOP DETAILS.
3. NEW CONCRETE CURB (STANDARD SECTION). REFER TO DETAIL ON SHEET C-4.
4. NEW CONCRETE CURB (WIDE SECTION). REFER TO DETAIL ON SHEET C-4.
5. NEW DRIVE-THRU ADDITION. REFER TO ARCHITECTURAL PLANS.
6. NEW CLEARANCE BAR. REFER TO ARCHITECTURAL PLANS.
7. NEW LANDSCAPE AREA. REFER TO SHEET L-1.
8. NEW MENU CANOPY. REFER TO ARCHITECTURAL PLANS.
9. NEW MENU BOARD. REFER TO ARCHITECTURAL PLANS.
10. NEW PREVIEW BOARD. REFER TO ARCHITECTURAL PLANS.
11. NEW "DO NOT ENTER" STRIPING AND TRAFFIC FLOW ARROWS. REFER TO DETAIL ON SHEET C-4.
12. NEW "DO NOT ENTER / THANK YOU" SIGN. REFER TO ARCHITECTURAL PLANS.
13. NEW "DRIVE-THRU" STRIPING AND TRAFFIC FLOW ARROW. REFER TO DETAIL ON SHEET C-4.
14. NEW TRAFFIC FLOW ARROW. REFER TO DETAIL ON SHEET C-4.
15. NEW "STOP" SIGN. REFER TO DETAIL ON SHEET C-4.
16. NEW STRIPING / NO PARKING AREA. REFER TO DETAIL ON SHEET C-4.
17. NEW BOLLARD(S). REFER TO DETAIL ON SHEET C-4. REFER TO ARCHITECTURAL PLANS FOR LOCATION AND QUANTITY.
18. NEW BUILDING MOUNTED CAMERA(S). REFER TO ARCHITECTURAL PLANS.
19. NEW "DRIVE-THRU" SIGN. REFER TO ARCHITECTURAL PLANS.
20. GREASE INTERCEPTOR TO BE REINSTALLED IN LOCATION SO AS TO NOT INTERFERE WITH SENSOR LOOP OVER COMPACTED SUBGRADE AND BACKFILLED WITH GRAVEL. EXISTING LATERALS TO BE EXTENDED TO NEW LOCATION AS NEEDED AND INTERCEPTOR TO BE RECONNECTED TO MAIN LINE.
21. APPROXIMATE LOCATION OF EXISTING SANITARY LATERAL. GC TO FIELD VERIFY. CONTRACTOR TO RELOCATE AS NEEDED FOR CONSTRUCTION.
22. APPROXIMATE LOCATION OF MANHOLE TO BE ADJUSTED TO GRADE TO MATCH NEW ELEVATION. GENERAL CONTRACTOR TO FIELD VERIFY. REFER TO GRADING PLAN ON SHEET C-3.
23. NEW MONUMENT SIGN.
24. RELOCATED LIGHT STANDARD WITH NEW CONCRETE BASE.
25. NEW CEDAR SCREENS. REFER TO ARCHITECTURAL PLANS.
26. NEW GRATED STORM WATER INLET. REFER TO DETAIL ON SHEET C-5.
27. NEW STORM PIPE.
28. NEW 2' CONCRETE FLUME. REFER TO DETAIL ON SHEET C-5.
29. NEW YARD DRAIN. TOP 750.70'
30. PARKWAY TREE PROTECTION CHAINLINK FENCE. REFER TO DETAIL ON SHEET C-5.
31. NEW CONCRETE CURB (TALL SECTION). SOLID HATCH. REFER TO DETAIL ON SHEET C-5.
32. NOT USED.
33. NEW DOWNSPOUT WITH UNDERGROUND PIPE TO EXIT AT CURB FACE. REFER TO ARCHITECTURAL PLANS.

CONSTRUCTION ENTRANCE NOTE:
GENERAL CONTRACTOR SHALL ONLY USE THE PARKING LOT AS A CONSTRUCTION ENTRANCE. NO OTHER ACCESS ACROSS THE VILLAGE RIGHT-OF-WAY WILL BE ALLOWED. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.

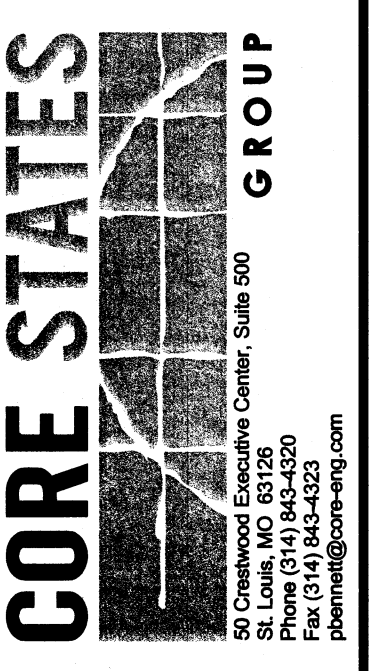
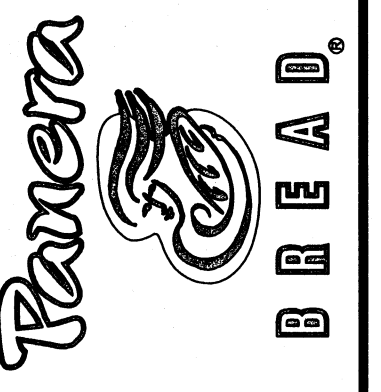


NOTE:
GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND UNCOVERING EXISTING UTILITIES IN THE VICINITY OF THE PROPOSED DRIVE THRU PRIOR TO THE START OF CONSTRUCTION TO ASCERTAIN EXACT MATERIALS, LOCATIONS, ELEVATIONS, ETC. AND THEIR POTENTIAL CONFLICT WITH PROPOSED IMPROVEMENTS. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON DESIGN DRAWINGS, RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. CORE STATES, INC. DOES NOT GUARANTEE THAT LOCATIONS SHOWN ARE EXACT. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF UTILITIES.

Rev. #	Date	Description
01	06/16/2011	REVISIONS ADDITION/OT COMMENTS
02	07/05/2011	SANITARY SEWER DISTRICT COMMENTS
03	07/26/2011	LANDSCAPING AND TREE CARES
04	08/02/2011	REVISIONS ADDITION
05	10/24/11	NEW DRIVE THRU ADDITION

Panera, LLC
3630 South Geyer Rd., Suite 100 St. Louis, MO 63127
Cafe # 734
Drive-Thru Addition
160 Ogden Ave., Downers Grove, IL 60515
Site and Utility Plan

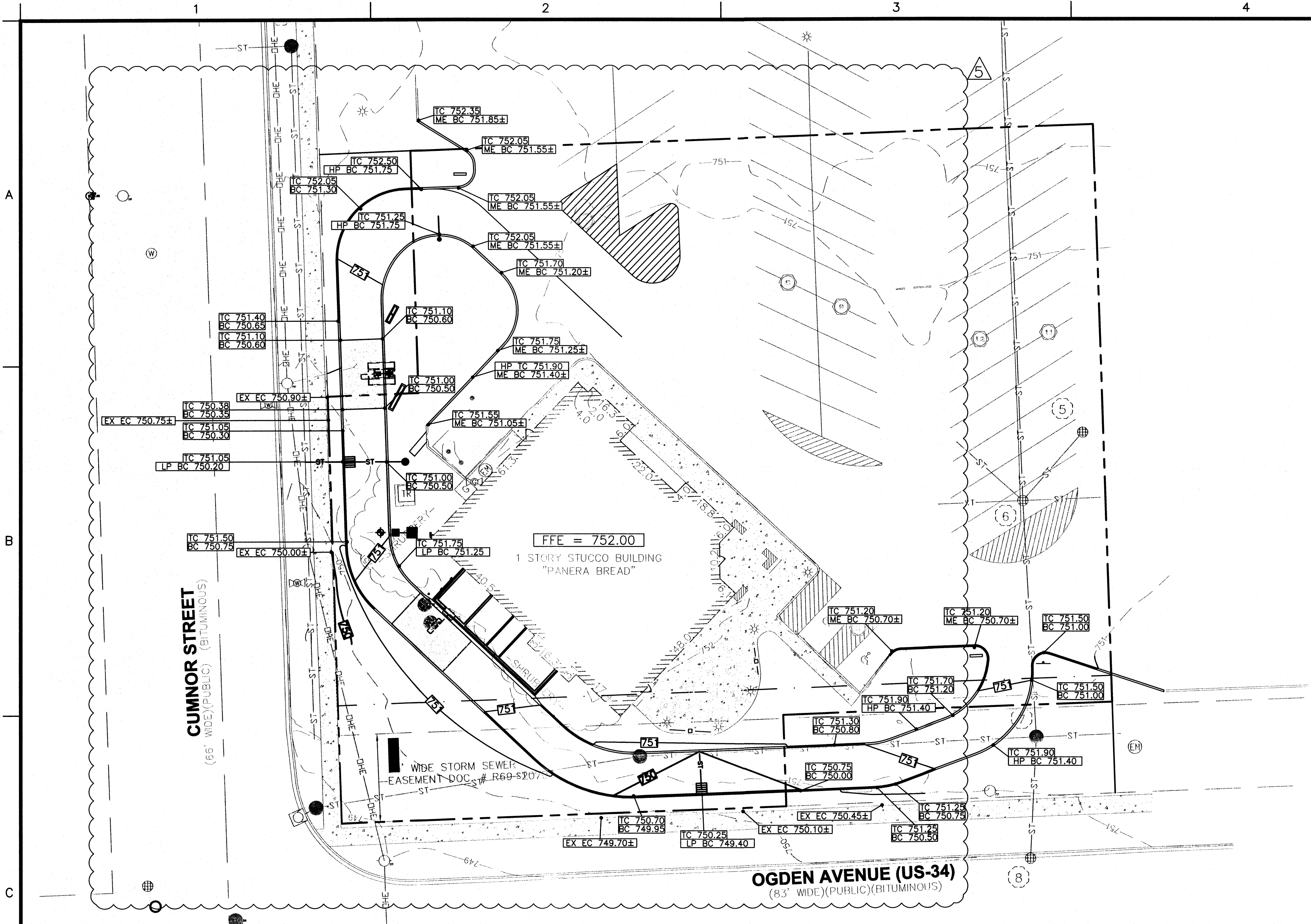


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Scale: 1"=20'
Date: 05-04-11
Drawn By: MAB
Checked By: PTB

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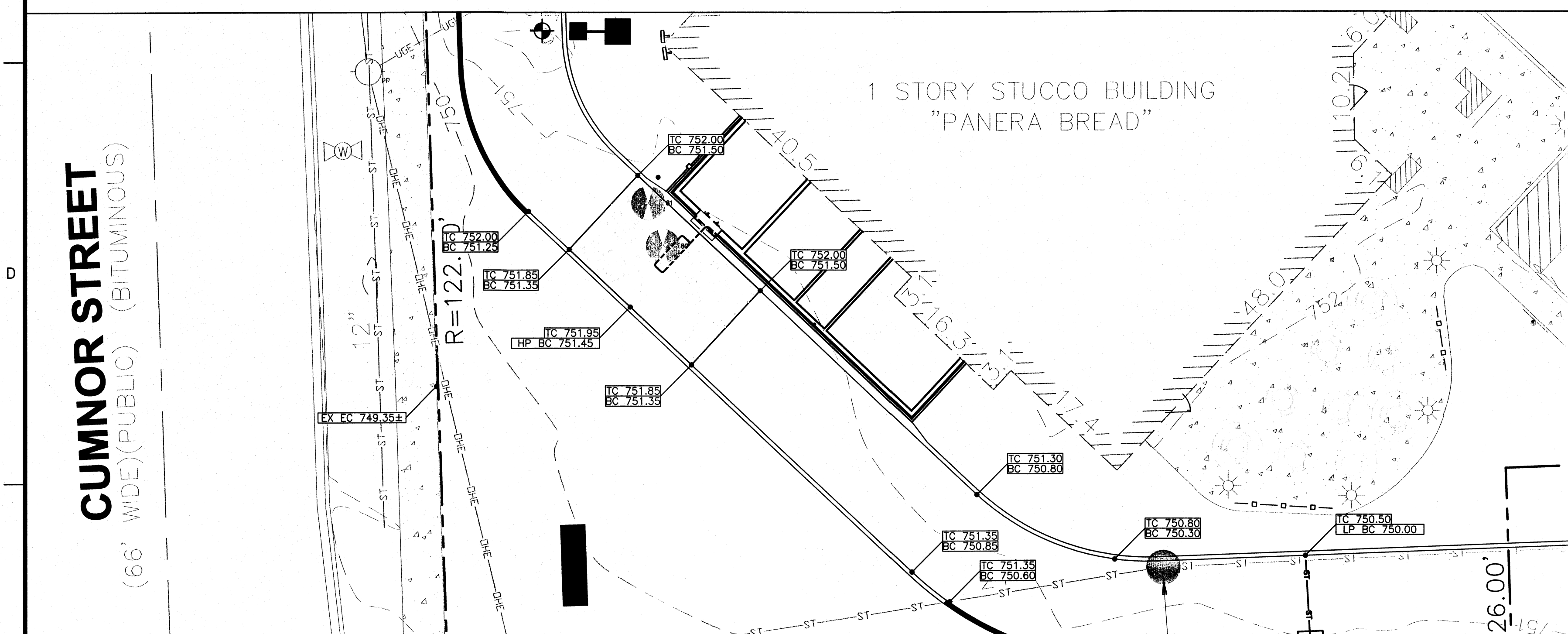
PATRIK T. BENNETT
02-055522
Signature
DATE
05/04/11
05/04/11

Plot Date/Time: 2011-10-25 - 08:18:44. User: LWI/TAKES. Drawing: ARC-12170 CD (Panera #734 - Downers Grove, IL) for 4.5.dwg



GRADING PLAN:

SCALE: 1" = 20'



ENLARGED GRADING PLAN:

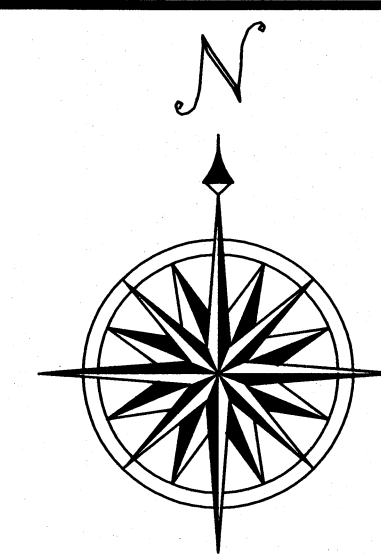
SCALE: 1" = 10'

GRADING NOTES

- A. REFER TO SHEET C-2 FOR DIMENSIONS AND IDENTIFICATION OF PROPOSED IMPROVEMENTS.
- B. REFER TO SHEET C-2 FOR ABOVE AND BELOW GROUND SEWERS AND UTILITIES.

ABBREVIATIONS

- BC BOTTOM OF CURB
- EC EDGE OF CONCRETE
- EX EXISTING
- GS GROUND SHOT
- HP HIGH POINT
- LP LOW POINT
- ME MATCH EXISTING
- TC TOP OF CURB
- TP TOP OF PAVEMENT



LEGEND	
EXISTING	PROPOSED
ASPHALT PAVEMENT (HEAVY DUTY)	[Symbol]
ASPHALT PAVEMENT (STANDARD DUTY)	[Symbol]
BENCHMARK	[Symbol]
BOLLARD (PIPE)	[Symbol]
BOLLARD (PIPE GUARD)	[Symbol]
BUSH/SHRUB	[Symbol]
CONCRETE PAVEMENT	[Symbol]
CONTOUR - MAJOR	[Symbol]
CONTOUR - MINOR	[Symbol]
CURB - TALL SECTION	[Symbol]
ELECTRIC - GUY WIRE	[Symbol]
ELECTRIC - LIGHT	[Symbol]
ELECTRIC - METER	[Symbol]
ELECTRIC - OVERHEAD LINE	[Symbol]
ELECTRIC - POWER POLE	[Symbol]
ELECTRIC - TRANSFORMER	[Symbol]
ELECTRIC - UNDERGROUND LINE	[Symbol]
FENCE-BARBWIRE	[Symbol]
FENCE-CHAINLINK	[Symbol]
FENCE-CONSTRUCTION	[Symbol]
FENCE-SILT	[Symbol]
FENCE-WOOD	[Symbol]
FLOWLINE OF STREAM	[Symbol]
GAS - GAS LINE	[Symbol]
GAS - METER	[Symbol]
GAS - VALVE	[Symbol]
PARKING STALL NUMBER	[Symbol]
SANITARY SEWER - CLEANOUT	[Symbol]
SANITARY SEWER - MANHOLE	[Symbol]
SANITARY SEWER - SANITARY LINE	[Symbol]
SANITARY SEWER - STRUCTURE LABEL	[Symbol]
SIGN	[Symbol]
SPOT ELEVATION	[Symbol]
STORM SEWER - CLEANOUT	[Symbol]
STORM SEWER - CURB INLET	[Symbol]
STORM SEWER - DOUBLE CURB INLET	[Symbol]
STORM SEWER - FLARED END SECTION	[Symbol]
STORM SEWER - GRATE INLET	[Symbol]
STORM SEWER - DOUBLE GRATE INLET	[Symbol]
STORM SEWER - MANHOLE	[Symbol]
STORM SEWER - MANHOLE (GRATED)	[Symbol]
STORM SEWER - STORM LINE	[Symbol]
STORM SEWER - STRUCTURE LABEL	[Symbol]
TELEPHONE - BOX	[Symbol]
TELEPHONE - TELEPHONE LINE	[Symbol]
TREE - GENERAL	[Symbol]
WATER - FIRE HYDRANT	[Symbol]
WATER - METER	[Symbol]
WATER - WATER LINE	[Symbol]
WATER - VALVE	[Symbol]
WATER - ENDCAP	[Symbol]

CONSTRUCTION ENTRANCE NOTE:
GENERAL CONTRACTOR SHALL ONLY USE THE PARKING LOT AS A CONSTRUCTION ENTRANCE. NO OTHER ACCESS ACROSS THE VILLAGE RIGHT-OF-WAY WILL BE ALLOWED. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.

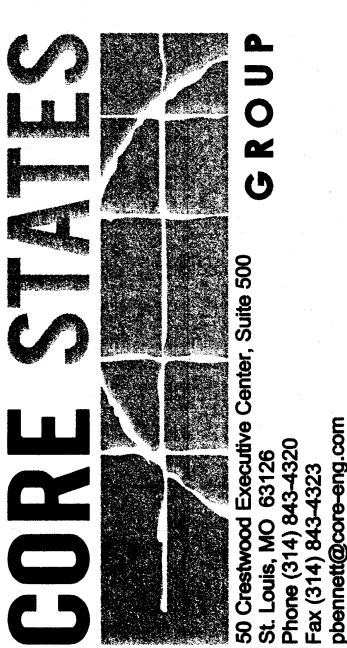
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Rev. #	Date	Description
01	06/16/2011	REDESIGNED ADDITIONAL DISTRICT COMMENTS
02	07/05/2011	SANITARY SEWER DISTRICT COMMENTS
03	07/29/2011	STORM SEWER DISTRICT COMMENTS
04	08/24/11	NEW DRIVE THRU ADDITION
05	10/24/11	NEW DRIVE THRU ADDITION

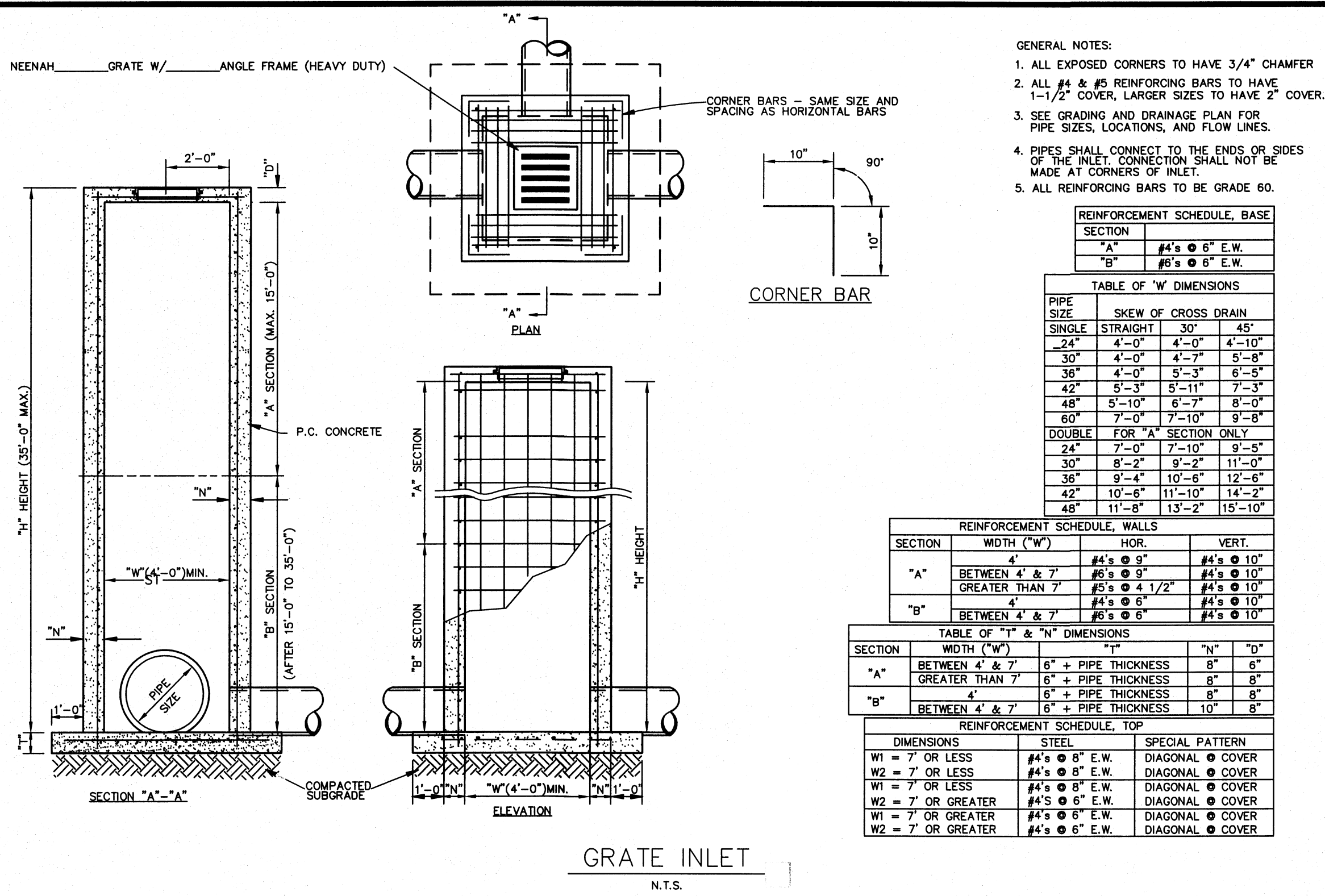
Panera, LLC
3630 South Geyer Rd., Suite 100 St. Louis, MO 63127
Cafe # 734
Drive-Thru Addition
160 Ogden Ave., Downers Grove, IL 60515
Grading Plan



Job#: ARC-12170
Scale: 1"=20'
Date: 05-04-11
Drawn By: MAB
Checked By: PTB

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Signature: [Signature]
Date: 05 25 2011



Municipal Codes regarding trees, including tree protection requirements for public parkway trees, are located in Chapter 24 of the Downers Grove Municipal Code <http://www.downers.us/code/chapters/24>. Parkway tree protection shall involve avoiding damage to both the above ground tree trunk, including the branches, and the below ground root system. Roots are the most vital part of a tree with the majority of nutrient and water absorbing roots in the upper 18 to 24 inches of soil. Tree roots must be protected from severing or changes in their soil environment (such as compaction or grade changes) to prevent irreversible tree decline or death in the coming years.

The Critical Root Zone, or CRZ, is the area immediately surrounding a tree that needs to be protected from damage. The size of this area, measured from the center of the tree, is ideally a circle with a radius of one foot for each inch of trunk diameter. The depth of the CRZ extends to 4 feet below the natural ground surface level. In a municipal parkway setting with utilities and paved or concrete surfaces, the CRZ cannot always be the ideal size. Instead, the CRZ has been adjusted to form a rectangle around the parkway tree trunk with the minimum dimensions listed in the following table. At a minimum, the listed CRZ shall be fenced with a 6 foot high temporary chain link construction fence secured to metal posts spaced no further than 10 feet apart. Whenever possible, the entire parkway shall be fenced in except where access has been permitted. Any exceptions shall be noted on the drawings submitted for a given permit.

PARKWAY TREE DIAMETER AT 4.5' (MINIMUM CURB TO SIDEWALK)	WIDTH FROM STREET TO PROPERTY	LENGTH ALONG STREET (MINIMUM)	DEPTH
0-12.0 INCHES	10.0 FEET	20 FEET	4 FEET
12.1-24.0 INCHES	10.0 FEET	30 FEET	4 FEET
24.1 OR MORE INCHES	10.0 FEET	30 FEET	4 FEET

For public parkway trees, roots located within the determined CRZ shall be protected from compaction, severing, and the storage of materials or equipment. Utilities must be cased underneath the tree as shown above. In cases when severing of roots within a portion of the CRZ may be unavoidable (ex. sidewalk installation, curb replacement, water main or sanitary main disconnects in the parkway), subject to the approval of the Village Forester, the smallest possible area shall be disturbed and sharp clean cuts shall be made on root ends to promote wound closure and root regeneration. All CRZ fencing shall be a 6 foot high temporary chain link construction fence secured to metal posts spaced no further than 10 feet apart, and shall be maintained daily in good condition. Any exceptions to the fence dimensions or parkway position shall be noted on the permit.

In addition to fines and citations that may be assessed for violations of any Chapter 24 municipal code (such as not maintaining fencing around the CRZ or unauthorized removal of parkway trees), violators may be subject to the following provisions:

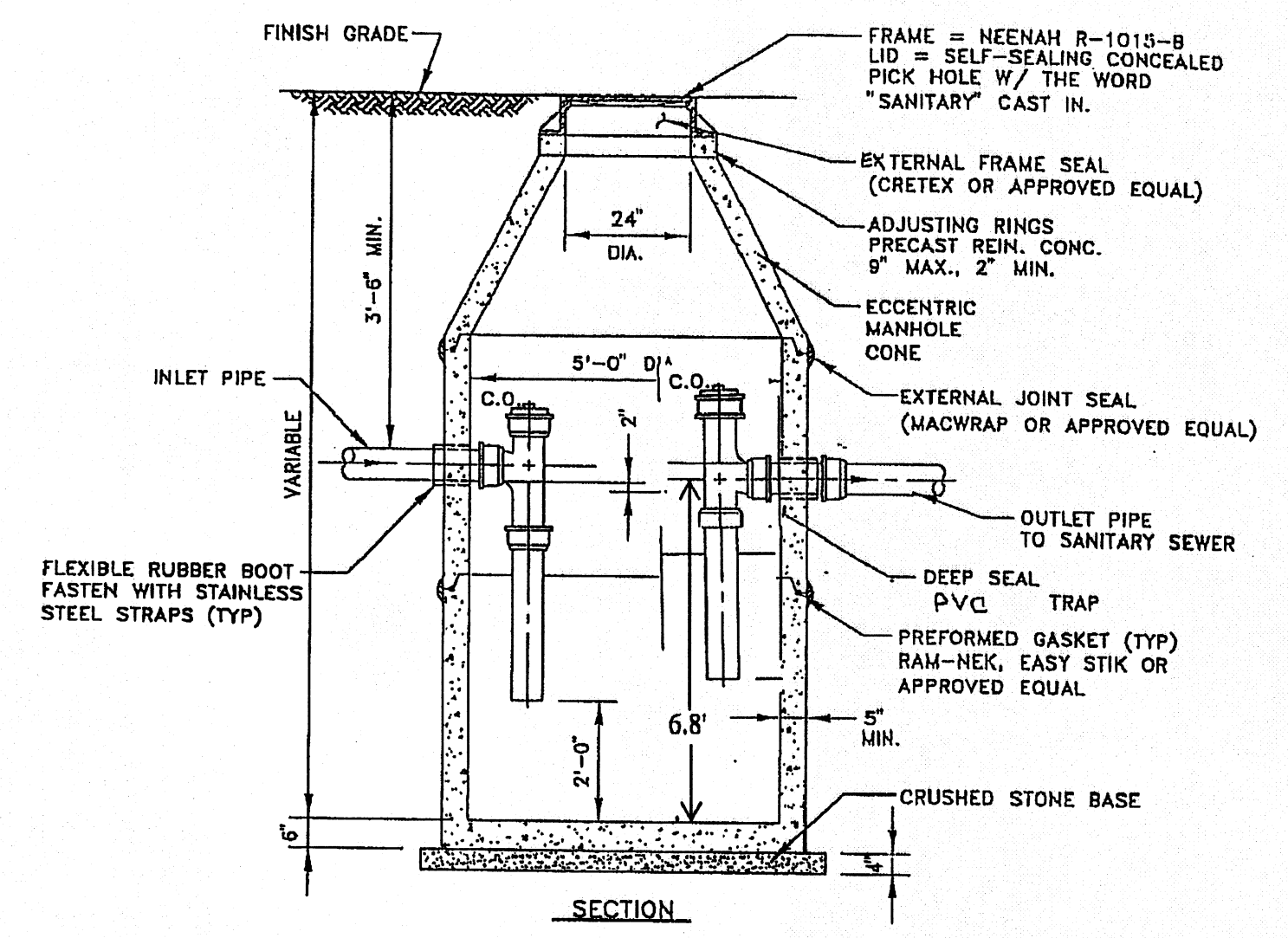
- issuance of an invoice for the monetary loss in tree value or partial value due to damage to either the above ground or below ground portions of the parkway tree, or unauthorized tree removal.
- forfeiture of bonds issued for the work should funds be sufficient to cover tree values and fines.
- costs of repairs, such as pruning or cabling, or costs for removal of the damaged parkway tree along with the stump if the tree cannot remain in the right-of-way.
- fines of \$500 for the 1st offense; \$1,000 for the 2nd offense; \$2,500 for 3rd and subsequent offenses.
- each day during which a violation continues shall be construed as a separate and distinct offense.

For more information, contact the Forestry Division at 434-5475 or 434-5476.

N.T.S.	DATE	REVISIONS	DRAWN BY	APP'D BY	STANDARD DETAIL
	02/20/07		J.M.L.	M.D.M.	PARKWAY TREE PROTECTION REQUIREMENTS

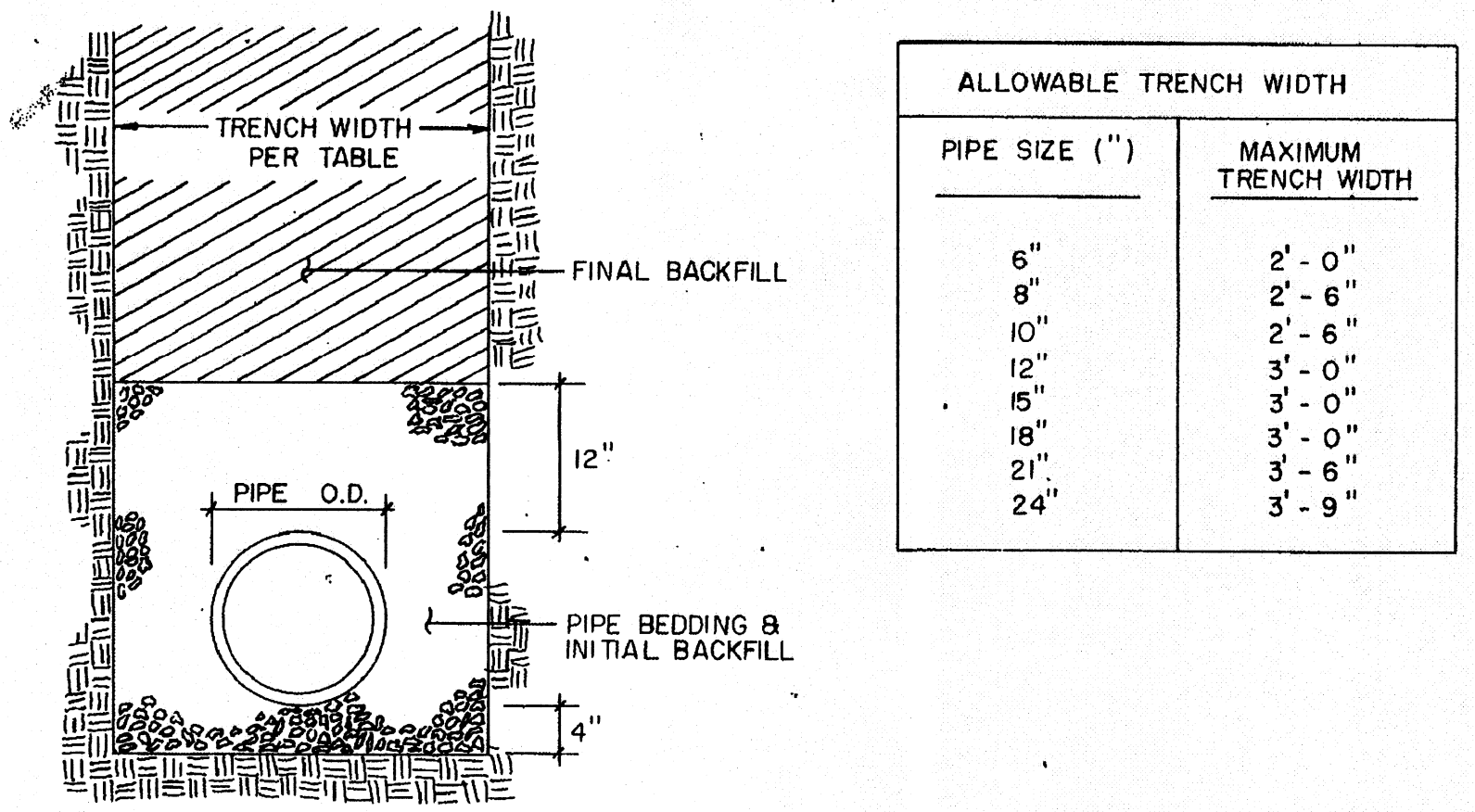
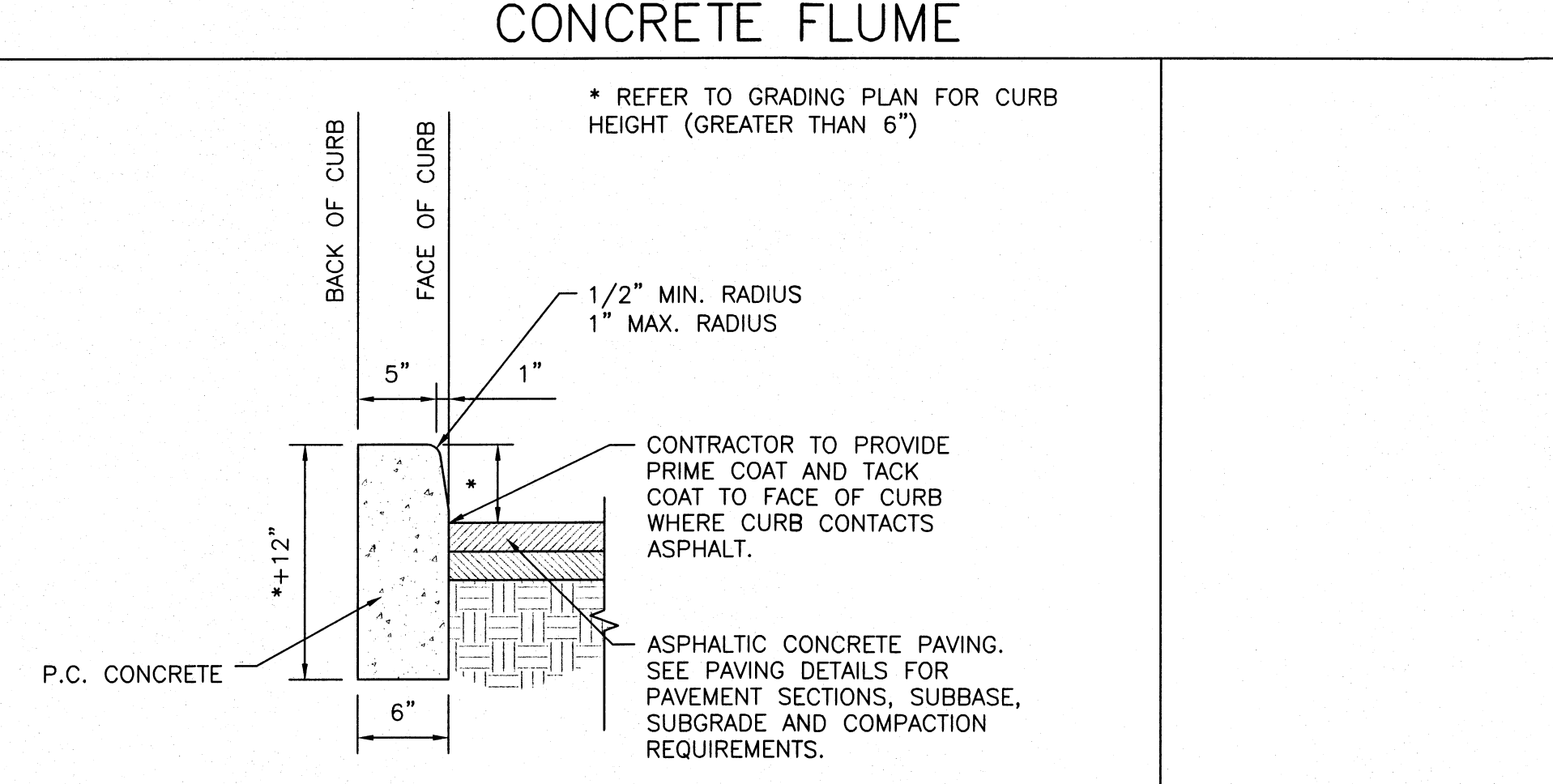
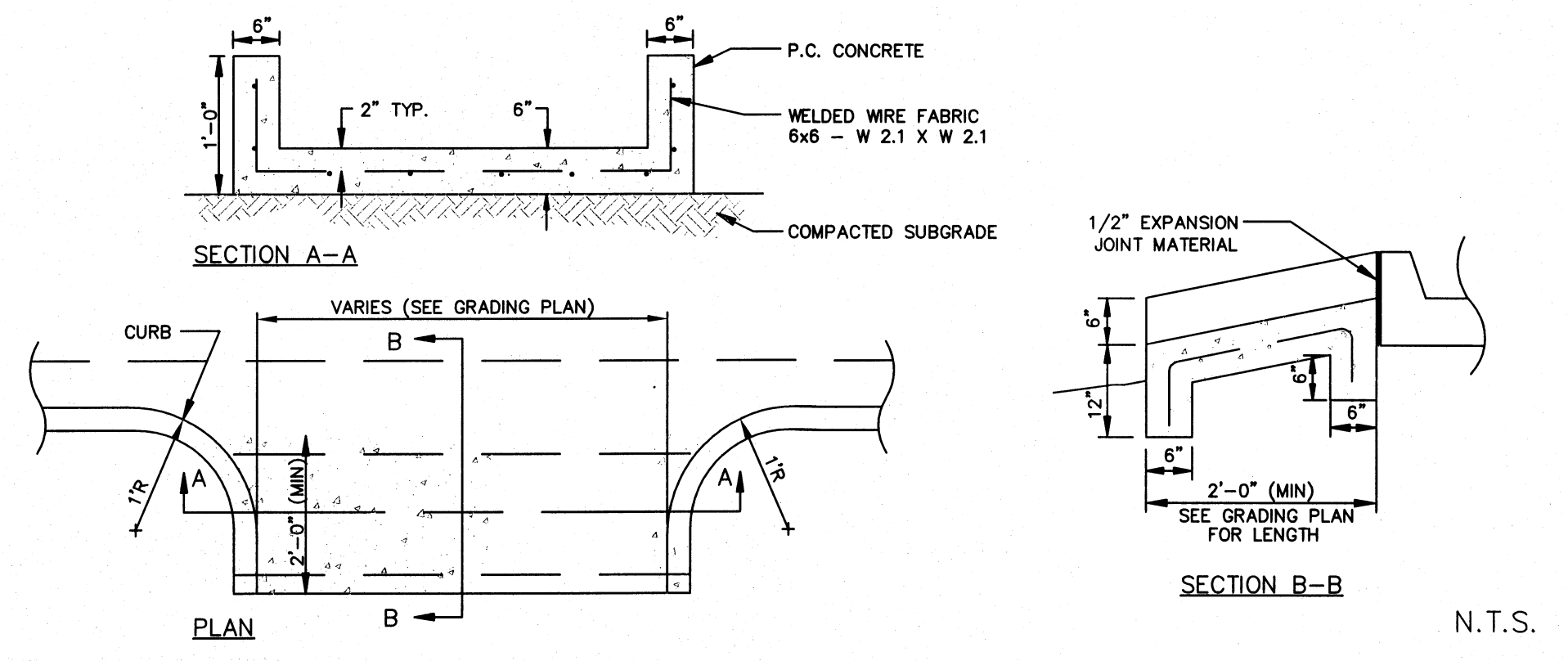
DRAWING NO. TRE-01
I:\PW - ENG & TRANSPORTATION\DESIGN DETAILS-FEB 07\TREES\TRE-01

1,000 Gallon Manhole Grease Trap



GENERAL NOTES:

- PRECAST REINFORCED CONCRETE RISERS, TOP AND BOTTOM SHALL CONFORM TO ASTM C-478.
- WHERE REQUIRED, WATERPROOF FRAMES SHALL BE NEENAH R-1919 W/ LD AS ABOVE OR APPROVED EQUAL.
- THE SPACE BETWEEN THE CONE, ADJUSTING RINGS AND FRAME SHALL BE COMPLETELY SEALED WITH PREFORMED BITUMINOUS MASTIC GASKET.



PIPE BEDDING & INITIAL BACKFILL MATERIALS SHALL BE LIMITED TO CRUSHED STONE 1/4" TO 3/4" IN DIAMETER, PLACED IN MAXIMUM 6" LIFTS AND COMPACTED TO 95% STANDARD PROCTOR DENSITY PER AASHTO T-99.

FINAL BACKFILL IN PAVEMENT AREAS AND WHERE TRENCH FALLS WITHIN A 1 TO 1 SLOPE EXTENDED FROM THE EDGE OF PAVEMENT, MATERIAL SHALL BE GRANULAR TRENCH-BACKFILL AS SPECIFIED BY THE CONTROLLING ROAD AUTHORITY (FRS FOR I.D.O.T. - CA 6 FOR VILLAGES OF DOWNERS GROVE AND WESTMONT) COMPACTED TO 95% STANDARD PROCTOR DENSITY PER AASHTO T-99.

IN LANDSCAPE AREAS MATERIAL SHALL BE SELECT EXCAVATED MATERIAL, FREE OF ROCKS AND DEBRIS COMPACTED TO 85% STANDARD PROCTOR DENSITY PER AASHTO T-99.

PIPE BEDDING & BACKFILL

N.T.S.
DOWNERS GROVE SANITARY DISTRICT

Rev. #	Date	Description
01	06/16/2011	REDESIGNED ADDITION/NOT COMMENTS
02	07/02/2011	SANITARY SEWER DISTRICT COMMENTS
03	07/26/2011	LANDSCAPING AND TALL CURBS
04	08/01/2011	REV. DRIVE THRU ADDITION
05	07/24/11	REV. DRIVE THRU ADDITION

Job#: ARC-12170
Scale: N.T.S.
Date: 05-04-11
Drawn By: MAB
Checked By: PTB

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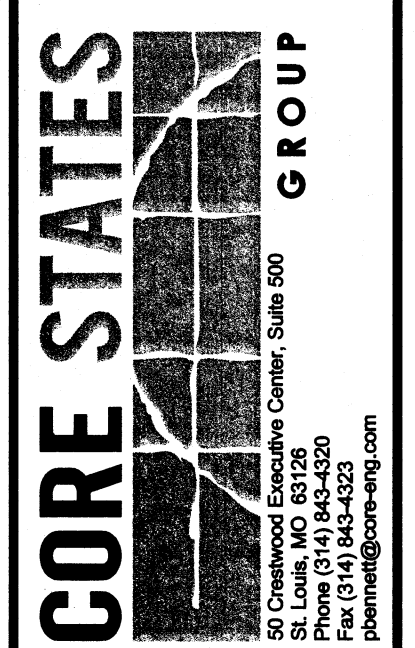
CoreStates GROUP
1000 E. Wood Dale Center, Suite 500
Wood Dale, IL 60191
Phone (314) 843-4320
Fax (314) 843-4320
www.corestatesgroup.com

Signature: PATRICK T. BENNETT
Date: 5-2-2011

REVISED JAN. 1998

5 OF 6

Panera, LLC
3630 South Geyer Rd., Suite 100 St. Louis, MO 63127
Cafe # 734
Drive-Thru Addition
160 Ogdan Ave., Downers Grove, IL 60515
Detail Sheet



Job#: ARC-12170
Scale: N.T.S.
Date: 05-04-11
Drawn By: MAB
Checked By: PTB

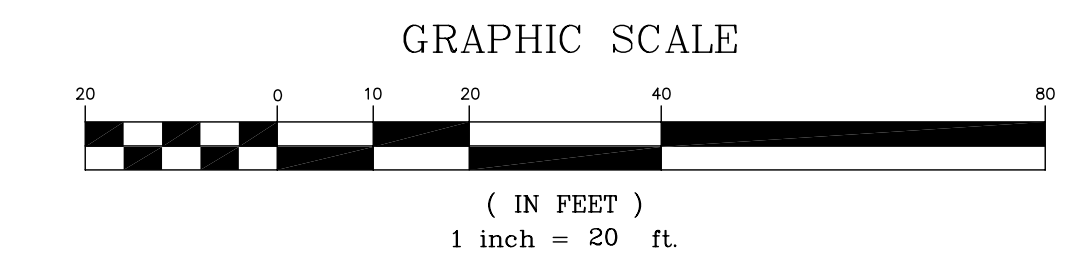
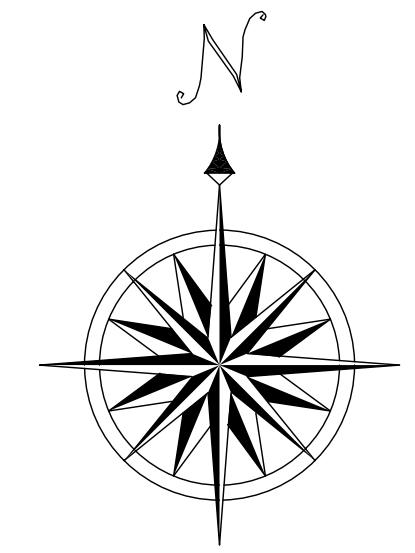
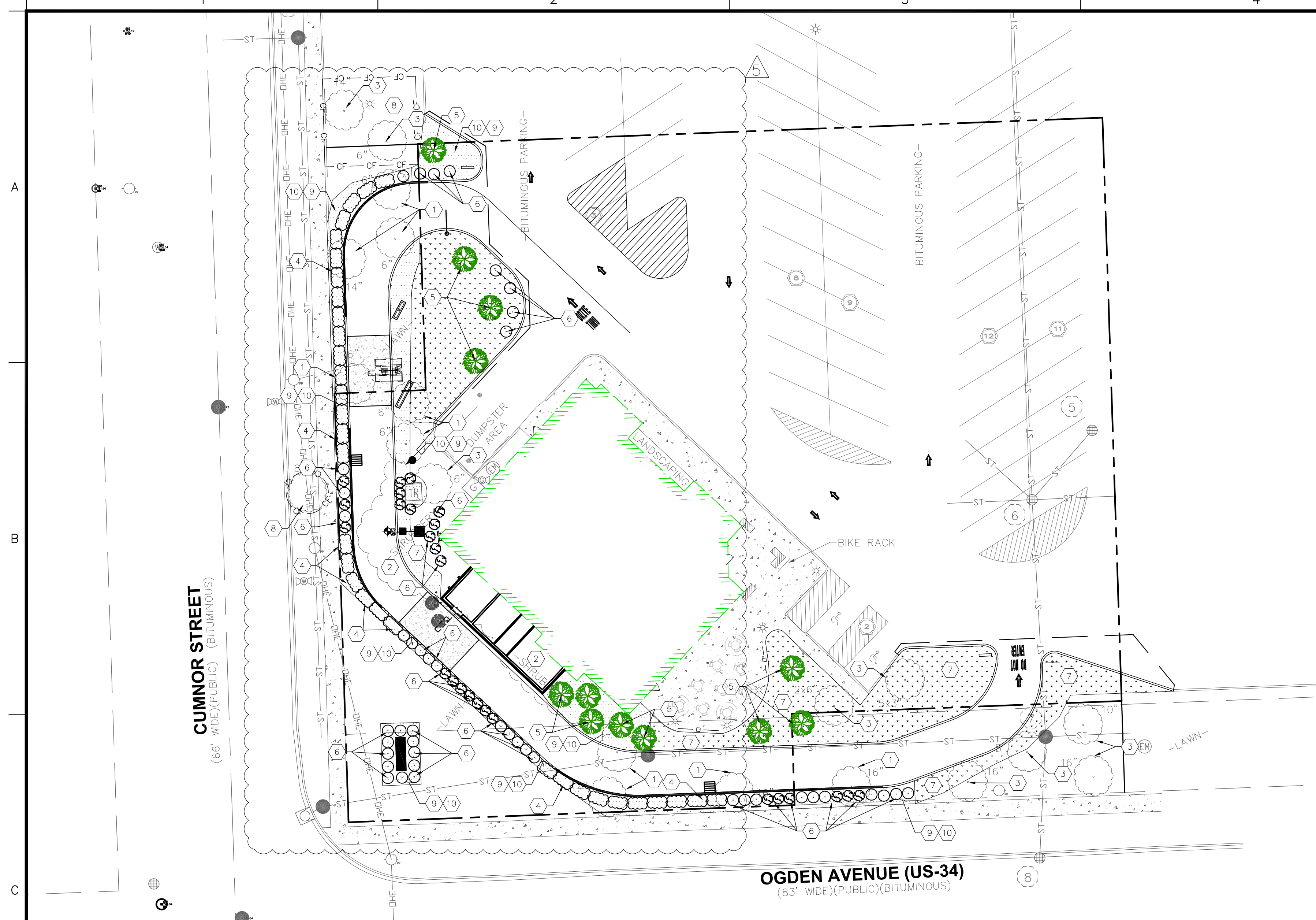
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5 OF 6



LANDSCAPE NOTES

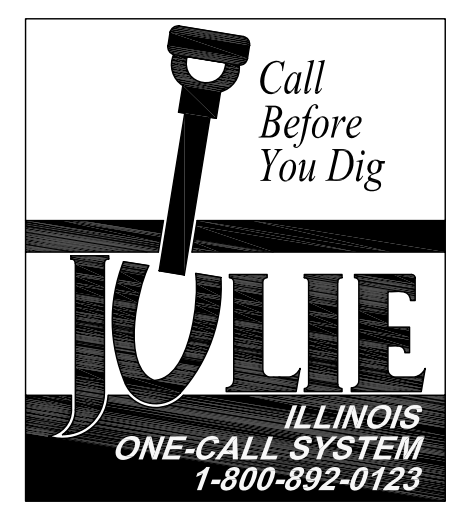
1. EXISTING LANDSCAPING SHOWN IS BASED ON AVAILABLE INFORMATION. CONTRACTOR TO VERIFY EXISTING LANDSCAPING.
2. IF EXISTING LANDSCAPING SHOWN TO BE RELOCATED CANNOT BE PRESERVED, CONTRACTOR SHALL PROVIDE NEW MATERIALS IN LOCATIONS SHOWN.

KEYED NOTES

1. EXISTING TREE TO BE REMOVED.
2. EXISTING LANDSCAPING TO BE REMOVED, PRESERVED AND TRANSPLANTED TO NEW LOCATION.
3. EXISTING TREE TO REMAIN.
4. NEW HEDGE ROW.
5. NEW TREE.
6. NEW SHRUB.
7. NEW SODDED AREA.
8. PARKWAY TREE PROTECTION AREA.
9. NEW MULCH IN LANDSCAPE AREA(S). MATCH EXISTING ONSITE MULCH TYPE/COLOR.
10. NEW WEED FABRIC IN NEW LANDSCAPE AREA.

LEGEND					
GROUNDCOVER					
SYMBOL	DESCRIPTION				
	SODDED AREA				
	MULCHED AREA				
SHRUBS					
SYMBOL	QUANTITY	COMMON NAME (botanical name)	ROOT	SIZE	REMARKS
	26	DWARF RED BARBERRY (<i>barberis thunbergii</i> var. <i>atropurea</i>)	CONT.	5 GAL.	18"-24"
	43	EMERALD MOUND HONEYSUCKLE (<i>lonicera xylosteum</i> 'emerald mound')	CONT.	5 GAL.	18"-24"
	31	WINTERGREEN BOXWOOD (<i>buxus microphylla</i> var. <i>koreana</i>)	CONT.	5 GAL.	18"-24" FOR HEDGEROW
TREES					
SYMBOL	QUANTITY	COMMON NAME	ROOT	SIZE	REMARKS
	12	FAIRVIEW JUNIPER (<i>juniperis chinensis</i> 'fairview')	B&B	2.5" CAL.	5' - 7'
		EXISTING TREE			

CONSTRUCTION ENTRANCE NOTE:
 GENERAL CONTRACTOR SHALL ONLY USE THE PARKING LOT AS A CONSTRUCTION ENTRANCE. NO OTHER ACCESS ACROSS THE VILLAGE RIGHT-OF-WAY WILL BE ALLOWED. GC SHALL CONSULT WITH CONSTRUCTION MANAGER AND ENGINEER AS APPROPRIATE BEFORE PROCEEDING WITH WORK.



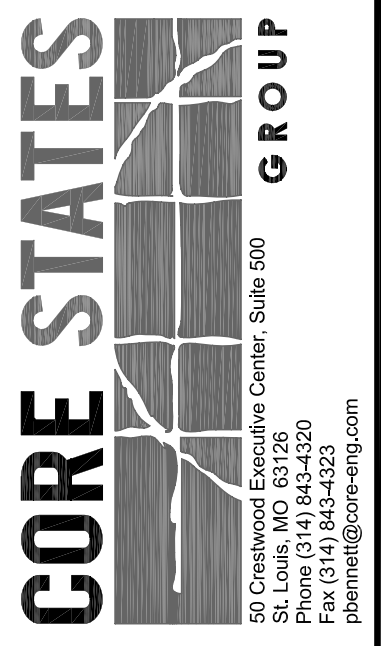
THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON DESIGN DRAWINGS, RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. CORE STATES, INC. DOES NOT GUARANTEE THAT LOCATIONS SHOWN ARE EXACT. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF UTILITIES.

Rev. #	Date	Description
01	06/01/11	ISSUANCE
02	07/05/2011	SANITARY SEWER DISTRICT COMMENTS
03	07/26/2011	LANDSCAPING AND TALL CURBS
04	08/19/2011	CITY COMMENTS/ OUT TO BID
05	10/24/11	NEED DRIVE THRU ADDITION

Panera, LLC
 3630 South Geyer Rd., Suite 100 St. Louis, MO 63127

Cafe-Thru Addition
 160 Ogden Ave., Downer's Grove, IL 60515

Landscaping Plan



Job#: ARC-12170
 Scale: 1"=20'
 Date: 05-04-11
 Drawn By: MAB
 Checked By: PTB

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GENERAL NOTES:

- A. ALL ITEMS TAGGED WITH "RELO" WILL BE RELOCATED FROM DEMOLITION.
- B. REFER TO ELECTRICAL DRAWINGS FOR ELECTRICAL INFORMATION.
- C. REFER TO PLUMBING DRAWINGS FOR PLUMBING INFORMATION.

BAKERY CAFE:
#734

SYSTEM: G2 (FUN)
REVISION: 10/28/11

DESIGN CONSULTANT
ARCISION
INCORPORATED
1000 W. 10TH ST. SUITE 200
DALLAS, TEXAS 75208
TEL: 214-353-1200 FAX: 214-353-1202

Panera
PANERA BAKERY
CAFE # 734
180 OGDEN AVE.
DOWNERS GROVE, IL



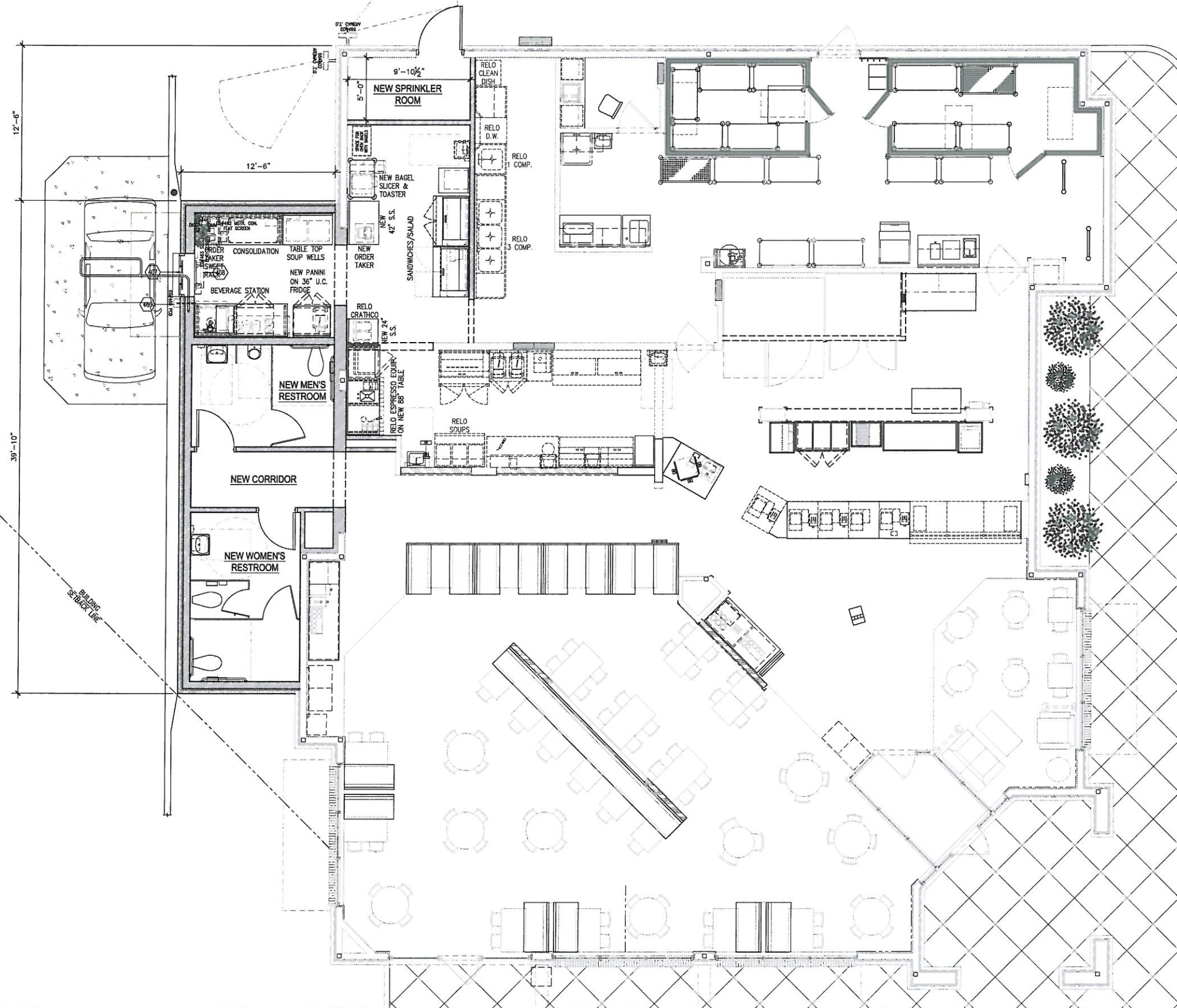
OPS PLAN

ISSUED: 10/28/11

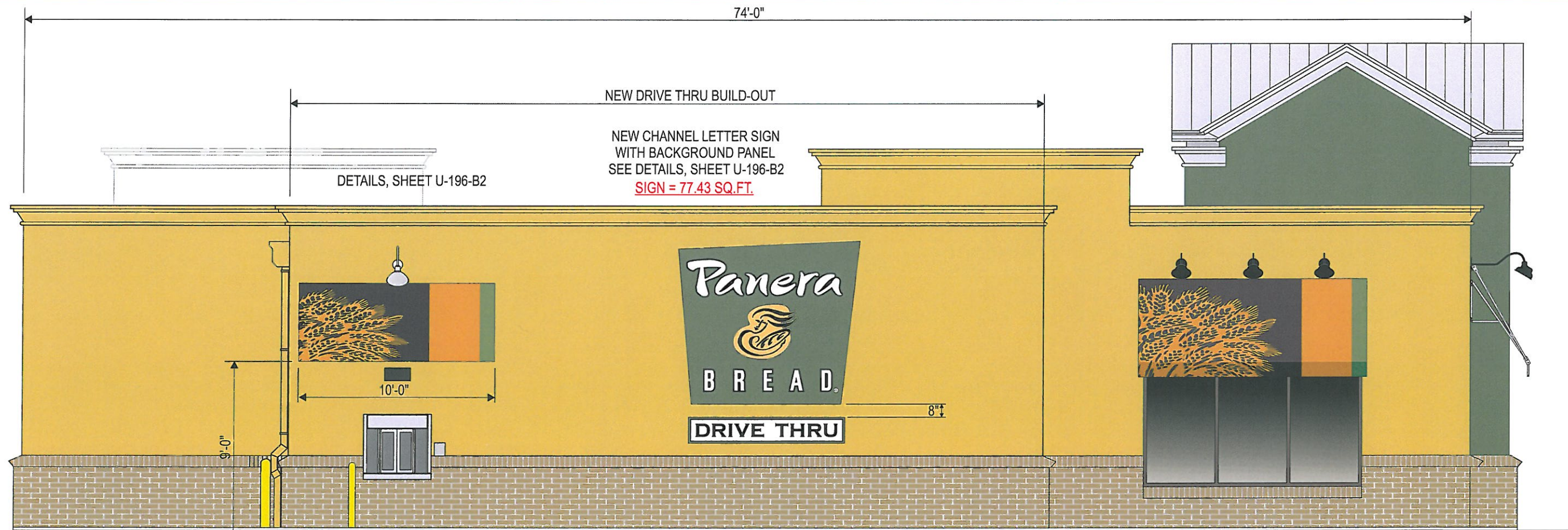
REVISION

ISSUE DATE: 10/28/11
PROJECT NUMBER: 100923
DRAWN BY: BTN
CHK: MS
DPM: MS
CPM: KE, JE

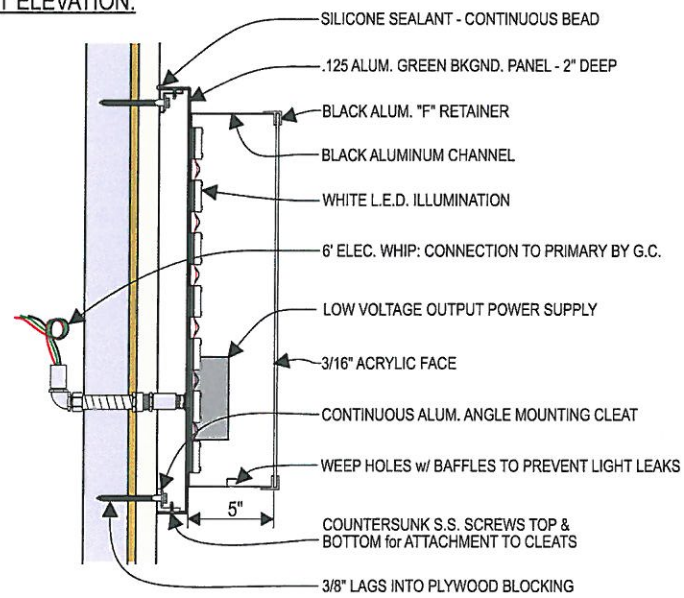
SHEET NO:
OPS-1
EQUIPMENT PLAN
FILE: 734-OPS-1.dwg



24 **PROPOSED OPS PLAN**
SCALE: 1/4"=1'-0"



SOUTHWEST STOREFRONT ELEVATION:
3/16" = 1'-0"



DRIVE THRU BOX SIGN SECTION: N.T.S.



D/T WALL SIGN ELEVATION: 3/8" = 1'-0"

ILLUMINATED "DRIVE THRU" SIGN CABINET:

- ALUMINUM CHANNEL LETTER TYPE CONSTRUCTION
- .040 ALUM. RETURNS, 5" DEEP, PAINTED BLACK
- 3/16" THICK WHITE ACRYLIC FACE
- 230-22 MATTE BLACK VINYL COPY
- 1" x 5/8" ALUM. "F" RETAINER PAINTED BLACK
- 2" DEEP ALUM. PAN BKGND. PANEL PAINTED PMS 5757U
- WHITE L.E.D. ILLUMINATION
- WEEP HOLES REQUIRED
- LOW VOLTAGE POWER SUPPLY
- FLUSH WALL MOUNT
- U.L. LABEL

ARTWORK EXCLUSIVE PROPERTY OF

SINCE 1917



MANDEVILLE SIGN
making your mark.

676 GEORGE WASHINGTON HIGHWAY
LINCOLN, RI 02865-4255

PHONE 401-334-9100 FAX 401-334-7799

WEB www.mandevillesign.com

APPROVALS

Signatures Required Before Release to Production

Engineering	DATE
Sales	DATE
Production	DATE
Quality Control	DATE

REVISIONS

NO.	BY	DESCRIPTION	DATE
1	JM	Updated building color to BM #2154-40 York Harbor Yellow	08/03/10
2	GL	rev. sign size, add drive thru wall sign. Add building frontage dim. Add DRIVE THRU build-out.	01/25/11
3	GL	rev. buildout size and location. revise wall sign and dt sign specs.	03/02/11
4	WD	revert back to previous version	05/11/11
5	WD	Updated D/T addition and signs, remove awning tie-backs	06/20/11
6			
7			

CLIENT



STORE # 734

160 Ogden Ave.
DOWNER'S GROVE, IL

LOCATION

21401-1

WORK ORDER NUMBER(S)

PROJECT MANAGER JIM MANDEVILLE

NOTED	JM	07/15/10
SCALE	ARTIST	DATE

DRAWING NAME U-196-B1