

**VILLAGE OF DOWNERS GROVE  
PLAN COMMISSION**

VILLAGE HALL COUNCIL CHAMBERS  
801 BURLINGTON AVENUE

August 1, 2016  
7:00 p.m.

**AGENDA**

**1. Call to Order**

**a. Pledge of Allegiance**

**2. Roll Call**

**3. Approval of Minutes – July 11, 2016**

**4. Public Hearings**

- a. 16-PLC-0023 (continued from June 6 and June 27, 2016):** A petition seeking approval of a Special Use to allow an office use to provide more than 4.5 parking spaces per 1,000 square feet of floor area and a Rezoning from M-1, Light Manufacturing to O-R-M, Office-Research-Manufacturing. The property is located on the northwest corner of Warrenville and Finley Road, commonly known as 2200 Warrenville Road (PINs 08-01-400-004, and -006). Adam Stokes, Agent of Nicolson Porter & List, Inc. and Arbor Vista LLC, Petitioners; Arbor Vista LLC, Owner.
- b. 15-PLC-0008 (continued from July 11, 2016):** A petition seeking approval of a Planned Unit Development, a Rezoning from B-3, General Services and Highway Business to B-3/PUD, Special Use and Right-of-Way Vacation of the Alley to redevelop a fueling station and car-wash. The property is currently zoned B-3, General Services and Highway Business. The property is located on Ogden Avenue between Seeley and Oakwood Avenue, commonly known as 1401 - 1445 Ogden Avenue, Downers Grove, IL (PINs 09-06-405-001, -007, -008, -009, -010, -026). Michael Green, Petitioner and Delta Sonic Car Wash Systems, Inc., Owner.
- c. 16-PLC-0032:** A petition seeking approval of a Special Use to allow an automobile dealership. The property is currently zoned B-3, General Services and Highway Business. The property is located on the north side of Ogden Avenue, between Lee Avenue and Downers Drive, commonly

known as 1620 Ogden Avenue, Downers Grove, IL (PIN 09-06-302-006).  
Local Community Signs, LLC, Owner.

- d. **16-PLC-0033:** A petition seeking to amend the existing Special Use Ordinance to allow all uses expressly permitted in the M-1 Zoning District. The property is currently zoned M-1, Light Manufacturing. The property is located on Wisconsin Avenue, 480 feet west of Belmont Road, commonly known as 2300 Wisconsin Avenue, Downers Grove, IL (PIN 08-12-407-006). MacNeil Real Estate Holdings, LLC, Owner.

## **5. Adjournment**

**THIS TENTATIVE REGULAR AGENDA MAY BE SUBJECT TO CHANGE**

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VILLAGE OF DOWNERS GROVE  
PLAN COMMISSION MEETING  
PUBLIC HEARING

JULY 11, 2016, 7:00 P.M.

Chairman Rickard called the July 11, 2016 meeting of the Downers Grove Plan Commission to order at 7:00 p.m. and led the Plan Commissioners and public in the recital of the Pledge of Allegiance.

**ROLL CALL:**

**PRESENT:** Chairman Rickard, Mr. Cozzo, Mr. Cronin, Ms. Gassen, Ms. Hogstrom, Mr. Quirk, Mr. Thoman

**ABSENT:** Ms. Johnson, Mrs. Rabatah, ex-officios Mr. Livorsi, Ms. Lupesco, Mr. Menninga

**STAFF:** Community Development Director Stan Popovich, Planner Rebecca Leitschuh,

**VISITORS:** Kathleen West with Delta Sonic, 111 E. Jefferson Ave., Downers Grove; Jeff Leitz and Jason Dutkovich with CVG Architects, 1245 E. Deihl Rd., Naperville; Bob Long, 1864 Grant St., Downers Grove; Rich Kulovany, 6825 Camden Rd., Downers Grove; Scott Richards, 1130 Warren Ave., Downers Grove; Saralee Weaver, 4032 Joslynn Rd., Downers Grove; R.C. Good, 741 Farley, Downers Grove; Skip and Roberta Muehlhaus, 1868 Grant St.; Downers Grove; Mr. Robert Harunger, 4123 Northcott, Downers Grove; Brian Nessbaum with Sus, Arnold & Schoenback; Mr. Kent Conness, 1846 Grant St., Downers Grove; Ms. Cathy Fritts 4417 Stonewall, Downers Grove, Mr. John Kahovec, 406 Lincoln Ave.,

**APPROVAL OF MINUTES**

**JUNE 27, 2016 MINUTES – MOTION BY MR. COZZO, SECONDED BY MR. THOMAN, TO APPROVE THE MINUTES, AS REVISED.** A change was noted on the bottom of Page 4, last five words of sentence, the chairman recalled that he was talking about a stepping down effect “when traveling down Main Street” when the verbiage should have been “when traveling perpendicular to Main Street” where he noted the short buildings are at the street and increase the height as one goes back from the street, not down the length of the street.

**MOTION CARRIED BY VOICE VOTE OF 6-0-1 (MS. HOGSTROM ABSTAINS.)**

**PUBLIC HEARINGS:**

Chairman Rickard explained the protocol for the public hearings and swore in those individuals that would be speaking on the petitions below.

**FILE 15-PLC-0008** – Petition for a Planned Unit Development, a Rezoning from B-3, General Services and Highway Business to B-3/PUD, Special Use and Right-of-Way Vacation of the Alley

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to redevelop a fueling station and car-wash. The property is currently zoned B-3, General Services and Highway Business. . The property is located on Ogden Avenue between Seeley and Oakwood Avenue, commonly known as 1401 - 1445 Ogden Avenue, Downers Grove, IL (PINs 09-06-405-001, -007, -008, -009, -010, -026). Michael Green, Petitioner and Delta Sonic Car Wash Systems, Inc., Owner.

Chairman Rickard announced that the petitioner, via a memorandum to staff (dated 7/11/2016), has requested to continue this public hearing to the August 1, 2016 Plan Commission meeting.

**WITH RESPECT TO FILE 16-PLC-0008, MR. THOMAN MADE A MOTION THAT THE PLAN COMMISSION ACCEPT STAFF'S RECOMMENDATION TO GRANT THE PETITIONER'S REQUEST TO CONTINUE THE PUBLIC HEARING TO THE AUGUST 1, 2016 PLAN COMMISSION MEETING.**

**SECONDED BY MS. GASSEN.**

**MOTION CARRIED UNANIMOUSLY BY VOICE VOTE OF 7-0.**

**FILE 16-PLC-0029** - A petition seeking approval for the Right-of-Way Vacation of an Alley. The adjacent properties are zoned R-4, Residential Detached House 4. The subject property is located on the north side of Franklin Street and runs north-south between the properties at 708 & 712 Franklin Street, 170 feet west of Stanley Avenue and 170 feet east of Prospect Avenue, Downers Grove, IL (09-08-204-022, -023). George Arnold, Petitioner and K-7 Builders, Inc., Owner.

Village Planner, Rebecca Leitschuh, reviewed the petition, pointing out on the overhead that all of the surrounding properties were zoned R-4 Residential Detached. She pointed out the right-of-way area that was being requested to be vacated – between Stanley Avenue and Prospect Avenue. The petitioner is seeking to split the alley 50/50 between the two neighboring properties. The total square footage being vacated is 2,062.5 feet, with 8.25 feet going to each property. The two properties currently have 50 feet of lot frontage and the frontage would now increase for both properties by 8.25 feet. Total square footage would increase from 6,250 sq. feet to 7,281.25 sq. feet.

A plat of survey was placed on the overhead depicting the existing driveway that travels through the public alley which is accessed by the owners of the 708 address. Per staff, the utility companies had no objections to the request; however, staff was requesting an easement across the subject area, which was standard procedure.

Ms. Leitschuh then displayed the plat of vacation request and proceeded to summarize the goals of the village's Comprehensive Plan, stating the proposal was consistent with the Comprehensive Plan and allowed flexibility for the property owners. The proposed right-of-way vacation also met the four criteria under Resolution No. 2003-58, which criteria were reviewed in more detail by Ms. Leitschuh. Referring to staff's three recommendations, Ms. Leitschuh stated staff supported the petition.

Questions to staff included whether this process was a new standard for the village as compared to going from street to street for vacations; whether a cross-access agreement would need to be entered

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into; and would a new survey be created reflecting two new lots or could owner A sell his half to owner B (staff confirmed yes). Responding to Mr. Quirk's comment that with previous vacation requests, staff usually provided a plat of survey reflecting the new lot of record. Ms. Leitschuh added that there were no new plans for development on either of the properties. Further clarification followed by staff as to what could be constructed across the alleyway according to the easement language.

Continuing, staff explained that the power line on the eastern lot line of the alley would remain unless the utility company had some reason to relocate it or if the owner wanted to pay for its relocation. Asked who was responsible for the "apron" on the parkway, Dir. Popovich stated that the redeveloper of the 708 address would be financially responsible for removing the "apron" out of the parkway and the verbiage would be inserted into the permit being requested. Setback requirements to power lines would fall under the purview of the utility company and would depend on the type of utility line being relocated.

Questions followed regarding the value that was ascertained to the property and how it was calculated which Ms. Leitschuh explained to Mr. Quirk. Mr. Quirk recommended reviewing the calculation of the value of land as it pertained to which zoning district it sat in, seeing there were probably different values per acre for the different zoning districts within the village. He shared a couple of examples, but Ms. Leitschuh pointed out that the value calculation was under the direction of the village council as to how it should be calculated.

Mr. Brian Nessbaum with Sus, Arnold & Schoenback, on behalf of petitioner Mr. George Arnold and the owners of 708 and 712 was present. Mr. Nessbaum stated he read staff's report, was in contact with staff, and had no issues with the report. His client did intend to raze the existing home at 708 for future development.

Chairman Rickard opened up the meeting to public comment. No comments followed. Public comment was closed and Mr. Nessbaum had no closing statement.

Mr. Quirk believed the petition was appropriate and was pleased the village was taking the approach to allow property owners to own what is rightfully theirs. Mr. Cozzo also believed the four criteria under Resolution 2003-58 were satisfied; the chairman concurred.

**WITH RESPECT TO FILE 16-PLC-0029, MR. THOMAN MADE A MOTION THAT THE PLAN COMMISSION FORWARD A POSITIVE RECOMMENDATION TO THE VILLAGE COUNCIL, TAKING INTO CONSIDERATION STAFF'S THREE (3) FOLLOWING CONDITIONS:**

- 1. THE VACATION SHALL SUBSTANTIALLY CONFORM TO THE STAFF REPORT DATED JULY 11, 2016;**
- 2. PRIOR TO FINAL VILLAGE COUNCIL CONSIDERATION, A MYLAR COPY OF THE FINAL PLAT OF VACATION SHALL BE PROVIDED INDICATING A PUBLIC DRAINAGE, UTILITY AND UTILITY ACCESS EASEMENT ALONG THE ENTIRE LENGTH AND WIDTH OF THE ALLEY TO BE VACATED; AND**

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- 3. PRIOR TO EXECUTION OF THE PLAT, THE PETITIONER AND PROPERTY OWNER OF 712 FRANKLIN STREET SHALL PAY THE VILLAGE A TOTAL OF \$8,601.92 (\$4,300.96 EACH).**

**SECONDED BY MR. QUIRK. ROLL CALL:**

**AYE: MR. THOMAN, MR. QUIRK, MR. COZZO, MR. CRONIN, MS. GASSEN,  
MS. HOGSTROM, CHAIRMAN RICKARD**

**NAY: NONE**

**MOTION CARRIED. VOTE: 7-0**

**FILE 16-PLC-0009** – Petition for a Planned Unit Development, a Rezoning from B-3, General Services and Highway Business to B-3/PUD, General Services and Highway Business/PUD and a Special Use to construct an automobile dealership. The property is currently zoned B-3, General Services and Highway Business. The property is located on Ogden Avenue at the T-intersection of Lacey Road and Ogden Avenue, commonly known as 1815 Ogden Avenue, Downers Grove, IL (PINs 09-06-304-013 & -014). Brad Webb, Petitioner and ALDI Inc., Owner.

Community Development Director Stan Popovich reviewed the petitioner's request and located the property on the overhead. Elevations of the proposed dealership building were also depicted. The building will sit in the center of the site. Functions within the building were pointed out and a second floor would house offices for the dealership. Two accesses into the site were pointed out – one at the west end, as a right in/right out onto Ogden Avenue, and one at the east end, as a full access. Packey Webb Ford will provide a cross-access point to access the Star Motors property. Parking was highlighted on the site plan. Per staff, the petitioner was not planning to construct a stand-alone car wash building at this time, but did intend to pursue approval of one in case it wanted to construct a car wash in the future. If constructed, the car wash would sit west of the main building. Currently there was a car wash bay within the main building.

Truck turning exhibits were reviewed by Director Popovich, noting car carriers could enter the site from the west and then exit on the east.

Staff pointed out that the existing wetland would be impacted based on the environmental remediation plan with the Illinois Environmental Protection Agency (IEPA). The wetlands would be remediated via a fee-in-lieu to a remediation bank. Water flow and drainage for the site was reviewed. Director Popovich stated that the petitioner plans to construct three (3) basins: one as an open air basin, one as a detention basin located east of the building, and one smaller one located at the northeast corner. All basins were designed to meet the village's stormwater ordinance. Per staff, the stormwater engineering and public works staff did review the plans and both departments indicated the proposal would meet the stormwater floodplain ordinance.

Next, a review of the landscape plan followed. No trees would be located in the detention basin area. Screening for the south property line would not take place because it would interfere with the water flow capacity needed to get through the swale, as cited by staff and the engineers. A solid fence would be inserted in its place.

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Per staff, a summary of the petitioner's neighborhood meetings were referenced in the commissioners' packets. And, after the neighborhood meeting, the developer reduced the size of the proposed building based on their requirements. Per staff, the proposal did meet the village's comprehensive plan, the criteria under the zoning ordinance, and all of the standards for approval under the Planned Unit Development, special use standards, and rezoning standards. The project was a desirable development for the community. Staff recommended the commission forward a positive recommendation with the conditions listed in its staff's report.

Commissioner questions raised included whether there was a signalized light proposed at Lacey Avenue (no) ; whether staff agreed with the traffic counts (staff concurred); and where would the water flow if it exceeded the 100 year flood event (overflow north onto Ogden Avenue, picked up by the LPDA in the southeast corner , then flow over the parking lot). Asked if the petitioner considered pavers for the parking lot, staff stated the petitioner decided to install the required underground water storage to accommodate the additional pavement on-site. Ms. Hogstrom asked staff to explain where the off-site wetland mitigation took place, wherein Director Popovich explained that mitigation would happen at the permit stage but he was not sure where the mitigation would occur.

Regarding the request for increased signage, Ms. Hogstrom asked if there were other nearby developments that had similar requests, wherein Director Popovich stated that X-Sport on Finley Road and the Art Van Furniture Store were approved developments with similar signage requests. Staff then confirmed that the lighting photometrics plan met the village's requirements and would further meet the village's lighting requirements, at the property lines, for non-residential businesses located next to residential areas. Per another question about audible "paging", Director Popovich understood paging would be done via telephone and no outdoor paging system would exist.

Lastly, Director Popovich explained in detail the three-year wetland monitoring maintenance program that was required by the petitioner and which was in accordance with the village's stormwater ordinance.

Applicant, Mr. Jeff Leitz, with CVG Architects, 1245 E. Diehl Rd., Naperville, stated he represented the owner, Brad Webb. He introduced the development team: wetlands consultant, Tom Mangan, with Geothink; construction manager, Scott Ledbetter, with International Contractors; and civil engineer, Jeff Nance, with R.A. Smith.

Mr. Leitz summarized the property was vacant for the past 30 years, it was contaminated with wetlands present, and reasons existed as to why the property was not developed. It was "not a simple site." A history of the project followed with Mr. Leitz noting that the team was trying to work with staff and a number of agencies on the best approach to make the project a success. Details followed.

Regarding the site plan, Mr. Leitz confirmed there were 815 parking spots on-site, pointing out that the car dealership would act as a display for the product being sold, which was why the lot was landscaped over 20%. As to the neighborhood meeting that took place, Mr. Leitz stated he was considerate of the neighbors, but also stated that more lighting would be installed on the property than what currently exists – and the petitioner was meeting the village's standards. He elaborated as to what would be installed along the various property lines: full vision landscape screening on the

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south property line with board on board fence; evergreens planted on the west property line; and a full vision fence along the western property line to block lighting from the residents.

Mr. Leitz agreed that loud noise on the site was a concern, and, as voiced at the neighborhood meeting but that communications on-site would continue via cell phones or two-way radios. Speakers would be attached on the exterior of the building for those few instances where someone had to be contacted. As to the future car wash and its location, Mr. Leitz, stated the southern line of the car wash was 130 feet from the property line. The internal equipment for the car wash was not purchased at this time, but Mr. Leitz stated he was aware of the village's noise ordinance and would not create a "disturbance to the neighbors".

Responding to the concerns voiced at the neighborhood meeting Mr. Leitz explained that a sanitary easement will be placed along the east and west property lines to accommodate any future sanitary needs. As to using a paver-block system instead of asphalt, pavers could not be used due to the site's contamination. Elevations of the building were depicted on the overhead with Mr. Leitz addressing the two-sided silver "brand wall" which element was similar to the Packay Ford. The reason for its increased size was to keep it proportionate to the building and to have the sign visible from the east- and west-bound traffic. No monument or pylon signs were being requested by the petitioner. Details of the building's material followed with Mr. Leitz explaining the building would be a "lantern" at night so that customers could see the building and purchase more vehicles. Delineation of the parking spaces were noted.

Hours of operation were as follows (including the future car wash for customers): Monday through Friday, 7:00 a.m. to 9:00 p.m.; Saturday - open until 6:00 p.m.; and closed on Sunday. Mr. Leitz pointed out the location for on-site delivery of new vehicles via a car-carrier. Addressing a question about the parking study and why the structure was decreased in size, Mr. Leitz indicated it had to do with economics and nothing to do with the number of vehicles to sell or the customers to draw.

Further questions followed as to what happened with the excavated soil on the site (mined per IEPA requirements); the status of the reported documentation to the IEPA; and whether the landscaper could review the list of native plantings again. Signage details were also reviewed.

Chairman Rickard opened up the meeting to public comment.

Mr. Scott Richards, 1130 Warren Ave., Downers Grove, was disappointed that another car dealership was being proposed for the large parcel and believed it was a waste of property. He voiced concern that a signalized light was not being installed at Ogden and Lacey Avenue for safety purposes and due to the proposed senior housing that was to be constructed.

Mr. Kent Conness, 1846 Grant St., Downers Grove, voiced concern that at the March 9, 2016 neighborhood meeting there comments about the project using TIF funds, which he did not believe this site needed. Also at the same meeting there was reference made to a 10-year agreement for a sales tax rebate from the village. Mr. Popovich stated that specific aspect would be addressed at the village council level should this petition move forward.

Continuing, Mr. Conness stated the sales tax rebate should be available to all businesses and not just certain ones. His other concerns included light pollution, light reflection, no landscape screening or fencing at the southeast corner of the site and outside speakers. The current site was a quiet, green

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10-acre oasis on Ogden Avenue and would now become noisy. Environmental contaminants were on the property.

Ms. Cathy Fritts, 4417 Stonewall, Downers Grove, was surprised that she and her husband were not “invited” to the March 9<sup>th</sup> neighborhood meeting since the rear of their lot backed up to the proposed site. She believed all owners surrounding the property should have been included. She agreed that traffic on Ogden Avenue was an issue. She asked for the height of the fence that was going to be installed (6 feet) and where test driving was going to take place.

Mr. John Kahovec, 406 Lincoln Ave., Downers Grove, attended the March 9 neighborhood meeting and did see some changes in the plan from that meeting. Referring to Sheet No. C-11 of the plans depicting the wetlands, he believed the petitioner was going to push the wetlands further south into the residential properties, devalue the property, not landscape as originally discussed at the neighborhood meeting, and was creating more issues by moving the natural flow of water in the area. Because TIFs and sales tax rebates were being used, he believed the residents should have more say in the development around the residential areas and figure out a way to preserve more of the natural wetlands. He did not believe that just because a developer says he uses Best Management Practices that he does it. He asked the petitioner to identify where the contaminated soil would be buried. He also requested that the residents be protected from the lighting and noise from the future car wash.

Mr. Robert Harunger, 4123 Northcott, Downers Grove, resides north of the project and agreed with many of the prior statements made. He would have preferred a mixed use development on the parcel. Since the petitioner was a long-time business owner, Mr. Harunger stated he would not be in favor of using TIF funds or tax incentives for the parcel and for the intended purpose. He believed a traffic signal at Lacey would solve the traffic issues on Ogden Avenue, provide access to the dealership and to the neighborhood to the north where a senior residence was currently being planned. It would also provide an additional crosswalk for pedestrians.

Mr. Skip Muehlhaus, 1868 Grant St., Downers Grove believed it would be more appropriate to place a signal at Lee Street versus Lacey. He recommended removing the contaminated soil off-site versus keeping it on-site only because then a paver system could be considered versus asphalt, similar to Star Motors, which would assist with the water problem.

Mr. Robert Harunger returned, stating that pavers would be a positive over asphalt since it was aesthetically pleasing and accomplished a drainage issue. However, leaving the contaminated soil on-site was feasible versus running into EPA issues when it is relocated off-site.

After hearing no further comments, Mr. Leitz returned to the podium to respond to some of the questions raised. Discussing the lighting trespass in the southeast corner of the site, he explained that because of the way the stormwater was designed, installing any landscaping or fencing would deter the drainage from the property. As for outside speakers, if there was a case where someone had to be contacted, there was no choice. Test driving would be taken out of the neighborhoods but he could not guarantee that. (Mr. Cozzo recommended that the dealership inform its sales reps to keep vehicles out of the neighborhoods.) Mr. Leitz also apologized to the resident who did not receive an invite to the neighborhood meeting and offered to sit down with her to review the plans if she preferred.

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Mr. Tom Mangan from Geothink, 611 Stevens St., Geneva, was present to answer questions regarding the environmental issues of the project. He explained in detail the flow of water from the current wetland (and its contaminants) along the southern part of the site into the larger wetland located at the southeast corner of the site. Details followed on how the mitigation would take place, how the surface contamination would be removed out of the wetland area and across the majority of the site, as well as the steps taken to alleviate some of the flooding issues in the neighborhood. Contaminated soils would be relocated (and separated) to the southeast corner of the site. Details of the property's grading also followed, with Mr. Mangan noting that when the process takes place, there will be a health and safety plan on-site, one with the IEPA, and one with the village due to possible mercury and PNA exposure.

Mr. Quirk asked what the cost difference was for burying the contaminated soil on-site versus hauling it off-site, wherein Mr. Mangan explained it would cost anywhere from \$2.5M to \$3.0M to haul the soil off-site and by keeping it on-site the cost was half, he estimated. Proper engineering and safeguards would take place on the site.

As for the traffic signal, Mr. Leitz summarized that the traffic study was for a future stop light but that it was not in the works for Packey Webb nor the village at this time. However, if the topic was to be discussed again, he said Packey Webb was willing to discuss it. As for the white color on the building, Mr. Leitz explained white was one of the colors required by Ford and, yes, there would be some reflection. Other than the security lights, the lot lights would be turned off at 9:00 P.M.

Mr. Leitz and the chairman proceeded to discuss whether the proposed parcel could be seen standing from the south property line when the trees were in full bloom, wherein Mr. Leitz stated the view was screened by the tree line.

However, Mr. Jared Fritts, 4417 Stonewall Ave., came forward and stated he resides at the southwest corner of the proposed lot and he could see the lot. He stated he could see the lights from Star Motors and would see the lights from the proposed dealership, especially off a white building.

Given the above statement, Mr. Leitz believed that since Mr. Fritts could see the lighting through the trees, he did not believe adding trees was going to screen the lot anyway. Conversation followed as to why the wetland, north of the tree line, was being located south towards the residents. Mr. Mangan responded that the trees were contaminated along the southern property line.

Mr. Fritts inquired as to why Aldi, the current property owner, was not being included in these discussions, since contaminated soils were being moved around on the property.

Mr. Leitz closed by summarizing that the petitioner has, over the past eight months, gone through many designs and engineering and was making the site better environmentally for the dealership. The petitioner was excited to become part of the community and be a good neighbor.

Per Ms. Gassen's question about lighting shields being used, Mr. Leitz said some shields would be used on the lights to cut down on glare, along with aimed LED lighting. He confirmed there was going to be "more light on this site" but that it would be minimized at all property lines.

Chairman Rickard closed the public hearing.

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Asked if within the village there was a similar-sized “vessel” to house the stormwater, Director Popovich could not answer affirmatively, given that the parcel was one of the largest parcels to come in for redevelopment since the new ordinance was in effect. However, he explained that the stormwater plans were sent to an outside engineering consultant who provided comments to staff, and staff was working with the petitioner. Both, in-house village engineers and the outside consultants confirmed the proposed stormwater system met the stormwater ordinance requirements.

Asked what the village’s current plans were regarding a proposed traffic light, Director Popovich explained to the chairman that past studies had shown that the traffic light would be better located somewhere other than the Lacey intersection. Asked if the village was aware that contaminants were leaving the property and traveling to the lower wetland, Director Popovich shared that he did not review the IEPA reports and could not say one way or the other that the village was aware of what was going on, on-site. He agreed it was beneficial to clean up the parcel, however.

The chairman pointed out there were three parts to the approval for this petition and proceeded to read the associated standards. Discussion followed on those standards that were met or not met. No changes were voiced by the commissioners. Next, commissioners discussed the standards under the request for a zoning amendment from B-3 to B-3 PUD. Commissioners agreed all criteria was met. Lastly, the third portion, as it related to the request for a special use for a car dealership, commissioners agreed all three standards were met.

Last comments from various commissioners included the following: 1) that the village consider implementing a traffic light in a location that does have traffic issues; 2) that the sign relief was warranted; 3) that the greenspace was a warranted request; and 4) that the future car wash be considered. Mr. Cozzo believed there was a thorough study of the stormwater management and wetlands and applauded the petitioner for the amount of time and effort spent to mitigate the site. However, he was disappointed that no stop light was planned for the Lacey and Ogden intersection, given there was a senior housing facility being planned directly across the street. Ms. Hogstrom concurred. She also thought there was a village restriction for test driving through neighborhoods. Discussion followed on how test drives would be restricted/enforced as well as a discussion that the new development provided an opportunity to stop the run-off of contaminants onto private property.

**WITH RESPECT TO FILE 16-PLC-0009, MR. COZZO MADE A MOTION THAT THE PLAN COMMISSION RECOMMEND THE VILLAGE COUNCIL TO APPROVE THE REQUESTED PLANNED UNIT DEVELOPMENT, REZONING AND SPECIAL USE AS REQUESTED, SUBJECT TO THE FOLLOWING CONDITIONS:**

- 1. THE PLANNED UNIT DEVELOPMENT, REZONING AND SPECIAL USE SHALL SUBSTANTIALLY CONFORM TO THE STAFF REPORT; ARCHITECTURAL AND PHOTOMETRIC DRAWINGS PREPARED BY CVG ARCHITECTS DATED JANUARY 29, 2016 AND LAST REVISED ON JUNE 28, 2016 AND ENGINEERING AND LANDSCAPE DRAWINGS PREPARED BY R.A. SMITH NATIONAL DATED JUNE 10, 2016, EXCEPT AS SUCH PLANS MAY BE MODIFIED TO CONFORM TO THE VILLAGE CODES AND ORDINANCES.**
- 2. THE BUILDING SHALL BE EQUIPPED WITH AN AUTOMATIC SUPPRESSION SYSTEM AND AN AUTOMATIC AND MANUAL FIRE ALARM SYSTEM.**

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3. **NO ADDITIONAL WALL OR MONUMENT SIGNS SHALL BE PERMITTED FOR THIS SITE THAT WOULD RESULT IN AN INCREASE IN OVERALL SIGN AREA.**
4. **THE APPLICANT SHALL ADMINISTRATIVELY CONSOLIDATE THE TWO LOTS INTO ONE LOT OF RECORD PRIOR TO ISSUING A BUILDING PERMIT.**
5. **THE APPLICANT SHALL PROVIDE A CROSS-ACCESS EASEMENT FROM THE EASTERNMOST OGDEN AVENUE CURB CUT TO THE CROSS-ACCESS DRIVE FOR THE PROPERTY TO THE EAST ON THE ADMINISTRATIVE LOT CONSOLIDATION.**

**SECONDED BY MR. CRONIN. ROLL CALL:**

**AYE: MR. COZZO, MR. CRONIN, MRS. GASSEN, MRS. HOGSTROM, MR. QUIRK,  
MR. THOMAN, CHAIRMAN RICKARD**

**NAY: NONE**

**MOTION CARRIED. VOTE: 7-0**

Director Popovich announced there will be four cases on the August 1, 2016 meeting agenda.

**THE MEETING WAS ADJOURNED AT 9:45 P.M. ON MOTION BY MR. QUIRK,  
SECONDED BY MR. CRONIN. MOTION CARRIED UNANIMOUSLY BY VOICE VOTE  
OF 7-0.**

/s/ Celeste K. Weilandt  
(As transcribed by MP-3 audio)



**VILLAGE OF DOWNERS GROVE  
REPORT FOR THE PLAN COMMISSION  
AUGUST 1, 2016 AGENDA**

<b>SUBJECT:</b>	<b>TYPE:</b>	<b>SUBMITTED BY:</b>
16-PLC-0023 2200 Warrenville Road	Zoning Map Amendment (Rezoning), Special Use to Exceed Maximum Parking Requirements	Scott Williams Planner

**REQUEST**

The petitioner is requesting approval of the following:

- Zoning Map Amendment (Rezoning) from M-1, Light Manufacturing to O-R-M, Office-Research-Manufacturing;
- Special Use to Exceed Maximum Parking Requirements

**NOTICE**

The application has been filed in conformance with applicable procedural and public notice requirements.

**GENERAL INFORMATION**

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**OWNER/APPLICANT:** Arbor Vista, LLC  
1300 West Higgins Road #104  
Park Ridge, IL 60068

**PROPERTY INFORMATION**

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**EXISTING ZONING:** M-1, Light Manufacturing  
**EXISTING LAND USE:** Vacant Light Industrial  
**PROPERTY SIZE:** 357,192 (8.2 acres)  
**PINS:** 08-01-400-004, -006

**SURROUNDING ZONING AND LAND USES**

	<b>ZONING</b>	<b>FUTURE LAND USE</b>
<b>NORTH:</b>	Illinois Tollway	N/A
<b>SOUTH:</b>	B-3, General Services and Highway Business	Corridor Commercial
<b>EAST:</b>	B-3, General Services and Highway Business	Corridor Commercial
<b>WEST:</b>	M-1, Light Manufacturing	Office/Corporate Campus

**ANALYSIS**

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**SUBMITTALS**

This report is based on the following documents, which are on file with the Department of Community Development:

1. Application/Petition for Public Hearing
2. Location Map
3. Project Summary/Narrative

4. Conceptual Site Plan/Elevations/Floor Plans
5. Site Photos
6. Plat of Survey
7. Traffic and Parking Study

#### **PROJECT DESCRIPTION**

The petitioner is requesting approval of a Special Use to exceed the maximum allowance of 4.5 parking spaces per 1,000 square feet of floor area and a rezoning from M-1 (Light Manufacturing) to O-R-M (Office-Research-Manufacturing) to facilitate the construction of a two-story Class-A office building. The subject property is located at the northwest corner of Warrenville Road and Finley Road and is commonly known as 2200 Warrenville Road.

The property is currently improved with two buildings across three lots formerly the Perkin Elmer Life Sciences buildings. The site is accessed via a curb-cut off of Warrenville Road at the southwest corner of the property.

The proposed tenant, although undisclosed at this point, is a Fortune 500 Company looking to construct a suburban headquarters to take advantage of the visibility and location of the site. The company will be consolidating multiple divisions from different locations under one roof based on concepts including “hoteling,” telecommuting, shared work spaces, and others.

The conceptual two story building with an open floor plan will be built in accordance with latest design efficiency trends in order to meet immediate and long-term employee staffing needs. According to the research provided by the petitioner, whereas 250 square feet per employee was the standard for office space before the recession, the current standard is now approximately 175 square feet per employee.

To provide enough parking for more employees working in shared or collaborative spaces, the parking ratio will be in excess of the maximum 4.5 spaces per 1,000 square feet of floor area the maximum allowed per section 7.020 of the Zoning Code. Although the site plan is conceptual at this stage with the potential for the building size to change, the applicant is projecting an increased ratio of 7.2 parking spaces per 1,000 square feet of floor area based on the company’s requirements and a previous parking study conducted by KLOA.

The actual construction of the onsite parking occurs in two phases. The first phase projects an initial peak demand of 6.5 cars per 1,000 square feet of floor area to accommodate up to 600 employees. The second phase would then draw from the land bank to provide enough spots for 7.2 cars per 1,000 square feet of floor area to accommodate future employee growth (up to 660 employees).

#### **COMPLIANCE WITH THE COMPREHENSIVE PLAN**

The Comprehensive Plan designates the subject property as Office/Corporate Campus, which is characterized by office uses with large-scale buildings and office parks in locations easily accessible from the I-88 & I-355 corridors. It also states that the village should continue to support office development in these corridors and that they should be of high quality and consistent with the developments at Esplanade and the Highland Landmark which places a greater emphasis on office versus industrial uses.

Based on the comprehensive plan, staff recommended the petitioner seek rezoning from M-1 to O-R-M even though office uses in M-1 are currently permitted. Based on the definition, O-R-M is the desired zoning district because the proposal is for a state-of-the-art corporate headquarters, and is the current zoning designation for the Esplanade and Highland Landmark developments.

Furthermore, this redevelopment of what is now a vacant industrial building would enhance the quality and appearance of the site. The proposal is consistent with the Comprehensive Plan.

#### **COMPLIANCE WITH THE ZONING ORDINANCE**

The subject property is currently zoned M-1, Light Manufacturing. The proposed development includes the rezoning to O-R-M, Office-Research-Manufacturing, and a corporate office is a permitted use. The proposed building will have to comply with all other regulations of the Zoning Ordinance that will be addressed through the building permit review process.

#### **Parking**

Parking requirements for business and professional office use are 3 spaces per 1,000 square feet of floor area. Parking ratios in excess of 4.5 spaces per 1,000 square feet of floor area are required to go through the special use process per section 7.020 of the zoning code. The petitioner is seeking approval to allow up to 7.2 cars per 1,000 square feet of floor area or 605 parking spaces. A parking study and research on office space trends has been provided.

Based on applying KLOA's average peak parking demand ratio of 0.88, this translates into approximately 528 parking spaces with another 17 visitor spaces during phase 1 of the project which makes a total of 545 spaces. The parking ratio for phase 1 is 6.5 spaces per 1,000 square feet of floor area. For phase 2, the land bank would provide additional spaces to meet the average peak parking demand. This is projected to be 581 parking spots with another 24 visitor spots making the total 605 spaces. Overall, the petitioner is seeking approval for the Phase 2 maximum ratio of 7.2 spaces per 1,000 square feet of floor area.

The higher parking ratios are attributed to multiple factors: efficiencies achieved by increasing employee density, lack of public transit servicing the area, and providing visitor parking. Technology and collaborative spaces allow for more employees to work in shared space with reduced square footage per employee compared to the specifications of older Class-A office buildings. Although the size of the building may change and an exact parking space count is not possible, the applicant is confident on accurately projecting the required parking ratios.

#### **ENGINEERING/PUBLIC IMPROVEMENTS**

All engineering provisions including stormwater regulations will be reviewed during the building permit process. Any proposed development will be compliant with the Stormwater and Floodplain Ordinance. At this time, no public improvements are required.

#### **PUBLIC SAFETY REQUIREMENTS**

The Fire Prevention division reviewed the proposal and had no comments.

#### **NEIGHBORHOOD COMMENT**

Notice was provided to all property owners 250 feet or less from the property line in addition to posting the public hearing sign and publishing a legal notice in *Downers Grove Suburban Life*. Staff has received no inquiries or public comment.

#### **FINDINGS OF FACT**

The petitioner outlined the request in the attached narrative letter, conceptual site plan, KLOA parking study, and commercial real estate trend documentation. Staff finds that the proposal meets the standards for granting the Rezoning and Special Use as outlined below:

Zoning Map Amendment – Rezoning

**Section 28.12.030.I. Review and Approval Criteria – Zoning Map Amendments (Rezoning)**

*The decision to amend the zoning map is a matter of legislative discretion that is not controlled by any single standard. In making recommendations and decisions about zoning map amendments, review and decision making bodies must consider at least the following factors:*

**(1) *The existing uses and zoning of nearby property.***

The subject property is currently zoned M-1, Light Manufacturing and contains an unoccupied industrial building. The surrounding properties are a diverse mix of zoning and uses. The adjacent property to the west as well as the rest of the properties north of Warrenville road are complementary office uses. To the south is a gas station, and across Finley Road to the east is a car dealership. This criteria is met.

**(2) *The extent to which the particular zoning restrictions affect property values.***

The proposed improvements and rezoning will have no negative impact on surrounding property values. It may have a positive impact because it is replacing an older industrial building with a high quality, corporate office development. This criteria is met.

**(3) *The extent to which any diminution in property value is offset by an increase in the public health, safety and welfare.***

The proposed rezoning will not negatively impact property values or the public health, safety and welfare of the community. This criteria is met.

**(4) *The suitability of the subject property for the zoned purposes.***

If the subject property were to remain M-1, allowed uses would continue to include building service, business support service, and consumer maintenance/repair. This does not complement the comprehensive plan with the future land use map showing this property as Office/Corporate Campus. This is the proposed use of the property, and it is more appropriate for this site to be rezoned to O-R-M. Similar examples include the Esplanade, Corridors, and the Highland Landmark which are zoned O-R-M. This criteria is met.

**(5) *The length of time that the subject property has been vacant as zoned, considering the context of land development in the vicinity.***

Under the current zoning, the existing industrial building has remained unoccupied for almost two years with little recent redevelopment on the north side of Warrenville Road. These properties are zoned M-1 and consist mainly of older, light-industrial buildings. The Corridors property farther to the west is zoned O-R-M and was developed with more modern office buildings. To the south and east, the Ogden corridor has experienced recent commercial redevelopment. This criteria is met.

**(6) *The value to the community of the proposed use.***

A modern, Class-A office building based on “forward thinking” principles is of high-value to the community and will redevelop a dormant industrial property. It demonstrates the community’s suitability to serve as the location for corporate headquarters. It may encourage the redevelopment of nearby industrial buildings. This criteria is met.

**(7) *The Comprehensive Plan.***

The proposal is consistent with the Comprehensive Plan and the goal of maintaining corporate campuses in close proximity to the interstates and providing for both the daily needs of residents as well as providing commercial services to the larger region. This criteria is met.

Special Use

The applicant is requesting Special Use approval for 7.2 parking spaces per 1,000 square feet of floor area in accordance with Section 28.7.020. The proposed use meets the standards for granting a Special Use as outlined below:

**Section 28.12.050.H Approval Criteria – Special Uses**

*No special use may be recommended for approval or approved unless the respective review or decision-making body determines that the proposed special use is constituent with and in substantial compliance with all Village Council policies and plans and that the applicant has presented evidence to support each of the following conclusions:*

1. *That the proposed use is expressly authorized as a Special Use in the district in which it is to be located; The maximum parking ratio is applicable to all commercial and office uses. Exceeding this parking ratio requires special use approval per Section 7.020 of the Zoning Ordinance. This criteria is met.*

2. *That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.*

The proposal is consistent with the Comprehensive Plan and the goal of maintaining high quality corporate campuses in close proximity to the interstates. The applicant has provided a parking study and research indicating the trend towards increasing parking ratios to accommodate employee densification. Modern construction of Class-A commercial space is required and the existing industrial building cannot be repurposed for a corporate headquarters. Without the additional parking, the corporate headquarters will seek other sites and the building will remain unoccupied. This criteria is met.

3. *That the proposed use will not, in the particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.*

The proposed use is complimentary to the area and will not have a negative impact on the health, safety or general welfare of the general vicinity. The redevelopment of an older industrial site is desired and may have a positive impact on adjacent property values. This criteria is met.

**RECOMMENDATIONS**

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The proposed Zoning Map Amendment (Rezoning) and Special Use for increased parking is compatible with the surrounding zoning and land use classifications, meets the criteria for Rezoning and Special Uses, and is consistent with the Comprehensive Plan.

Based on the findings listed above, staff recommends that the Plan Commission make a positive recommendation to the Village Council regarding 16-PLC-0023 subject to the following conditions:

1. The parking ratio for Phase II will not exceed 7.2 spaces per 1,000 square feet of floor area.
2. The petitioner shall consolidate the two lots into a single lot of record pursuant to Section 20.507 of the Subdivision Ordinance prior to the issuance of any site development or building permits.
3. The petitioner shall provide easement documentation for permissible construction on the Nicor Lot.

Staff Report Approved By:



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Stanley J. Popovich, AICP  
Planning Manager

SP:sw  
-att

P:\P&CD\PROJECTS\PLAN COMMISSION\2016 PC Petition Files\16-PLC-0023 - 2200 Warrenville Road - Rezoning & Special Use\Staff  
Report 16-PLC-0023.docx



0 62.5 125 Feet

2200 Warrenville Rd. - Location Map



Mr. Stan Popovich  
Directory of Community Development  
801 Burlington Avenue  
Downers Grove, IL 60515

[spopovich@downers.us](mailto:spopovich@downers.us)

Dear Mr. Popovich

As a follow up to our recent meetings with Village staff, we submit to you a brief summary of the very exciting project that we are planning at 2200 Warrenville Road, together with our formal request for the zoning change and special use we are requesting.

### ***The Site***

Arbor Vista LLC (“Owner”) recently acquired the former Perkin Elmer property located at 2200 Warrenville Road in Downers Grove (the “Site”). The 7-acre site offers extraordinary visibility, with direct frontage on I-88 and I-355. The site is seen by over 240,000 cars per day. Access to full 4-way interchanges on both highways is approximately 1/4 mile from the site at Ogden Avenue.

### ***The Company***

We are currently in active negotiations with a Fortune 500 company (the “Company”) to develop a very unique and “forward looking” suburban Chicago office / headquarters property for their *exclusive use*.

The Company is consolidating multiple suburban office spaces into this build-to-suit facility which will be home for up to 550 to 600 corporate employees with potential to accommodate future growth up to 660 employees. We anticipate ground-breaking in October, 2016 with project completion scheduled for late 2017.

### ***The Project***

The project will consist of a two-story “Class-A” office building designed to accommodate the Company’s immediate and long term needs. The “forward-looking” design will involve the latest in design efficiency trends for Class-A office space, geared specifically toward reducing the footprint of the building (increasing employee density) to achieve cost-savings and reduce impact to the environment. We have attached exhibits containing just a few samples of the many articles written on this subject by architects and design experts. Further, recent developments in IT and telecommunications systems allow employees to share workspace (a concept called “hoteling”). Hoteling is a workplace strategy which reduces office space costs by increasing the number of workers supported by each desk or workspace. Hoteling eliminates the high rate of vacancy that occurs when employees are on vacation (or working from another site), leaving desks empty for the day. This hoteling concept will be applied for many of the employees who will be based at this location.

The size of the building will be specifically designed to meet the Company's anticipated (long term) peak daily occupancy of approximately 7.2 occupants per 1,000 square feet of building area (initially 6.5 occupants per 1,000 SF) taking these considerations into account. The vast majority of the existing Class-A buildings in the I-88 corridor were built in the 1980's (and earlier) and therefore cannot accommodate corporate users seeking to achieve this level of efficiency (due to lack of available parking, insufficient HVAC capacity, IT infrastructure and restroom facilities). It is for these reasons that new construction is the only viable option for this requirement.

Parking for this development will be designed to meet the daily peak demand referenced above, with surface parking initially designed for approximately 6.5 cars per 1,000 SF with potential to expand up to a total of approximately 7.2 cars per 1,000 SF. Any parking not required for the initial staffing needs will be "landbanked" (green space set aside for future parking).

It is important to note that the initial employee count at this time remains undetermined and therefore the ratio of initial parking to landbanked parking is subject to change; however the intent is to accommodate up to 7.2 cars per 1,000 square feet to meet the Company's growth projections.

The plans attached hereto are conceptual only and are being submitted to illustrate the general orientation and configuration of the site based on the largest projections at this time. ***We are not requesting approval of the attached site plan per se. At this time we are simply seeking a Special Use so as to permit the ratio of 7.2 cars per 1,000 SF of building area.***

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### ***Special Use Request***

The Review and Approval Criteria for a Special Use Criteria per Section 12.050.H of the Zoning Ordinance (and our responses) are as follows:

#### **28.12.050**

##### **I. Review and Approval Criteria**

- 1. that the proposed use is expressly authorized as a special use in the district in which it is to be located; The proposed office use is a permitted use within the M-1 Zoning District and is consistent with the Village of Downers Grove Comprehensive Plan; the Special Use request relates only to the permitted parking ratio;*
- 2. that the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community; The proposed office use is consistent with the Village of Downers Grove Comprehensive Plan and the proposed parking ratio is consistent with the trends in Class-A office designed as further illustrated in the attached exhibits;*
- 3. that the proposed use will not, in the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity. The proposed office use is consistent with the Village of Downers Grove Comprehensive Plan and will not be detrimental to the health, safety, or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.*

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## ***Zoning Change Request***

Although the proposed office use is a permitted use within the M-1 Zoning District (as the Subject Property is presently zoned), we are seeking a zoning change from M-1 to ORM to more closely align with the Village of Downers Grove Comprehensive Plan and with the proposed Class-A Corporate Headquarters building;

The Review and Approval Criteria for a Zoning Change per Section 28.12.030.I in the Zoning Ordinance (and our responses) are as follows:

### **28.12.030**

#### **I. Review and Approval Criteria**

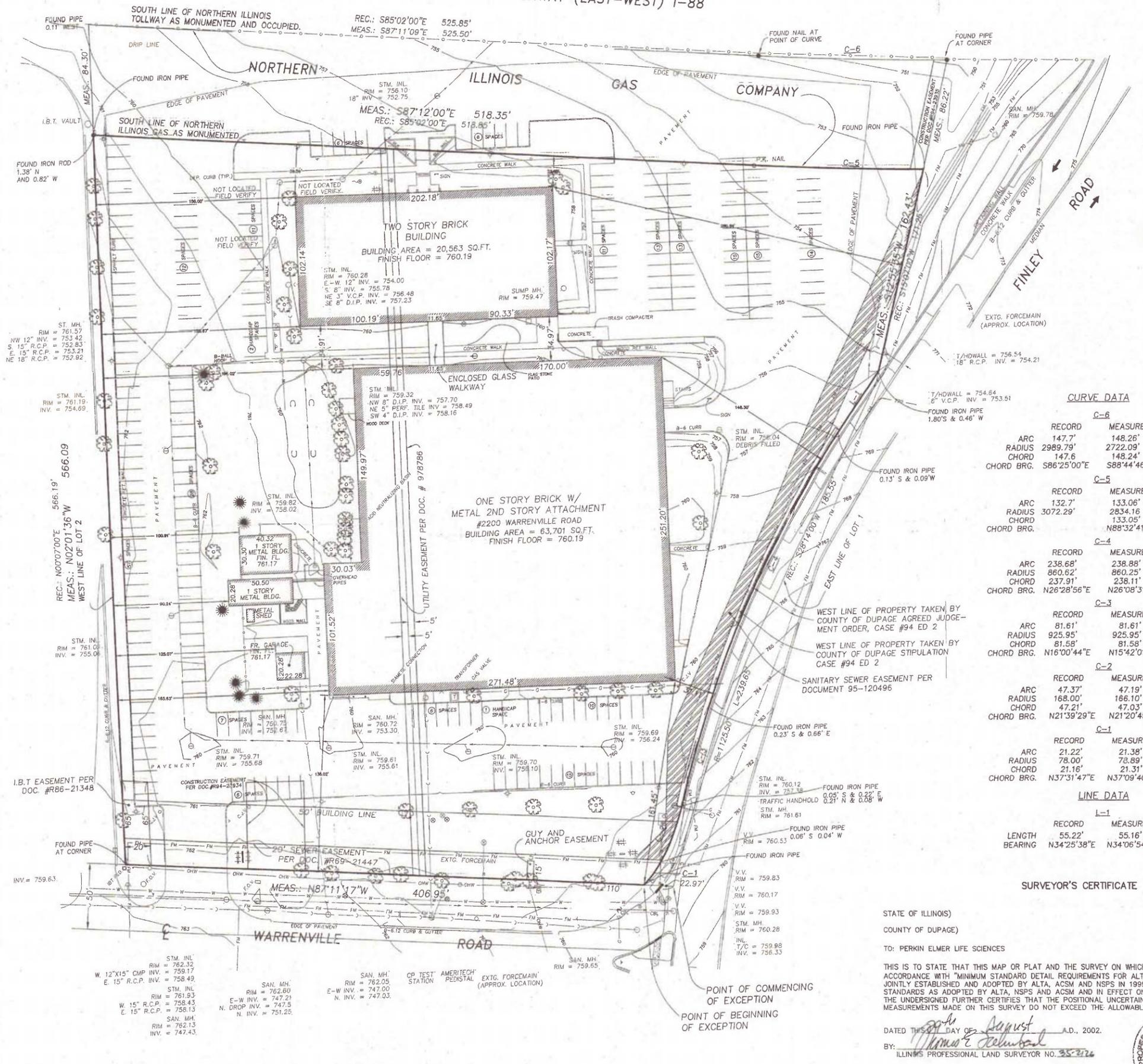
*The decision to amend the zoning map is a matter of legislative discretion that is not controlled by any single standard. In making recommendations and decisions about zoning map amendments, review and decision-making bodies must consider at least the following factors:*

- 1. the existing use and zoning of nearby property:* The Subject Property and the neighboring properties along the north side of Warrenville Road are presently zoned M-1, however the zoning of the Subject Property from M-1 to ORM is consistent with the Village of Downers Grove Comprehensive Plan and with the proposed Class A Corporate Headquarters building;
- 2. the extent to which the particular zoning restrictions affect property values:* The proposed office use is a permitted use within the current M-1 zoning district, however the rezoning of the Subject Property from M-1 to ORM may have a positive impact on adjacent property values along the north side of Warrenville Road;
- 3. the extent to which any diminution in property value is offset by an increase in the public health, safety and welfare:* There will be no diminution in value associated with the proposed rezoning of the Subject Property from M-1 to ORM;
- 4. the suitability of the subject property for the zoned purposes:* The rezoning of the Subject Property from M-1 to ORM is consistent with the Village of Downers Grove Comprehensive Plan and with the proposed Class-A Corporate Headquarters building;
- 5. the length of time that the subject property has been vacant as zoned, considering the context of land development in the vicinity:* The Subject Property has been vacant for approximately two years. The existing manufacturing and office facility is obsolete.
- 6. the value to the community of the proposed use:* The rezoning of the Subject Property from M-1 to ORM is consistent with the Village of Downers Grove Comprehensive Plan and with the proposed Class A Corporate Headquarters building;
- 7. the comprehensive plan:* The rezoning of the Subject Property from M-1 to ORM is consistent with the Village of Downers Grove Comprehensive Plan and with the proposed Class-A Corporate Headquarters building.

# PLAT OF SURVEY

OF:

NORTHERN ILLINOIS TOLL HIGHWAY (EAST-WEST) I-88



LOTS 1 AND 2 IN SMITH REALTY COMPANY'S RESUBDIVISION BEING A RESUBDIVISION IN THE EAST 1/2 OF SECTION 1, TOWNSHIP 38 NORTH, RANGE 10- EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED ON SEPTEMBER 8, 1960 AS DOCUMENT NO. 978786 IN BOOK 40 PAGE 75, IN DUPAGE COUNTY, ILLINOIS.

EXCEPTING THEREFROM THAT PART OF SAID LOT 1 BEING DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHEAST CORNER OF SAID LOT 1; THENCE WESTERLY ON THE SOUTH LINE OF SAID LOT 1, SAID LINE ALSO BEING THE NORTHERLY RIGHT-OF-WAY OF WARRENVILLE ROAD, SAID LINE HAVING A BEARING OF NORTH 87 DEGREES 10 MINUTES 56 SECONDS WEST, A DISTANCE OF 22.97 FEET TO A POINT; THENCE NORTHEASTERLY ON A LINE, A DISTANCE OF 21.22 FEET TO A POINT; THENCE NORTHEASTERLY ON A CURVE CONCAVE TO THE NORTHWEST, HAVING A RADIUS OF 78.00 FEET, A CENTRAL ANGLE OF 15 DEGREES 35 MINUTES 20 SECONDS AND THE LONG CHORD OF SAID CURVE BEARS NORTH 37 DEGREES 31 MINUTES 47 SECONDS EAST, A CHORD DISTANCE OF 21.16 FEET TO A POINT; THENCE NORTHEASTERLY ON A LINE, A DISTANCE OF 47.37 FEET TO A POINT; THENCE NORTHEASTERLY ON A CURVE CONCAVE TO THE WEST, HAVING A RADIUS OF 168.00 FEET, A CENTRAL ANGLE OF 16 DEGREES 09 MINUTES 18 SECONDS AND THE LONG CHORD OF SAID CURVE BEARS NORTH 21 DEGREES 39 MINUTES 29 SECONDS EAST, A CHORD DISTANCE OF 47.21 FEET TO A POINT; THENCE NORTHEASTERLY IN A LINE, A DISTANCE OF 81.61 FEET ON A CURVE CONCAVE TO THE EAST, HAVING A RADIUS OF 925.25 FEET, A CENTRAL ANGLE OF 5 DEGREES 02 MINUTES 59 SECONDS AND THE LONG CHORD OF SAID CURVE BEARS NORTH 16 DEGREES 00 MINUTES 44 SECONDS EAST, A CHORD DISTANCE OF 238.68 FEET TO A POINT; THENCE NORTHEASTERLY IN A LINE, A DISTANCE OF 238.68 FEET ON A CURVE CONCAVE TO THE SOUTHWEST, HAVING A RADIUS OF 860.62 FEET, A CENTRAL ANGLE OF 15 DEGREES 53 MINUTES 24 SECONDS AND THE LONG CHORD OF SAID CURVE BEARS NORTH 26 DEGREES 28 MINUTES 56 SECONDS EAST, A CHORD DISTANCE OF 237.91 FEET TO A POINT; THENCE NORTHEASTERLY ON A LINE HAVING A BEARING OF NORTH 34 DEGREES 25 MINUTES 38 SECONDS EAST, A DISTANCE OF 55.22 FEET TO A POINT IN THE WESTERLY RIGHT-OF-WAY LINE OF FINLEY ROAD; THENCE SOUTHWESTERLY ON SAID WESTERLY RIGHT-OF-WAY LINE, SAID LINE HAVING A BEARING OF SOUTH 12 DEGREES 50 MINUTES 48 SECONDS WEST, A DISTANCE OF 7.01 FEET TO A POINT; THENCE SOUTHWESTERLY ON SAID WESTERLY RIGHT-OF-WAY LINE, SAID LINE HAVING A BEARING OF SOUTH 26 DEGREES 26 MINUTES 35 SECONDS WEST, A DISTANCE OF 186.86 FEET TO A POINT; THENCE SOUTHWESTERLY ON SAID WESTERLY RIGHT-OF-WAY LINE, A DISTANCE OF 239.69 FEET ON A CURVE CONCAVE TO THE EAST, HAVING A RADIUS OF 1091.80 FEET, A CENTRAL ANGLE OF 12 DEGREES 34 MINUTES 43 SECONDS AND THE LONG CHORD OF SAID CURVE BEARS SOUTH 20 DEGREES 09 MINUTES 13 SECONDS WEST, A CHORD DISTANCE OF 239.21 FEET TO A POINT IN THE NORTHERLY RIGHT-OF-WAY LINE OF WARRENVILLE ROAD; THENCE NORTHWESTERLY ON SAID NORTHERLY RIGHT-OF-WAY LINE, SAID LINE HAVING A BEARING OF NORTH 87 DEGREES 10 MINUTES 56 SECONDS WEST, A DISTANCE OF 22.97 FEET TO THE POINT OF BEGINNING. ALL IN DUPAGE COUNTY, ILLINOIS.

FOR THE PURPOSE OF THIS SURVEY THE BASIS OF BEARING IS THE NORTH LINE OF WARRENVILLE ROAD BEING NORTH 87 DEGREES 10 MINUTES 56 SECONDS WEST.

ST. MH. RIM = 761.57  
 NW 12" INV. = 753.42  
 S. 15" R.C.P. = 753.83  
 E. 15" R.C.P. = 753.21  
 NE 18" R.C.P. = 752.92

ST. IN. RIM = 761.19  
 INV. = 754.69

REC.: N00707'00"E 566.19'  
 MEAS.: N0201'36"W 566.09'  
 WEST LINE OF LOT 2

ST. IN. RIM = 761.04  
 INV. = 755.04

ST. IN. RIM = 761.17  
 INV. = 755.06

ST. IN. RIM = 761.71  
 INV. = 755.68

### CURVE DATA

RECORD	MEASURED
ARC 147.7'	148.26'
RADIUS 2989.79'	2722.09'
CHORD 147.6'	148.24'
CHORD BRG. S86°25'00"E	S88°44'46"E

RECORD	MEASURED
ARC 132.7'	133.06'
RADIUS 3072.29'	2834.16'
CHORD 132.7'	133.05'
CHORD BRG. N88°32'41"W	

RECORD	MEASURED
ARC 238.68'	238.88'
RADIUS 860.62'	860.25'
CHORD 237.91'	238.11'
CHORD BRG. N26°28'56"E	N26°08'31"E

RECORD	MEASURED
ARC 81.61'	81.61'
RADIUS 925.95'	925.95'
CHORD 81.58'	81.58'
CHORD BRG. N16°00'44"E	N15°42'01"E

RECORD	MEASURED
ARC 47.37'	47.19'
RADIUS 168.00'	166.10'
CHORD 47.21'	47.03'
CHORD BRG. N21°39'29"E	N21°20'45"E

RECORD	MEASURED
ARC 21.22'	21.38'
RADIUS 78.00'	78.89'
CHORD 21.16'	21.31'
CHORD BRG. N37°31'47"E	N37°09'40"E

### LINE DATA

RECORD	MEASURED
LENGTH 55.22'	55.16'
BEARING N34°25'38"E	N34°06'54"E

### AREA SUMMARY

LOTS 1 & 2	307,605 SQ.FT.
EXCEPTION	5,904 SQ.FT.
NET	301,701 SQ.FT.

### LEGEND

- DECIDUOUS TREE
- PINE TREE
- FIRE HYDRANT
- VALVE VAULT
- VALVE BOX/VALVE INDICATOR
- STORM INLET
- STORM MANHOLE/OPEN LID
- STORM MANHOLE
- SANITARY MANHOLE
- FORCE MAIN
- SIGN
- PEDISTAL
- POWERPOLE/GUY WIRE/OVERHEAD WIRES
- YARD LIGHT
- LIGHTPOST
- TRANSFORMER
- BALLARD POST
- FLAGPOST
- HORSESHOE PIT
- FIBRE OPTIC VAULT
- CABLE JUNCTION VAULT
- FENCE LINE

NOTES:  
 ALL UTILITIES WERE LOCATED BY FIELD INVESTIGATION AND AVAILABLE MAPS AND PLANS. CALL J.U.L.I.E. AT 1-800-892-0123 BEFORE DIGGING.

YARD SPRINKLERS NOT SHOWN.  
 LANDSCAPING NOT SHOWN.

THIS SURVEY WAS PREPARED WITHOUT BENEFIT OF A TITLE REPORT.

THE SANITARY SEWER EASEMENT LOCATED ON THE EAST SIDE OF THE BUILDING DOES NOT MATHEMATICALLY CLOSE BY A DISTANCE OF 19.05', THEREFORE THE FIGURE SHOWN REPRESENTS THE INTENT AS DEPICTED UPON DOCUMENT R95-120496.

TOPOGRAPHIC INFORMATION SHOWN IS BASED UPON FIELD WORK PERFORMED IN AUGUST OF 1998. TOPO HAS NOT BEEN UPDATED WITH THIS SURVEY.

### SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS  
 COUNTY OF DUPAGE)

TO: PERKIN ELMER LIFE SCIENCES

THIS IS TO STATE THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH "MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS," JOINTLY ESTABLISHED AND ADOPTED BY ALTA, ACSM AND NSPS IN 1999, PURSUANT TO THE ACCURACY STANDARDS AS ADOPTED BY ALTA, NSPS AND ACSM AND IN EFFECT ON THE DATE OF THIS CERTIFICATION, THE UNDERSIGNED FURTHER CERTIFIES THAT THE POSITIONAL UNCERTAINTIES RESULTING FROM THE SURVEY MEASUREMENTS MADE ON THIS SURVEY DO NOT EXCEED THE ALLOWABLE POSITIONAL TOLERANCE.

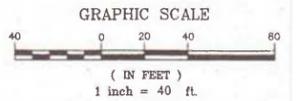
DATED THIS 29th DAY OF August A.D., 2002.

BY: *Thomas E. Fahrenbach*  
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35272



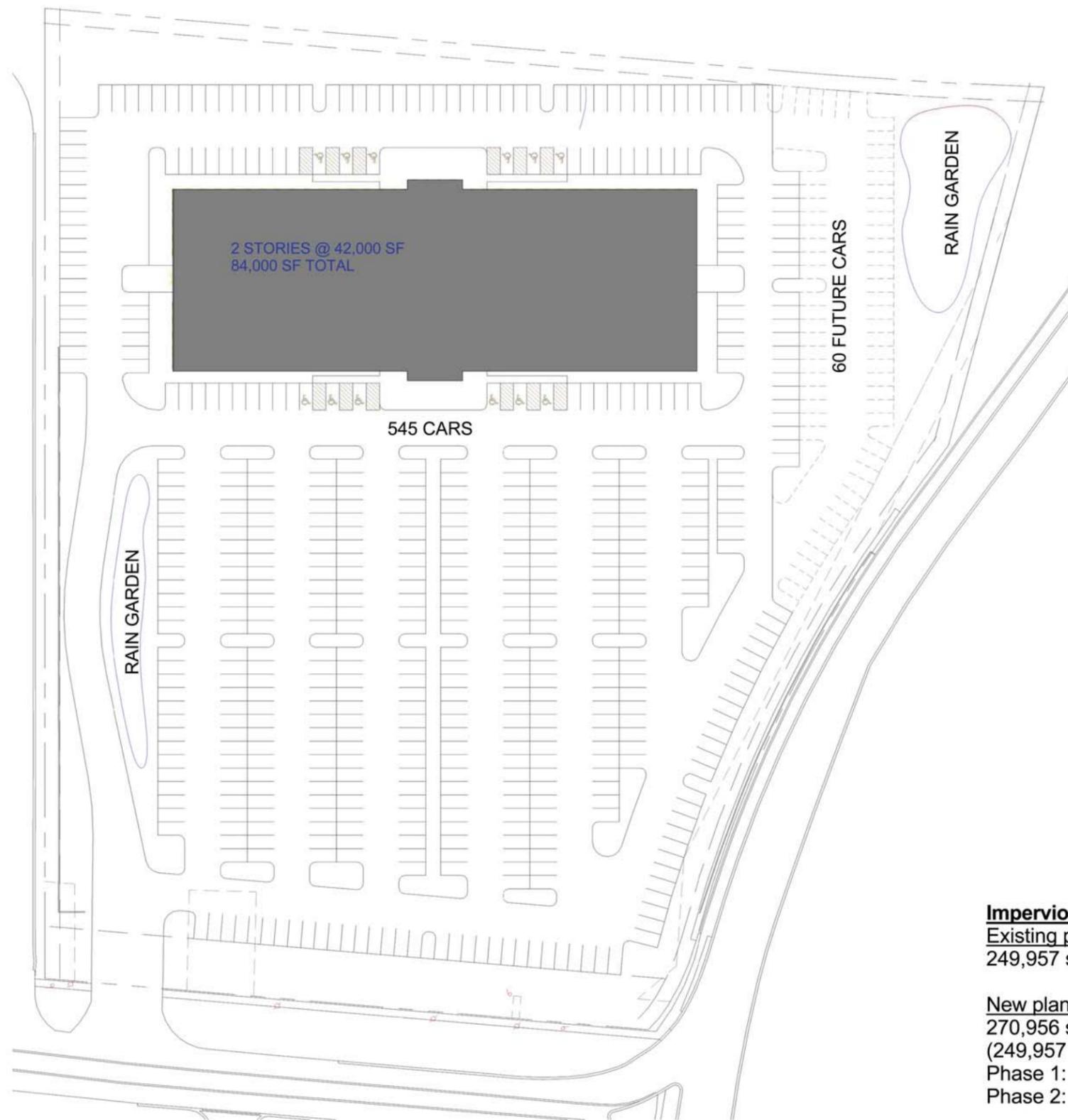
BENCHMARK:  
 CROSS CUT ON THE SOUTHWEST STUD OF THE NORTH POST FOR THE MOBILE SIGN, LOCATED AT THE SOUTHEAST CORNER OF BELMONT ROAD AND OGDEN AVE. (RT. 34). ELEVATION = 756.73 VILLAGE OF DOWNERS GROVE DATUM.

SCALE: 1" = 40'



**INTECH CONSULTANTS, INC.**  
 ENGINEERS / SURVEYORS  
 5413 WALNUT AVE. DOWNERS GROVE, IL. (630) 964-5656

REVISION: 08-29-02  
 PREPARED: 08-28-98



**Impervious area calcs:**

Existing plan:  
249,957 sf impervious area

New plan:  
270,956 sf impervious area maximum  
(249,957 sf + 24,999 sf)  
Phase 1: 239,622 sf impervious area  
Phase 2: 260,365 sf impervious area

**Car counts:**

Phase 1: 545 cars (6.5 cars/1,000 sf)  
Phase 2: 605 cars (7.2 cars/1,000 sf)

**1 PROPOSED SITE PLAN**  
A1.0 1" = 40'-0"



**DESIGN DRAWINGS:  
NOT FOR CONSTRUCTION**

This plan is not to be used for construction. Plan layouts are for concept design only, and are subject to change based on detailed local code analysis and thorough evaluation of specific existing building conditions. Employee counts, furniture inventories and product inventories are provided as preliminary guidelines estimates only. Detailed design documents will be required by licensed design professionals of record, as project moves forward.

**2200 WARRENVILLE**

2200 Warrenville Rd  
Downers Grove, IL 60515

ISSUE DATE  
04-28-2016

PROGRESS SET

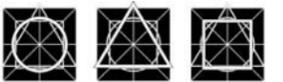
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**1** FIRST FLOOR FRAMING AND CORE  
**A2.0** 1/16" = 1'-0"

**2200 WARRENVILLE**

2200 Warrenville Rd  
Downers Grove, IL 60515

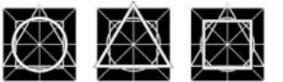
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FIRST FLOOR FRAMING AND CORE

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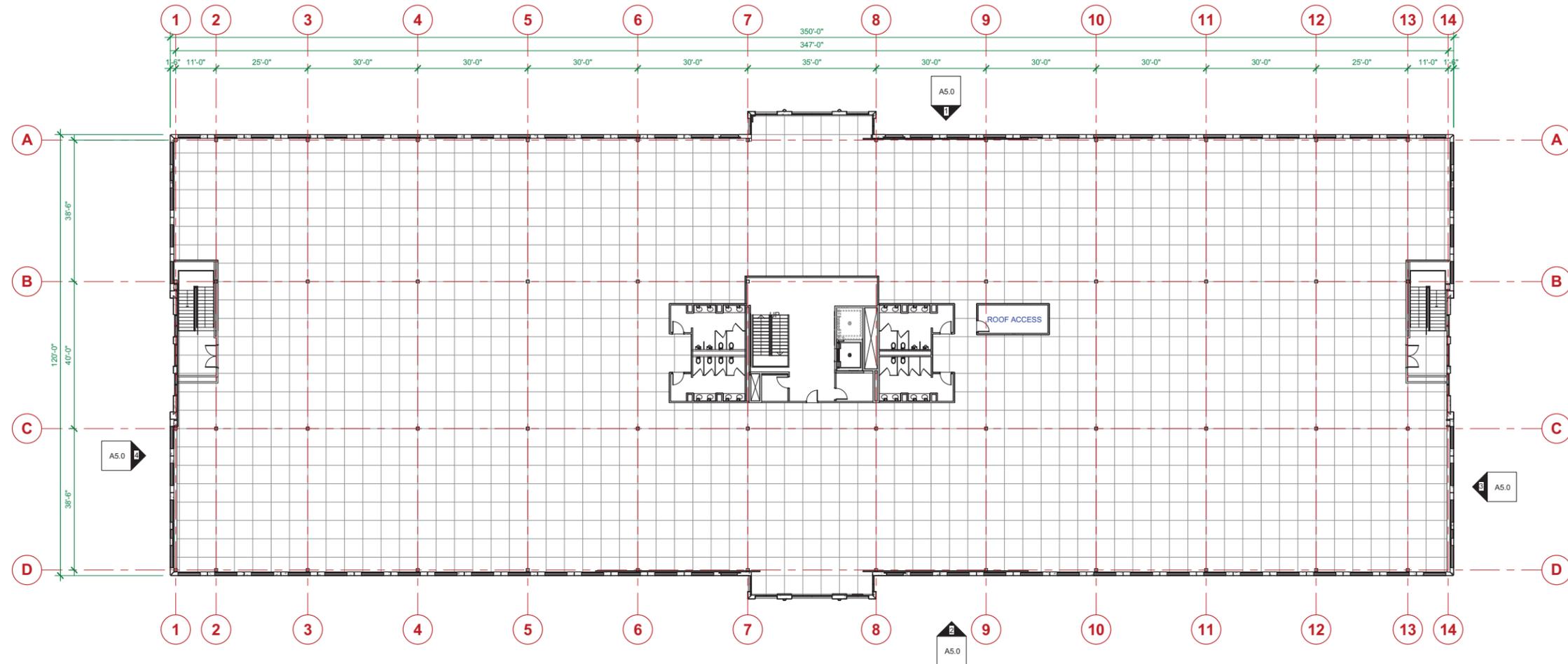


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**1** SECOND FLOOR FRAMING AND CORE  
**A2.2** 1/16" = 1'-0"

**2200 WARRENVILLE**

2200 Warrenville Rd  
Downers Grove, IL 60515

ISSUE DATE  
04-28-2016      PROGRESS SET

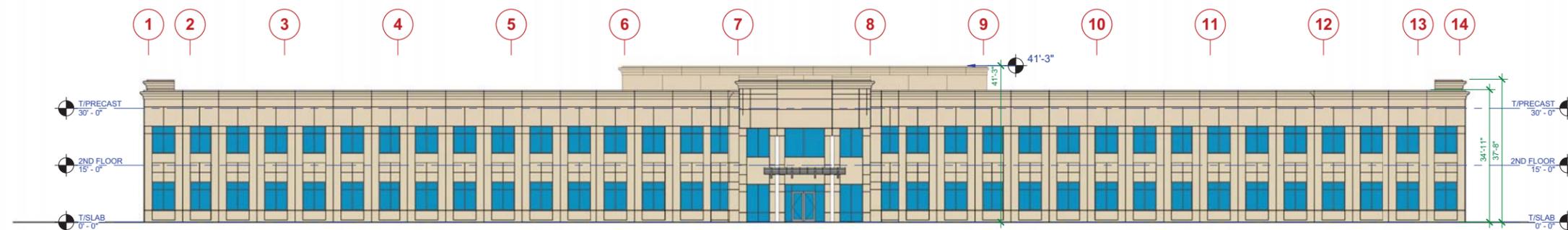
SHEET TITLE  
SECOND FLOOR FRAMING AND CORE

DATE      SHEET NUMBER  
04/28/16      **A2.2**  
PROJECT NUMBER  
215149

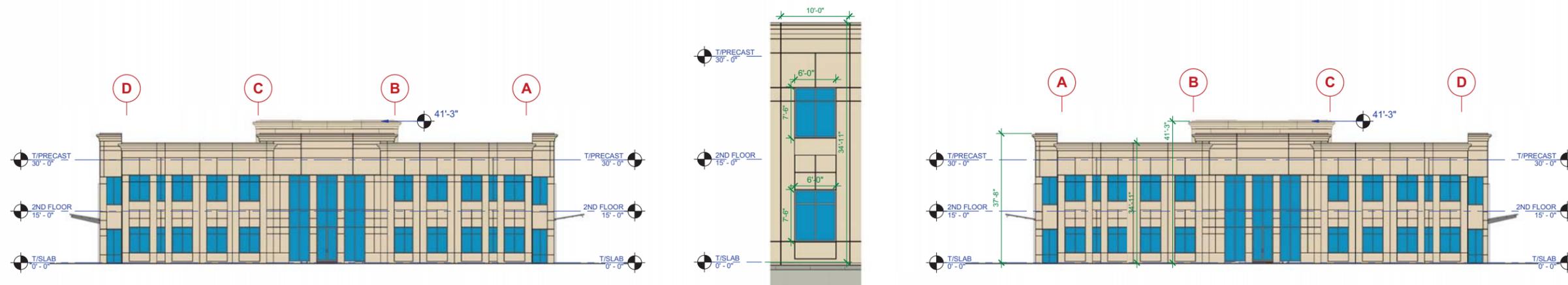
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**1 NORTH ELEVATION**  
A5.0 1/16" = 1'-0"



**2 SOUTH ELEVATION**  
A5.0 1/16" = 1'-0"



**3 EAST ELEVATION**  
A5.0 1/16" = 1'-0"

**5 TYP. PANEL**  
A5.0 1/8" = 1'-0"

**4 WEST ELEVATION**  
A5.0 1/16" = 1'-0"

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**2200 WARRENVILLE**

2200 Warrenville Rd  
Downers Grove, IL 60515

ISSUE DATE  
04-28-2016    PROGRESS SET

SHEET TITLE  
BUILDING ELEVATIONS

DATE  
04/28/2016

PROJECT NUMBER  
215149

SHEET NUMBER

**A5.0**

As discussed, we are happy to provide any additional information the Village may require to review and approve this request.

The Owner's primary point of contact and communication for all matters relating to this request will be:

Adam Stokes, Manager  
Arbor Vista LLC  
Nicolson Porter & List, Inc.  
1300 W. Higgins Road, Suite #104  
Park Ridge, IL 60068

We appreciate your consideration and look forward to working with the Village of Downers Grove on this exciting project.

Very truly yours,

A handwritten signature in black ink that reads "Adam Stokes". The signature is written in a cursive style and is positioned to the left of a vertical line.

Adam Stokes  
Vice President



**MEMORANDUM TO:** Adam Stokes  
Nicolson Porter & List, Inc.

**FROM:** William R. Woodward  
Senior Consultant

Luay R. Aboona, PE  
Principal

**DATE:** July 8, 2016

**SUBJECT:** Parking Evaluation  
2200 Warrenville Road  
Downers Grove, Illinois

The purpose of this memorandum is to summarize a parking evaluation conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for the proposed office development to be located at 2200 Warrenville Road in Downers Grove, Illinois. The existing building will be removed to accommodate the proposed development.

The conceptual plans presented call for a two-story, 84,000 square-foot office building. The building will be occupied by a single, corporate tenant. The tenant intends to initially staff the building with approximately 550 to 600 employees with the potential to increase the employee occupancy to accommodate up to approximately 660 employees.

The development proposes a total of approximately 545 off-street parking spaces with a land bank to potentially provide an additional 60 parking spaces (for a total of 605 parking spaces) should the staffing levels increase as noted above. The parking supply includes approximately 525 parking spaces (initially) for the employees with the remaining to be provided for visitors.

The proposed parking supply of 545 to 605 off-street parking spaces will be sufficient and appropriate to accommodate the initial and projected peak parking demands of the proposed development based on the following:

- The proposed 545 to 605 parking spaces provide one parking space per employee in addition to parking spaces for visitors. Given the location of the site in a suburban location with limited or no access to public transportation, it is expected that a majority, if not all, of the employees will drive to/from their workplace on a daily basis.

- According to the Parking Generation Manual, 4th Edition, published by the Institute of Transportation Engineers (ITE), the average peak parking demand ratio for an office land use is 0.83 parking spaces per employee. Based on the initial employee occupancy of up to 600 employees, the development will need a total of 498 employee parking spaces, plus parking for visitors. As such, the proposed 545 off-street parking spaces will be adequate to accommodate the peak parking demand for this development. Based on the projected increase in employee occupancy to 660 employees, the development will need a total of 581 employee parking spaces plus parking for visitors. As such the proposed future (landbanked) parking for 605 off-street parking spaces will be adequate to accommodate the future peak parking demand for this development.
- The results of a survey conducted by KLOA, Inc. for a previous study for a different single-occupancy tenant showed that the average peak parking demand ratio is 0.88 parking spaces per employee, resulting in a total of 528 employee parking spaces needed to accommodate the initial peak parking demand and 581 employee parking spaces needed to accommodate the future peak parking demand (based on 600 and 660 employees respectively), plus parking for visitors. As such the proposed 545 off-street parking spaces (and 605 future parking spaces) will be adequate to accommodate the initial (and future) peak parking demand (employees and visitors) for this development.
- The Village of Downers Grove's Code requires a minimum of 3 spaces per 1,000 square feet of floor area, or 252 parking spaces which is insufficient to accommodate the peak parking demand for this development.

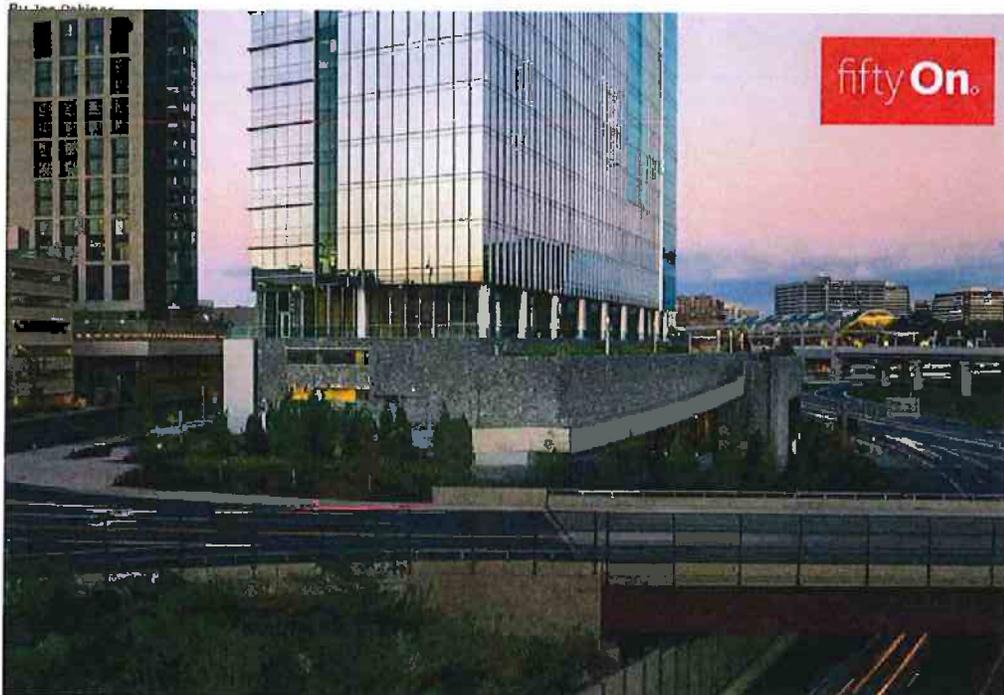


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## Five Megatrends Shaping Commercial Office Development



*Tysons Corner is a perfect example of a mixed-use development that's stayed relevant by ensuring its commercial office buildings give workers what they want. Image © Prakash Patel*

We've heard it all before. After World War II, the Baby Boom generation escaped the city in favor of the green horizon of the suburbs, and escaped the travails of urban life. Traffic, smog, crowds and crime were to become a thing of the past.

Except that never happened.

The post-war migration to the suburbs rid the country of certain problems while creating new ones. Communities separated developments by use. Offices, schools, stores—and everything else—were built to be accessible by car. Regulatory codes written for the car-dependent lifestyle compounded the problem and increased traffic. As a result, many of the suburban office

developments built after World War II now seem antiquated in an American culture where people no longer want to be tethered to automobiles.

The developments and offices we plan and construct today are being governed by something I like to call New Office-ism. It's a design approach that eschews monolithic uses in favor of mixed-use, walkable patterns. New Office-ism incorporates aspects of master planning, urban design and landscape architecture into commercial office development. It is what the next generation workforce expects, and communities that cannot provide it will fail to attract new businesses and be in danger of losing the ones they already have.

New Office-ism yields fully-functioning and walkable neighborhoods that just happen to be anchored by commercial office buildings. Here are five megatrends driving this new paradigm.

### **1. Population Growth and Urbanity**

The U.S. population currently stands at just over 320 million. When we hit the 400 million mark sometime after 2030, we'll need to consider where everyone will live and work. A joint AIA/APA 2007 study projected that by 2030, 66 percent of American jobs would be located in areas that were developed after 1950 – i.e., suburbs. While the Great Recession blunted some of that momentum, the trajectory is still ever outward, though Americans have shown a renewed interest in the kind of lifestyle that comes with living in an urban area.

To this end suburbs are thinking more like cities. There's been a renewed emphasis on mixed-use development patterns. New Office-ism is a direct result of this: it's transforming suburbs into quasi-proto-urban areas.

### **2. Expectations of the Next Generation Workforce**

New Office-ism encourages a mixed-use approach to corporate campus development, because that's what the next generation workforce wants.

Highly-amenitized office campuses epitomize what office workers want. They want more than a desk and a place to park. They want environments that provide access to retail, dining, recreation and child care amenities. They want places to grab lunch and run a few errands. And they would like to be close enough to home to check in with pets and family.

These new predilections have turned office design into an "arms race" in which attracting and retaining employees is the primary objective. Companies that fail to provide New Office-ist amenities risk being on the losing end. Companies are relocating to wherever such developments are available, or to cities that allow such development. Cities and suburbs that fail to modify regulatory codes to smooth the transition of existing and future office districts into mixed-use developments will find themselves behind the curve.

### **3. Design for People, Not Cars**

In the near future, the car will no longer be the primary form-giver of commercial office development. It's high time we got the memo.

We used to plan offices with parking ratios of just four spaces per 1,000 square feet. But as more offices adopted high performance

workspace layouts, that ratio increased dramatically, to eight or more spaces per 1,000 square feet. Today, areas for vehicle storage can easily be 140 to 280 percent more than people-oriented spaces, so at some point the question arises: who are we really planning for—people or cars?

The answer to that question should be people, because the next generation workforce appears not to have much interest in car ownership. They make decisions on where to work based on quality of life factors, which often do not involve cars. So how should New Office-ism incorporate this crucial evolution?

One potential approach: prioritize location. New developments adjacent to rail transit stations can reduce parking demand by 15 to 20 percent. This frees up space and prevents the need for additional capital investments in garages and parking lots. Transit friendly offices also offer employees that coveted choice: car or light rail. Next generation workers have consistently shown a preference for work environments that offer multiple transportation choices.

Luckily the number of U.S. cities with light rail transit systems has steadily grown since the 1980s. Today, there are more than 30 cities with regional rail systems. Even car-oriented cities like Los Angeles and Dallas have adopted rail as a real alternative to driving.

Depending on where you are, rail transit may only shift a small percentage of commuters out of their cars. But they also increase property value and appeal through proximity to transit-oriented development (TOD), which places stations in close proximity to residential, office and retail uses and gives workers easy access to the amenities they crave. TOD has transformed Tysons Corner in suburban Washington, D.C., from a traditional suburban regional shopping destination into a mixed-use, pedestrian-friendly environment. It can have that same transformative effect for both suburban and urban infill sites.

#### **4. The Parking Conundrum Part II: Autonomous Vehicles**

In order to stay relevant, New Office-ism must anticipate what happens when demand for existing and future parking garages decreases.

Self-driving cars are no longer the stuff of science fiction. They are currently being tested on the roads of Los Angeles, Austin and Las Vegas. Increased adoption of these technologies can radically decrease parking demand. Who needs a parking space, if your car can drop you off and park itself remotely?

So if parking garages are destined to become less relevant, how should we design them? It is easy enough to return portions of a surface parking lot to green space or other non-car uses, but what about the humble garage? Because garage spaces are much more expensive to construct, we should start designing them with built-in exit strategies. We should conceive of garages as more than vehicle storage space—they should be the framework for future buildings. We can accomplish this by increasing floor-to-floor heights and designing on a module that would allow garages to transition into occupy-able space.

Call it future-proofing the garage of today.

#### **5. Technology and the Definition of "Office"**

Collaboration is the typical justification for bringing people together under one roof, but now that workers can easily video

conference with colleagues from other time zones, the nature of collaboration has changed. Even if employees commute/walk to a single campus every day, their workplace may exist in multiple locations.

Of course not everyone is going to be camped out at the local Starbucks with a laptop and a latte and a schedule of cross-country conference calls. But the next generation workforce campus will learn from technology and adopt New Office-ism principles of mixed-uses, walkability, social spaces and retail/dining options. Doing so creates work environments as flexible as the technologies we rely upon. Hotelling, telepresence devices, and even traditional conference rooms and workspaces equipped with the right technologies allow New Office-ism to emphasize choice, mixed-use, socialization, and—yes—collaboration.

These trends are shaping how we conceive and design commercial office buildings and corporate campuses, and designers must continue to understand what workers and companies want if they hope to create relevant and lasting design solutions.



**Joe Pobiner** is a master planner and urban designer in Gensler's Dallas office. He specializes in applying responsible planning and urban design principles that strengthen the physical, natural, economic, and cultural frameworks of our rapidly urbanizing planet. Contact him at [joe\\_pobiner@gensler.com](mailto:joe_pobiner@gensler.com).

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by lpoui at k'eer laps on 12.26.2015  
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at Five Megatrends Shaping Commercial Office on 01.20.2016  
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- Response: valentines days weeklist  
by afu at website on 01.30.2016
- Response: ouverture de porte paris 17  
by ouverture de porte paris 17 at ouverture de porte paris 17 on 02.6.2016  
ouverture de porte paris 17
- Response: Fotocopy Machine  
by Devan at Fotocopy Jakarta on 02.10.2016  
I think this kind of office is waste of money. Because it didn't have much people work there :) So better keep this kind of office didn't build again in the near future. Well glad that my building didn't as big as this :D
- Response: Advanced Auto Parts Coupons  
at Advanced Auto Parts Coupons on 02.16.2016  
Hiya Cool post, thank you
- Response: whatsapp status quotes for Holi 2016  
by backforu faggchal at Holi whatsapp messages 2016 on 02.16.2016
- Response: Corporate Marketing Photography Washington DC  
at Corporate Marketing Photography Washington DC on 02.23.2016  
Five Megatrends Shaping Commercial Office Development - fifty On - architecture and design
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at Five Megatrends Shaping Commercial Office on 03.3.2016  
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# Increased Office Density is Causing Real Trouble for Parking Lots

BY LIZ WOLF · JANUARY 25, 2016

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As companies squeeze more employees into less office space – in an effort to increase efficiency and productivity — landlords are facing a dilemma: How can they accommodate increased parking demands? And, what happens if a space becomes unleaseable because of inadequate parking?

This quandary is especially true in suburban markets not served well by mass transit where employees are dependent on cars. These buildings' parking lots are becoming clogged, and landlords are looking for creative solutions.

## What's driving the space reduction?

In addition to cost savings, today's employee work habits are spurring the downsizing of office space. Collaborative, flexible workspaces are replacing big, private offices and fancy conference rooms.

"It's been an easy transition because, just as companies are trying to get more efficient and save money, millennials are more open to the idea of less hierarchy in real estate," Christian Beaudoin, director of corporate research for JLL in Chicago, told Hightower in an interview. "So those two trends have combined at the same time — companies trying to save money and millennials entering the workforce, who value compensation and freedom and flexibility more than they do a big office."

But just how much less office space are we talking? Pre-recession, 250 square feet per employee was the standard in office space. Today, that's been slashed to around 175 square feet or less, with projections estimating a drop to an average of 151 square feet per employee by 2017.

That's a significant reduction in space, and Beaudoin said that such density takes a toll on office buildings that were not designed to handle these increased demands. It not only impacts parking, but also building's elevators, restrooms and utilities.

## What can landlords do?

Before the trend of shrinking office space, a parking ratio of four stalls per 1,000 square feet was sufficient for most tenant parking space requirements. However, buildings today may need six or even seven parking spots per 1,000 square feet to accommodate the more packed offices.

To manage this greater density, landlords are exploring several options including:

**Build more spaces**

Some building owners are accommodating needs by building parking decks on top of surface parking lots. Of course, that's not cheap - it could cost around \$100 per-square-foot to build that deck.

If building parking decks aren't feasible, landlords are also exploring the use of adjacent lots. In one Chicago suburb example, Principal Real Estate Investors demolished 68,000 square feet of warehouse space of a nearly 200,000-square-foot building to create more parking for tenant CVS Caremark Corp.

**Shuttle tenants**

"Owners are experimenting with the idea of shuttles," Beaudoin said. "If there's an off-site parking lot like at a shopping mall or a nearby stadium, they can shuttle people in with a shuttle bus. Also, owners are looking at encouraging the use of public transportation, at least as close as they can get to the site and then bussing from there to the actual building." Carpooling and biking are also encouraged, and many office buildings have bike racks and locker rooms/showers if they're near a trail system.

These ideas may work well for millennials, who drive less and own fewer cars than previous generations. They prefer to bike, car-share, walk and use public transportation. According to the Department of Transportation and American Automobile Association, miles traveled by car for people 34 or younger dropped 23 percent and the percentage of high school seniors with driver's licenses dropped 73 percent between 1996 and 2010.

**Acquire new assets with better parking**

As new office development is starting up again in some markets, developers are paying close attention to parking ratios. "Markets like Phoenix are seeing new suburban office development, and they're building parking spaces of six spots per 1,000 square feet," Beaudoin said. Landlords may look to acquire these assets to mitigate future challenges.

## Looking ahead

Further down the road, the importance of on-site parking might be less significant. Driverless cars could have a huge impact on office parking lots. Although these cars are still being tested, it's believed that they will be available for average consumers to purchase in the next decade.

With self-driving vehicles, people won't have to follow current parking routines. For example, rather than park at the office, they could park at a distant, centralized lot and call for the car when they're ready to leave. This trend could eliminate parking lots as we know them today.

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**ABOUT**  
**Liz Wolf**

*Liz Wolf is a Twin Cities-based freelance writer with 30 years of business and commercial real estate reporting experience. She previously served as editor of the Minnesota Real Estate Journal.*

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Blog - Latest News Home / AtmosAir in The News / More Employees are being squeezed in the workplace

## More Employees are being squeezed in the workplace

November 1, 2014 / in AtmosAir in The News / by Administer

IN THE POSTRECESSION office environment, “creative” and “collaborative” workspaces are the new mode. Companies today—particularly in knowledge-based industries, technology, and life sciences—want open floor plans with minimally obstructed floorplates and much higher worker density. Office densification is an evolving trend with significant opportunities and challenges.

What do local zoning codes have to say on this topic?

### Zoning falls behind

“I’m just not hearing planners talk about this as an issue,” says Kirk Bishop, vice president at Duncan Associates’ Chicago office. “I hear about office densification and I’ve heard about some issues, but I’ve not seen any rush to change regulations yet.

" Both startup companies and major companies are increasing the number of workers in office environments. Traditionally, zoning regulations have based office parking, traffic, utility demand, and development standards on populations of three to four workers per thousand square feet of usable floor area. Today companies are pushing to double that number.

Opportunities include more economic development; challenges include infrastructure impacts. Impacts on parking, utilities and site amenities are being driven by commercial real estate brokers, interior architects, and mechanical engineers. Few planning departments look at the impacts of more workers in the same square footage. Zoning standards tend to be driven by building size, not the number of people working in the building.

"This is a trend that started before the recession, but is taking off," explains Dean Bellas, president of Urban Analytics, Inc., and adjunct professor of real estate at Catholic University's School of Architecture and Planning in Washington, D.C. "In 2005 in the (D.C.) area, the typical urban office allocated about 200 square feet per worker. By 2017, that's expected to drop to less than 180 square feet."

Brokers are already seeing that trend in markets in the West, Southwest, and Southeast. With properties capable of holding up to eight workers per thousand square feet, listings tout parking ratios of six to seven spaces per thousand square feet for Class A office space as compared to zoning regulations

requiring three or four spaces per thousand square feet.

### **What tenants ask for?**

"Tenants are demanding the kinds of amenities planners have sought all these years," says Michael Starling, director of economic development for Dunwoody, Georgia. "We're a former edge city (of Atlanta) now looking at urbanization. Tenants are demanding, and developers are building, amenities like on-site parks, restaurants, a walkable environment, and access to public transit. These are features planners have called out as necessary for quality communities. They are now minimum standards to attract quality tenants.

" Starling says that even older office buildings are upgrading lobbies, adding green space, and providing free shuttles to Metropolitan Atlanta Rapid Transit Authority stations. "State Farm paid a premium to build its new one-million-square-foot regional corporate headquarters on top of a MARTA station," he adds. "We're seeing owners building parks or locating near city parks, trails, and developing amenities such as gathering spaces, fitness centers, and places to eat.

" State Farm Insurance, based in Bloomington, Illinois, takes the same tack in Dallas and in Tempe, Arizona. Its two-million-square-foot Arizona campus sits on the shore of a large urban lake with an extensive trail network.

"We're building a 40,000-square-foot plaza with seating, WiFi, and space for an event venue,"

explains Richard Drinkwater, principal and senior architectural designer with DAVIS, the architectural firm charged with designing Marina Heights, State Farm's new western regional headquarters. "We're building restaurants, a fitness center, training rooms, and open floorplates," he says. "There is a lot of bike parking and public space."

Tempe plans to add a station for its downtown trolley to connect the 6,000 State Farm workers at Marina Heights to Metro, the Phoenix area light-rail system. Dunwoody has several projects that include fast-casual restaurants on pads in the office campus parking areas—similar to traditional retail center design. Dave Seeger, a broker with Jones Lang LaSalle in Phoenix, says he's heard of that concept in Minneapolis and in urbanizing suburbs.

Office developers are responding to tenant demands. "We're building Mach One (an office campus) dedicated to a happy and healthy employee," reports Sven Tustin, vice president of development and investment for Trammell Crow Company's Arizona business unit. That Chandler, Arizona, office campus is designed from the ground-up for collaborative and creative workspaces. "We've designed the building to handle 6.5 workers per 1,000 square feet. To make this work, we have amenities to help tenants attract and retain employees," Tustin adds.

Mach One is located in a suburban market with little access to public transit, but easy access to freeways. About half the spaces in the surface parking area will be covered—a coveted amenity in a desert city.

"With that much parking, it's a long walk from

parking spaces to the office,” says Tustin. “We’re building tree-lined and shaded walkways through the parking lot. Other amenities include collaborative space in the lobby and outdoor green areas, all with Wi-Fi access.”

“Tenants are really focused on the quality environment of a building and its setting,” explains Katie Ekstrom, a vice president in the Austin, Texas, office of CBRE, a national commercial real estate brokerage. “We have one new building going up, and two historic buildings being renovated,” she says.

“Tenants want lots of light, so the few hard-wall offices are on the interior,” Ekstrom notes. She talks about one tenant who installed all-glass dividers in the office so that natural light can reach all workspaces and every employee has a window view.

In addition to natural light, new-economy tenants want high ceilings and offices conducive to workgroups and Millennial working styles. One thing Millennials do not want is long commutes, obliging companies to look for work sites that combine housing and urban living—the heart of transit-oriented development. This is a major opportunity for central cities’ older office buildings.

### **Retrofitting old offices**

While the market is driving this change in new buildings, most regulations don’t really apply. This creates challenges when an owner wants to convert older offices to meet newer tenants’ needs.

Companies’ second highest operating cost, according to Bellas, is the office lease. “Many companies are

increasing density in order to decrease the amount of space they need to rent," he explains. "With newer buildings designed to accommodate those needs, older buildings are struggling to compete."

During the recession, market rents dropped to levels where companies in Class B and Class C office space could move up to Class A or Class B space for the same or even lower rents. Vacancy rates in older buildings skyrocketed.

The evolving Western trend is for older office buildings to be "demolished-to-shell" and redeveloped into Class A, collaborative workspaces. The campuses in outlying and suburban areas, usually with multiple buildings, are short on transit access and parking. Owners are literally demolishing existing buildings to provide surface parking for the structures that remain.

"It's more cost effective to demolish a Class C building, put in surface parking, and upgrade the remaining buildings to a higher lease value," says Tim Olson, a senior vice president with JLL in San Diego. "Accommodating six or more workers per thousand square feet increases the building value and generates an effective return on investment."

Applying "lipstick" to upgrade a building isn't enough. Olson says that Scripps Plaza, an office complex on Interstate 15 about 16 miles north of downtown San Diego, struck a positive note with tenants even before construction started. "We took a building that was nearly empty and have rented almost a third of it," he says.

The reason, he says, is amenities. "Renovation alone will not make a building attractive to today's tenants," Olson says. "We are building a 4,000-square-foot collaborative area with Wi-Fi and a lobby cafe. We've added a shower for bike commuters, locker room, and fitness center. We opened ceilings and created open floor flexibility."

In Phoenix, JLL is the broker on a similar project, Tempe 10/60. The former three-building, 90,000-square-foot Class C complex is being repurposed into a two-building 70,000-square-foot Class A campus. The recovered ground will become parking. "Parking garages cost \$20,000 per space; it was more cost effective to tear down the smallest building" to make space for more surface lots than it was to build a parking deck, says Seeger, who is a managing director at JLL, Phoenix.

### **Zoning and redeveloping offices**

"We haven't had any issues with (the city of Austin)," says Ekstrom. "The bigger challenges come when our engineers and consultants start looking at what it's going to take to convert an existing building."

"Renovating and redeveloping offices has both upsides and downsides, says Bellas. "There are the benefits to the community in terms of economic development, and there are the costs imposed on community by the increased occupancy," he adds.

"Perimeter Center is different than the rest of our city," says Dunwoody community development director Steve Foote, aicp. "We saw the need to address development and redevelopment

specifically for this area." The city, incorporated in 2008, appears to be one of the first to craft a zoning code covering office densification.

Rebecca Keefer, aicp, the city planner, says that Dunwoody plans to create incentives for height or density. "Considering changes in worker density is an issue that could be on the table," she says. If so, it appears to be one of the first communities to take that step—with Bishop and Duncan Associates as the consultant.

Existing buildings present special problems when redeveloped for higher densities. "Who thinks about elevator traffic?" asks CBRE's Ekstrom. "In one of our historic buildings, we're allowing high density on lower floors where stairs are an option. You can't add an elevator."

"Sometimes people don't realize the air quality problems coming from adding more people into a smaller space," says Tony Abate, vice president of operations for Atmosair, in Fairfield, Connecticut. "Mechanical systems suddenly become inadequate when worker population increases from the original rating," he says. "Go to an open ceiling and the (heating, ventilating, and air conditioning) system also has 20 percent to 30 percent more air to move in addition to what it has to exchange for the workers."

When the HVAC is "overpopulated," interior humidity builds up, affecting electrical equipment, transporting microbes, and creating an environment for mold.

"We've seen mold and spore problems in (high-density) offices where tenants have had to relocate," says Justin S. Dixon, president of Snyder Environmental, in Little Rock, Arkansas. "Older buildings may not physically be able to accommodate increased duct sizes. Roofs may not have been engineered to hold up a larger HVAC system."

### **When it's hot**

Danielle Casey, economic development director for Scottsdale, Arizona, knows about the impacts of technology company migration into the city's downtown and riverfront area. A failed retail plaza, the Galleria, has become the hot tech spot partly because it adjoins Scottsdale's famous nightclub district. Microsoft and tech startups are among the firms gobbling up the available space.

"We've had a number of high-tech companies move into the downtown area," she says. "They're packing six, even nine employees per thousand square feet. It's completely revitalized the area, but taken every parking space. I've talked with executives, and they say the hip, urban atmosphere is what attracts and keeps employees." Parking is just one problem. Peak hour traffic jams are another. Scottsdale has moved to articulated buses and a faster rush hour schedule to accommodate workers. There is much, however, to keep the employees downtown, with restaurants, clubs, and entertainment surrounding the workspace. Several developers are building upscale residential communities within walking distance of the job center.

The old Galleria is a successful conversion to creative work spaces that the tenants demanded. Some owners have had bigger challenges with old building infrastructure or undesirable locations.

Not all conversions will work. "On one project," says Dixon, "we determined the building infrastructure could not physically be upgraded to accommodate the owners' plans. They had to walk away.

"As a result, some structures come down. "We've had some office buildings and shopping centers—and not that old a facility— demolished for new office buildings," reports Foote, the community development director in Dunwoody. Olson sees the same in San Diego.

### **Driving economic growth**

Double the number of people working in an area and there's an immediate boost to the local economy. Bellas says that in addition to the multiplier of the construction and office furniture purchases, "you'll need more restaurants, more service businesses, and more retail to handle the increased employee population.

"We have nearly single digit vacancy rates for retail—and I don't think there is any open restaurant space in Perimeter Center," says Starling in Dunwoody, referring to a mixed office and retail campus that was built in unincorporated DeKalb County before the town formed. "In addition, it has really given a boost to the number of people who are using MARTA and other public transit. At the same

time, it's aggravated an already challenging traffic situation. We're struggling for a solution.

" At some stage in the evolution, the demand for infrastructure capacity is going to outweigh the ability to deliver the services in a timely way. Dunwoody has taken steps to act before that point is reached. Will other agencies follow its lead?

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**VILLAGE OF DOWNERS GROVE  
REPORT FOR THE PLAN COMMISSION  
AUGUST 1, 2016 AGENDA**

<b>SUBJECT:</b>	<b>TYPE:</b>	<b>SUBMITTED BY:</b>
15-PLC-0008 1401 – 1445 Ogden Avenue	Planned Unit Development, Rezoning, Special Use, and Alley Vacation	Swati Pandey Planner

**REQUEST**

The petitioner is requesting approval for a Planned Unit Development, a Rezoning from existing B-3, General Services and Highway Business to B-3/PUD, General Services and Highway Business/Planned Unit Development, a Special Use, and a vacation of the 20-foot wide improved alley to the south of the property to redevelop an existing fueling station and carwash at 1401-1445 Ogden Avenue.

**NOTICE**

The application has been filed in conformance with applicable procedural and public notice requirements.

**GENERAL INFORMATION**

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**OWNER & APPLICANT:** Delta Sonic Car Wash Systems, Inc.  
570 Delaware Avenue  
Buffalo, NY 14202

**PROPERTY INFORMATION**

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**EXISTING ZONING:** B-3, General Services and Highway Business  
**EXISTING LAND USE:** Fueling Station and Car Wash  
**PROPERTY SIZE:** 119,946 sq ft (2.75 acres)  
**PINS:** 09-06-405-001, -007, -008, -009, -010, -026

**SURROUNDING ZONING AND LAND USES**

	<b>ZONING</b>	<b>FUTURE LAND USE</b>
<b>NORTH:</b>	B-3, General Services and Highway Business	Corridor Commercial
<b>SOUTH:</b>	R-1, Residential Detached House 1	Single Family Residential
<b>EAST:</b>	B-3, General Services and Highway Business	Corridor Commercial
<b>WEST:</b>	B-3, General Services and Highway Business	Corridor Commercial

**ANALYSIS**

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**SUBMITTALS**

This report is based on the following documents, which are on file with the Department of Community Development:

1. Project Narrative

2. Plat of Survey
3. Architectural Plans
4. Engineering Plans
5. Landscape Plan
6. Photometric Plan
7. Traffic Impact Study
8. Plat of Consolidation
9. Plat of Vacation
10. Summary of Neighborhood Meeting

### **PROJECT DESCRIPTION**

The applicant is proposing to redevelop the existing fueling station and car wash facility on Ogden Avenue between Seeley and Oakwood Avenues. Currently, the subject property is improved with a one-story commercial building with a V-shaped fuel station canopy in the center of the site, a maintenance and repair shop with an attached office building on the east side of the property and a former Burger King building on the west. The proposal is to redesign, expand and improve the facility and operations by demolishing the former Burger King building (approximately 25,000 square feet of property at the intersection of Ogden Avenue and Seeley Avenue) and vacating the alley at the south side of the property. The existing single story Delta Sonic building will remain. The maintenance and repair shop and the office building shall also remain as existing on the east side of the property. New fuel canopies are proposed and the car wash operations are expanded on the west side of the property. A new eleven space vacuum area is proposed immediately to the west of the existing building. A new trash enclosure will be built to the rear of the building and the existing detention basin will be moved underground so as to improve the existing stormwater infrastructure and onsite circulation.

The property is zoned B-3, General Services and Business Highway District and the current services offered by Delta Sonic include a car wash, auto detailing, gas station, convenience store and office use. Based on the variety of uses, the applicant is applying for a Planned Unit Development (PUD) and the associated rezoning to accommodate this development which would be difficult to carry out under conventional zoning standards. The petitioner is also applying for a Special Use. A 'fueling station' and 'personal vehicle repair and maintenance' is an allowable Special Use in the B-3 zoning district per Section 5.010 of the Zoning Ordinance. The vacation of a 20-foot wide alley to the south has also been requested to facilitate the redevelopment of this site.

The site contains five curb cuts on Ogden Avenue, including two on the Burger King property, and two off of Oakwood Avenue. The petitioner has proposed to improve on-site circulation by removing and consolidating curb cuts, as shown below:

- Two full access will be provided onto Ogden Avenue (a reduction of two)
- One full access is provided onto Oakwood Avenue (a reduction of one)

IDOT has reviewed the proposed curb-cut locations and has approved the proposed layout and locations. Pedestrian access will be provided from Ogden Avenue to the building main entrance with distinct material as required per the Zoning Ordinance.

The existing one-story commercial building will have new EIFS cornice installed and brick repairs undertaken based on the removal of the existing canopy. The new pre-wash building (2 bay prep hut) façade shall have the same materials and have a continuous fascia panel attached to the existing car wash. The fuel canopies shall be redesigned and the old fuel canopies will be removed. Four gas islands with 16 pumps shall be removed and replaced with five smaller islands and 10 gas pumps. The pumps shall be oriented in the same direction under a single rectangular canopy which will have a similar fascia panel with brick clad

columns. The detail, maintenance and office building shall remain as existing.

The petitioner is proposing 52 parking spaces including three handicapped spaces. Parking areas are distributed throughout the site to service each building. New parking areas have been proposed north of the main building entrance, south of the office and detail shop, and on the southern edge of the property.

The petitioner is proposing new landscaping on the property, in conformance with the Village requirements. A significant amount of landscaping is proposed on the west and south perimeter of the property. Foundation landscaping is proposed in front of the building to soften the impact of the wide building and concrete area around it. A fence is proposed on the south and west side of the property. Any new signage proposed for the development shall comply with the Sign Code requirements and a separate sign permit will be required at the time of building permit application.

A 598 feet by 20 feet public alley running east – west to the south of the property is requested to be vacated. The eastern side of the alley is currently paved and is being used as parking. The west half of the alley is open green space. The vacation has been requested by the petitioner to allow greater flexibility in site design and allow depth in the property to accommodate parking setbacks. A part of the parking lot directly behind the building will be improved and encroach five feet into the alley. Per the Village's Right-of-Way Vacation Policy (Resolution #2003-58), staff contacted the public agencies and determined that the utility providers and the Village do not have an objection to the vacation of the right-of-way as long as a public drainage, utility and access easement is retained along the entire width and length of the alley. The required easement has been provided as noted on the Plat of Vacation.

The subject property contains multiple lots of record which will require the applicant to administratively consolidate the lots if the project is approved.

#### **COMPLIANCE WITH THE COMPREHENSIVE PLAN**

The Comprehensive Plan identifies the subject site as the *Ogden Avenue Central - Key Focus Areas*. The Comprehensive Plan notes this area should be redeveloped with attention to pedestrian circulation, reducing the number of curb-cuts, cross-access between lots, and overall improved appearance reflective of the larger Downers Grove community. The proposed development:

- Removes three curb-cuts and improves access onto Ogden Avenue
- Improves connectivity by installing a sidewalk along Ogden Avenue and pedestrian access from Ogden Avenue to the main building
- Consolidates multiple lots including the vacant 'Burger King' to the west to improve onsite operations and eliminate stacking on Ogden Avenue
- Eliminates the existing detention area at the southwest corner of the detail shop to allow better site design and onsite circulation
- Provides enhanced landscaping and screening in order to provide a buffer to the residential areas to the south and a more attractive image from Ogden Avenue.
- The Village of Downers Grove worked with IDOT to reduce curb cuts along Ogden and Oakwood Avenue as recommended by the Comprehensive Plan.

The property is included within the corridor commercial area in the comprehensive plan. The plan recommends the corridor commercial area include a blend of neighborhood-oriented commercial retail, offices, smaller regional retail and service uses. Delta Sonic provides these types of services. The proposed development meets the goals of Comprehensive Plan.

**COMPLIANCE WITH ZONING ORDINANCE**

The property is zoned B-3, General Services and Highway Business. The bulk requirements of the proposed development in the B-3 zoning district are summarized in the following table:

**Zoning Requirements**

<b>1401-1445 Ogden Avenue</b>	<b>Required</b>	<b>Proposed</b>
Building North Setback (Street Yard)	75 ft from Ogden Avenue centerline	118 ft
Building South Setback (Rear Yard)	22 ft	44 ft (new) 38 ft (existing)
Building West Setback (Street Yard)	25 ft	67 ft
Building East Setback (Street Yard)	25 ft	N/A (as existing)
Canopy North Setback	50 ft from Ogden Avenue centerline	66 ft
Parking North Setback	50 ft from Ogden Avenue centerline	124 ft
South Setback Parking (Rear Yard)	20 ft	14.31 ft
Floor Area Ratio	0.75 max	0.21
Building Height	60 ft max	22 ft
Open Space (10% / 50%)	10,811 sq ft / 5,405 sq ft	12,403 sq ft / 8,902 sq ft
Parking	35 spaces	52 spaces
Stacking (Fueling, Carwash, Detail)	36	52

The proposed development is compliant with the vast majority of the bulk regulations in the B-3 zoning district, however, the vehicle stacking requirements for the fuel pumps and detail building does not comply with the minimum requirements of the Zoning Ordinance, but it is a significant improvement and brings a non-conforming condition closer into conformity. Also, the rear parallel parking spaces extend into the 20 feet required setback; however, these parking spaces are required to meet the required parking for the property and will be used primarily for employee parking. The applicant is also providing landscaping and a fence along the south property line to screen the parking. The drive through lane for the carwash is six feet from the west property line where 25 feet is required. This stacking lane is necessary to allow a double row of vehicles to stack on-site versus extending the stacking lane to Ogden Avenue.

The applicant is applying for a Planned Unit Development in order to redevelop the property with a variety of uses and structures on a single property that would not be allowed under general zoning regulations because of the existing conditions on the site. In order for the applicant to apply for a Planned Unit Development, the applicant's proposal must meet one or more objectives identified in Zoning Ordinance Section 4.030.A.2. These objectives work to balance the needs of the applicant and the additional public benefits gained from permitting the Planned Unit Development. The increased safety, onsite circulation, connectivity, keeping operations restricted to the subject site, reducing curb-cuts, and improved landscaping requirement will result in several public benefits that meet the following identified Planned Unit Development Objectives:

- Implementation of and consistency with the comprehensive plan and other relevant plans and policies
- Efficient and economical provision of public facilities and services
- A coordinated transportation system that includes an inter-connected hierarchy of facilities for motorized and non-motorized travel

The proposed development meets the provisions of a Planned Unit Development as it proposes improvements to the property which would result in public benefits that are at least commensurate with the degree of development flexibility provided. The project proposes significant improvements such as removing three curb cuts on Ogden Avenue and one on Oakwood Avenue; adding stacking lanes for carwash operations thereby reducing onsite congestion; upgrading fuel storage capacity by reducing the number of tanks onsite which also reduces the number of trips generated by fuel tankers in the area; consolidating multiple lots to allow and improved circulation onsite; removing the V-shaped fuel canopy and replacing it with an improved design and orientation of the fuel pumps; redesigning the parking area in the southeast corner of the property; relocation of the existing detention area on the property to underground storage; reducing the number of fuel pumps from 16 to 10; vacating the alley to allow for improved landscape buffer adjacent to the residential area and providing additional employee parking; and improved access through pedestrian walkways and sidewalk along Ogden Avenue.

With all the above analysis considered, the applicant's proposal is consistent with the Village's Zoning Ordinance.

#### **ENGINEERING/PUBLIC IMPROVEMENTS**

Based on the existing and proposed impervious area on the site, new stormwater detention is not required for the proposed development. The existing detention on-site is being relocated underground allowing improved circulation for both pedestrians, employees and vehicles. Post Construction Best Management Practices are not required for this property, however, the required stormwater easements will be established as part of this petition. The project will meet all provisions of the Stormwater and Floodplain Ordinance.

Additional public improvements include the reduction of curb-cuts onto both Ogden Avenue and Oakwood Avenue and the installation of sidewalk along Ogden Avenue.

#### **TRAFFIC**

A traffic and parking impact study for the proposed development was completed by the petitioner in March, 2015 and updated in June, 2016. The primary access to the property will be through two new access points on Ogden Avenue with a secondary access point onto Oakwood Avenue. All deliveries will be able to enter and exit the site via the two Ogden Avenue curb cuts.

The proposal significantly improves traffic flow and internal circulation. Currently, there is a two lane pay station that allows practically no stacking between the pay stations and Ogden Avenue. The two lane pay station merges into a single lane going into the car wash. The lack of stacking space and the immediate merging into a single lane of car wash traffic leads to extensive stacking on the site and towards Ogden Avenue.

The proposal is to provide three pay station lanes merging into a two lane drive into the new dual lane prep hut bay which leads to the single lane car wash. This layout accommodates 19 vehicles between the pay station and the carwash entrance. Additionally, because the pay stations have been relocated further west there is space to stack 15 vehicles between the pay stations and the nearest Ogden Avenue curb cut.

The increased stacking, reduced curb-cuts and improvement in on-site circulation and traffic patterns will positively impact the traffic in the surrounding area and staff concurs with the findings of the report.

#### **NEIGHBORHOOD COMMENT**

Notice was provided to all property owners 250 feet or less from the property in addition to posting public hearing notice signs and publishing the legal notice in the *Downers Grove Suburban Life*. There have been no public comments received by Staff.

As required by the Zoning Ordinance, the petitioner held a neighborhood meeting on June 23, 2016. Two residents attended and discussed setback, screening, landscaping and lighting issues as a result. Staff made two recommendations to increase perimeter landscaping and enhanced screening as conditions of approval. A summary of the meeting is attached.

#### **FINDINGS OF FACT**

The petitioner is requesting a Planned Unit Development, Rezoning, a Special Use and an alley vacation to redevelop a fueling station and carwash at 1401 – 1445 Ogden Avenue. Staff finds that the proposal meets the standards for granting a Planned Unit Development, Rezoning, a Special Use and an alley vacation as outlined below:

##### ***Section 28.12.040.C.6 Review and Approval Criteria***

*The decision to amend the zoning map to approve a PUD development plan and to establish a PUD overlay district are matters of legislative discretion that are not controlled by any single standard. In making recommendations and decisions regarding approval of planned unit developments, review and decision-making bodies must consider at least the following factors:*

***a. The zoning map amendment review and approval criteria of Sec. 12.030.I.***

See the analysis of rezoning review and approval criteria below. This standard has been met.

***b. Whether the proposed PUD development plan and map amendment would be consistent with the comprehensive plan and any other adopted plans for the subject area.***

The proposed project is consistent with the Comprehensive Plan. The Plan identifies this area within the *Ogden Avenue Central - Key Focus Area*. A PUD overlay shall provide the necessary tools to redevelop the property with multiple buildings and uses with creative and modern development to address the key concepts of improved circulation, access, screening, safety, and commercial expansion per the Comprehensive Plan.

***c. Whether PUD development plan complies with the PUD overlay district provisions of Sec. 4.030.***

The proposed project meets several of the PUD overlay district provisions and objectives as found in Section 4.030 of the Zoning Ordinance. One of the objectives of a PUD is to provide flexible and creative solutions to allow change based on changing market conditions. The proposed development meets this as the demolition of the vacant restaurant will be consolidated with the existing car wash property to improve the services of the existing business while improving site design. This project will advance the objective to enhance the existing transportation system with an inter-connected hierarchy of facilities for both motorized and non-motorized travel by improving off-street parking, stacking, and installing a public sidewalk. This standard has been met.

***d. Whether the proposed development will result in public benefits that are greater than or at least equal to those that would have resulted from development under conventional zoning regulations.***

The proposal will result in redevelopment of an existing business while retaining and expanding the business in the Village of Downers Grove. The consolidation of the smaller parcels into one large

commercial property is a goal for Ogden Avenue corridor development in the Comprehensive Plan. The improved facility as discussed above shall provide numerous public benefits that would not be possible under the conventional zoning regulation. This standard has been met.

- e. Whether appropriate terms and conditions have been imposed on the approval to protect the interests of surrounding property owners and residents, existing and future residents of the PUD and the general public.*

The petitioner has worked with Village staff to optimize the redevelopment potential of the site as envisioned by the Comprehensive Plan, including elimination of three Ogden Avenue curb cuts, increased additional landscaping and buffering, increased pedestrian access and safety, and more efficient on-site circulation. These elements of the site design protect the interests of the surrounding property owners, businesses, residents and the general public. Delta Sonic will work with the Village to establish appropriate terms and conditions in the required Village documents. This project will advance many goals and objective laid out in several adopted documents and the conditions below will ensure that those goals and objectives are advanced. This standard has been met.

***Section 12.030.I. Zoning Map Amendment Review and Approval Criteria***

The decision to amend the zoning map is a matter of legislative discretion that is not controlled by any single standard. In making recommendations and decisions about zoning map amendments, review and decision-making bodies must consider at least the following factors:

- 1. The existing use and zoning of nearby property.***

The current use of the subject property is an existing auto-oriented use which will remain unchanged and the conversion of a vacant restaurant building. The surrounding properties to the north, east and west are zoned B-3, General Services and Business Highway District and consist of a variety of commercial uses. The properties to the south are zoned R-1, Residential Detached House 1 and are improved with single family homes. The required building setbacks have been maintained with the adjacent properties. The proposed use and development is appropriate as compared to the surrounding zoning and uses. This standard has been met.

- 2. The extent to which the particular zoning restrictions affect property values.***

The PUD overlay and the proposed project will protect the character and integrity of adjacent properties by requiring subsequent approvals for major changes, which will assist in maintaining property values. Also, the subject property will be improved through site design modifications to improve current site conditions and bring the property closer to compliance and decrease non-conformity. This project will include PUD overlay restrictions which will not negatively affect property values but should protect property values. This standard has been met.

- 3. The extent to which any diminution in property value is offset by an increase in the public health, safety and welfare.***

The proposed rezoning will not impact property values or the public health, safety and welfare of the community or neighborhood. The property will be redeveloped with numerous improvements, features and public amenities to increase the public health, safety and welfare. This standard has been met.

- 4. The suitability of the subject property for the zoned purposes.***

The property is zoned B-3, General Services and Business Highway District. The auto-oriented use is existing and is an allowable Special Use in the B-3 district. Additionally, the proposed PUD overlay will enhance the suitability of the proposed use for the subject property. The PUD overlay will allow multiple buildings on a single property, improved internal circulation, and attractive and

high quality development overall. This standard has been met.

**5. *The length of time that the subject property has been vacant as zoned, considering the context of land development in the vicinity.***

The property is not vacant. The rezoning of the property for the PUD overlay will only enhance the subject site, provide numerous benefits to the public and allow for zoning flexibility to be offered in order for several property enhancements to take place. This standard has been met.

**6. *The value to the community of the proposed use.***

The property is identified within the *Key Focus Areas – Ogden Avenue Central* in the Comprehensive Plan. The proposal is to improve the facility through the enhancement of landscaping, pedestrian access and connectivity, elimination of three Ogden Avenue curb-cuts, additional stacking spaces for the carwash operation and re-configuration of on-site circulation. Rezoning the property for the PUD overlay will allow creative options for the applicant to incorporate the key concepts and objectives identified in the Comprehensive Plan for the general benefit of the public. This standard has been met.

**7. *The comprehensive plan.***

The proposed PUD overlay and the proposed project are consistent with the Comprehensive Plan. The proposal will develop the property within the *Key Focus Area – Ogden Avenue Central* as desired in the Comprehensive Plan. This standard has been met.

***Section 28.12.050.H Approval Criteria***

*No special use may be recommended for approval or approved unless the respective review or decision-making body determines that the proposed special use is constituent with and in substantial compliance with all Village Council policies and plans and that the applicant has presented evidence to support each of the following conclusions:*

**1. *That the proposed use is expressly authorized as a Special Use in the district in which it is to be located;***

The property is located in the B-3, General Service and Highway Business zoning district. Under Section 5.010 of the Zoning Ordinance, a *Fueling station* and *Personal vehicle repair and maintenance* which includes a carwash is listed as an allowable Special Use in the B-3 zoning district. This standard has been met.

**2. *That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.***

The existing property is presently being used as a car wash, auto detailing, convenience store, gas station and offices. These auto-oriented uses serve the needs of the local residents and the larger region. Its location on Ogden Avenue is appropriate and the uses provided are in the interest of public convenience. The proposed improvements to the property will enhance the existing facility; improving both safety on the adjoining roadway system and internal traffic circulation. The proposed improvements will increase the stacking for the existing car wash, which will result in cars having room to stack on-site instead of on Ogden Avenue when the property is busy. Additionally, the removal and replacement of the gas islands and installation of new UFTs will provide for safer fuel storage. The larger UFTs will also result in the need for fewer fuel deliveries. Additionally, removing the stormwater basin and placing those facilities underground will open more space on site for on-site circulation and improve traffic flow by making the property less congested. Finally, the improvement will allow for a new service to be provided, self-service vacuums, as well as allow for improved cleaning of vehicles by the installation of the proposed prep hut. All of these improvements will contribute to the general welfare of the community.

The property is identified as a property within the Key Focus Areas within the Comprehensive and the above mentioned project details address most key concepts that are desirable by the community for the improvement of the property. This standard has been met.

**3. *That the proposed use will not, in the particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.***

The proposed development will not be detrimental to the health, safety or general welfare of persons residing in or working in the vicinity and will not be injurious to property values or improvements in the vicinity. The proposed development will redevelop and expand an existing use with improved connectivity and combine a vacant/underutilized parcel to a larger commercial development which also a goal of the Comprehensive Plan for Ogden Avenue corridor. The improvements to the property promote safety and aesthetics which may help increase property values in the vicinity. This standard is met.

***Compliance with the Procedure to be followed in the Vacation of Streets, Alleys, and Public Rights-of-Way (Resolution #2003-58)***

The Village's alley vacation policy asks the following questions when it comes to determining if an alley can be vacated. These questions and staff's findings are listed below:

1. Is there written consent of at least two property owners who abut the proposed parcel to be vacated?
  - The two property owners immediately south of the alley have provided waiver letters in support of alley vacation. The letters are attached.
2. Whether the Parcel or portion thereof, is no longer necessary for public use and whether the public interest will be served by such vacation request.
  - A part of the alley is improved currently and a five foot encroachment is proposed due to the parallel parking spaces proposed on the rear side of the building. As noted above, staff contacted the utility companies and outside public agencies to determine the extent of public interest. Based on their replies, staff has determined the public interests are addressed by placing a public drainage, utility and access easement over the entire vacated alley.
3. Whether the Parcel or portion thereof, should be vacated and whether public utility easements and any ingress-egress easements are to be maintained.
  - A public drainage, utility and utility access easement will be retained over the entire alley length and width. As such, the petitioners will not be able to construct any permanent structure, other than a driveway or fence, within this easement.
4. The amount and type of compensation, if any, to be required as a condition to the effectiveness of the vacation of the parcel.
  - Staff requires the petitioners provide the Village with compensation for the alley to be vacated. The petitioner has provided an appraisal report for the alley prepared by Property Valuation Services on July, 11, 2015. Based on the appraisal report, the value of the alley is \$270,000. As such, the petitioner will be required to pay the Village a total of \$270,000 as compensation for the alley.

## RECOMMENDATIONS

The proposed Planned Unit Development, Rezoning, Special Use and alley vacation for a fueling station and carwash at 1401 - 1445 Ogden Avenue is consistent with the Comprehensive Plan, the Zoning Ordinance and surrounding zoning and land use classifications. Based on the findings listed above, staff recommends the Plan Commission recommend the Village Council **approve** the requested Planned Unit Development, Rezoning, Special Use and alley vacation as requested in case 15-PLC-0008 subject to the following conditions:

1. The Planned Unit Development, Rezoning, Special Use and alley vacation shall substantially conform to the staff report; engineering, architectural and landscape drawings prepared by Intech Consultants, Inc. dated January 28, 2015 and last revised on June 7, 2016, except as such plans may be modified to conform to the Village codes and ordinances.
2. The proposed fence along the south property line should be a maximum of eight feet in height per the Zoning Code. The fence along the west property line should be a maximum of four feet open design.
3. The applicant shall pay the Village \$270,000 prior to the execution of the alley vacation.
4. The fence along the west property line should be placed along the driveway curb and allow landscape materials to be planted along the streetside of the fence.
5. The building shall be equipped with an automatic suppression system and an automatic and manual fire alarm system.
6. The applicant shall maintain all sidewalks and plaza areas that are on and immediately adjacent to the subject property.
7. Stormwater and utility easement shall be granted over the detention area in the parking lot.
8. The applicant shall administratively consolidate all lots into one lot of record prior to issuing a building permit.
9. The applicant shall reduce light levels to security level no later than 30 minutes after the close of business.

Staff Report Approved By:



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Stanley J. Popovich, AICP  
Director of Community Development

SP; sp  
-att



0 40 80 Feet

1401 - 1445 Ogden Avenue - Location Map





## DELTA SONIC CARWASH SYSTEMS, INC

•570 Delaware Avenue, Buffalo, NY 14202•Phone (716)886-0931•Fax (716) 886-1026

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June 6, 2016

Department of Community Development  
Village of Downers Grove  
801 Burlington Avenue  
Downers Grove, IL 60515

**Re: Application of Delta Sonic Car Wash Systems, Inc.  
Petition for Plan Commission  
Requested Relief: Special Use/Planned Unit Development/Variances/Lot  
Consolidation/Right-of-Way Vacation  
Property: 1407-1415 Ogden Avenue**

Dear Sir or Madam:

Enclosed please find the application materials required for our Petition for Plan Commission which requests the certain relief as specified herein.

**The Subject Property:**

The subject property is the location of an existing Delta Sonic car wash, auto detailing, gas station, convenience store and office as shown on the survey prepared by Intech Consultants, Inc. ("Intech"), dated October 10, 2014, and which is included with the site plan materials submitted herewith. As demonstrated by the survey, the Delta Sonic currently maintains three driveways onto Ogden Avenue, as well as an access through an existing alley to Oakwood Avenue. The property is currently improved with four (4) gas islands with (16) sixteen pump stations along with eight (8) underground fuel tanks ("UFTs") which hold approximately 38,000 gallons of fuel,

On or about January 16, 2014, the vacant Burger King fast food restaurant, located to the west of the existing Delta Sonic, at the southeast corner of Ogden Avenue (US Route 34) and Seeley Avenue, was purchased with the intent of incorporating that property into the existing Delta Sonic to improve internal traffic flow and the services which can be offered at the property. The Burger King property currently maintains two driveways to Ogden Avenue.

## **The Proposed Project**

The project seeks to upgrade and improve the subject property. The proposed improvements include:

- The demolition of the vacant Burger King fast food restaurant and the closure of the two existing curb cuts which service that property as shown on the Removal Plan (Sheet 3 of 20) of the site plan materials prepared by Intech, revision date April 22, 2016;
- The construction of new car-wash stacking lanes, utilizing the Burger King property, so that the stacking for the car wash will be significantly improved from currently being able to stack fourteen (14) cars to the car wash as shown on the Existing Traffic Flow Plan (Sheet 18 of 20) to being able to stack thirty four (34) cars to the car wash prep hut as shown on Proposed Traffic Flow Plan (Sheet 19 of 20) of the enclosed site plan materials;
- The addition of a “prep hut” to the car wash, which will also be located on the Burger King property, to allow for the mechanical pre-wash of cars prior to entering the car wash as shown on the Geometric Plan (Sheet 4 of 20) of the site plan materials prepared by Intech, revision dated April 22, 2016. Specifications for the new prep hut can be found on the Elevations and Sections Plan (Drawing A-1.3 Rev 1 April 29, 2016) of the enclosed site plan materials;
- The addition of eleven (11) self-service vacuum stalls facing Ogden Avenue as shown on the Geometric Plan (Sheet 4 of 20) of the site plan materials prepared by Intech, revision dated April 22, 2016, and a canopy over the proposed vacuums. Specifications for the new vacuum canopy can be found on the Vacuum Canopy Plan (Drawing A-1.2) of the enclosed site plan materials;
- The removal of the eight (8) existing UFTs and associated piping and replacement of those tanks with two (2) new, state-of-the-art double-hulled fiberglass fuel storage tanks with a leak alarm system. The new UFTs are a significant upgrade to the single wall fiberglass tanks which currently exist. In addition, reducing the number of UFTs results in less piping to connect the tanks and therefore a safer situation underground. The two new fuel tanks will hold 50,000 gallons of fuel, with one tank being 30,000 gallons and the other being a split 20,000 gallon tank (12,000 gallon and 8,000 gallon compartments). Specifications of the new UFTs can be found on the Gasoline Details Plans (Drawings G2.0 and G2.1) of the enclosed site plan materials;
- The removal of the existing four (4) gas islands with sixteen (16) gas pumps, as shown on the Removal Plan (Sheet 3 of 20) of the site plan materials prepared by Intech, revision dated April 22, 2016, and the replacement of those islands with five (5) smaller gas islands with ten (10) gas pumps. The new configuration provides improved ADA access;

- The removal of an existing stormwater detention basin located on the eastern portion of the site, shown on the Removal Plan (Sheet 3 of 20) of the site plan materials prepared by Intech, revision dated April 22, 2016. The detention area will be placed underground and that area will be paved to improve access and internal circulation at the property;
- The closure of the center curb cut on Ogden Avenue with the remaining two Ogden Avenue curb cuts being full ingress/egress driveways;
- The provision of additional parking spaces for the convenience store;
- The creation of pedestrian access throughout the site;
- The closure of the northern driveway on Oakwood Avenue;
- The installation of new landscaping as shown on the proposed Landscape Plan (Sheet 12 of 20) of the enclosed site plan materials;
- The vacation of the existing alley located behind Delta Sonic, to the south, which is currently paved and being utilized for access to Oakwood Avenue; and
- The consolidation of the lots into one parcel.

### **Site Operations**

Delta Sonic has been a part of the Downers Grove business community and has provided services to the community's residents since 1989.

The existing site is approximately 83,783 square feet in size. With the acquisition of the Burger King property and the Alley, the entire site will be 119,946 square feet in size. This additional property will allow site operations to be significantly improved.

Delta Sonic does not anticipate altering its current business operations by expanding or decreasing any service. It is also anticipated that the number of customers will increase by approximately 10%. The total number of employees will be 16 during a normal operation shift and 24 during a peak operation shift. The hours of operation will remain the same:

- Monday through Saturday: 7:00am – 9:00pm (all services)
- Sunday: 8:00am – 7:00pm (car wash/detail shop)
- Sunday: 8:00am – 8:00pm (store/gas pumps).

The purpose of this renovation is to improve safety, improve on-site circulation, upgrade the site and to provide a better and higher level of customer service.

### **Plan Commission Application Requests:**

Pursuant to the March 10, 2016 letter issued by the Village of Downers Grove Community Development Department General Comment #3, the following are the specific Plan Commission Application requests for the project:

- i. A Planned Unit Development.
- ii. A Special Use for Personal Vehicle Repair and Maintenance (for the detail shop and car wash).
- iii. A Special Use for a Fuelling Station.
- iv. A Variance to allow for detached accessory structures in the street yard. VODG Section 28.6.010.A.7.a. The Geometric Plan (sheet 4 of 20) shows the setback dimensions for all accessory structures. A Variance is required for the proposed automated express lane island for the car wash which is 64ft from the center line of Ogden Avenue.
- v. A Variance to reduce the required street yard setback for a detached accessory structure. VODG Section 28.6.010.A.7.b. The Geometric Plan (sheet 4 of 20) shows the setback dimensions for all accessory structures. A Variance is required for the setback for the proposed automated express lane island for the car wash which is 64ft from the center line of Ogden Avenue.
- vi. A Variance to reduce the required setback for the Drive-Through Stacking Lane. VODG Section 28.7.130.E. The Geometric Plan (Sheet 4 of 20) shows the setback dimensions for all proposed drive-through stacking lanes. Variances are required for:
  - The setback of the proposed car wash drive-through stacking lane to the south boundary abutting residential which is 56.16 ft.
  - The setback of the proposed car wash drive-through stacking lane to the west boundary (Seeley Avenue R.O.W.) which is 6 ft.
  - The setback of the proposed car wash drive-through stacking lane to the north boundary (Ogden Avenue R.O.W.) which is 22 ft.
  - The setback of the proposed detail shop drive-through stacking lane to the south boundary abutting residential which is 23.23 ft. Note that this is an existing stacking lane on site.
- vii. A Variance to reduce the required setback for off-street parking spaces (for both the street yard and the rear yard). VODG Section 28.7.070. The Geometric Plan (Sheet 4 of 20) shows the setback dimensions for all proposed off-street parking. Variances are required for:
  - The setback for proposed parking at the south boundary abutting residential which is 14.31 ft. The setback for existing parking at the south boundary abutting residential is 4.73 ft.
  - The setback for the existing parking at the east boundary (Oakwood Avenue R.O.W.) which is 2.01ft. Note that this was an existing parking lot and Delta Sonic is improving the existing situation by removing parking spaces to add landscape areas to the entrance.
  - The setback for proposed parking at the west boundary (Seeley Avenue R.O.W.) which is 19.54 ft.
- viii. A Variance to allow for off-street parking spaces along the south property line. VODG Section 28.6.040.A.

General Comment #3 requested that a variance be requested for to reduce the number of required off-street parking spaces (if applicable). VODG Section 28.7.030. Based on the Parking Analysis submitted with this re-submission, a variance is not required.

**Compliance with Section 28.12.030 Zoning Map Amendments (Rezoning):**

As demonstrated by the materials enclosed with this application. Delta Sonic submits that the project meets the approval criteria contained in Municipal Code Section 28.12.030 for Zoning Map Amendments as follows:

**Part I: Review and Approval Criteria:**

**1. The existing use and zoning of nearby property;**

Delta Sonic is an existing auto oriented use located in the Ogden Avenue commercial corridor. It consists of a car wash, convenience store, detail shop and gas station. All properties along this portion of Ogden Avenue are zoned B-3 (General Services and Highway Business District) and are improved with general commercial uses. The Delta Sonic facility is consistent and compatible with the other Ogden Avenue properties.

Because of the various uses on the site and their interconnection, a Planned Unit Development is appropriate for the Delta Sonic property.

**2. The extent to which the particular zoning restrictions affect property values;**

Delta Sonic is an existing facility which does not comply with the zoning restrictions. The proposed renovations to the Delta Sonic property will significantly improve safety, circulation and access. These improvements will positively affect surrounding properties, thereby improving property values.

**3. The extent to which any diminution in property value is offset by an increase in the public health, safety and welfare;**

This Delta Sonic facility has experienced traffic and circulation issues with car wash traffic stacking out onto Ogden Avenue during peak periods. The acquisition of the Burger King property and its incorporation into the Delta Sonic facility will allow for the reconfiguration of the on-site circulation and for an expansion of on-site stacking spaces. These site modifications will improve the public health, safety and welfare.

**4. The suitability of the subject property for the zoned purposes;**

Delta Sonic is an older existing facility consisting of various auto-oriented uses. Its zoning classification of B-3 is appropriate for this facility. The designation of a Planned Unit Development allows for the existing site conditions and the proposed modifications thereto. The proposed redevelopment of the Delta Sonic facility is consistent with its B-3 zoning classification and the PUD designation.

**5. The length of time that the subject property has been vacant as zoned, considering the context of land development in the vicinity;**

The Delta Sonic property is not vacant and has been developed with uses permitted in the B-3 District. The Burger King property has been underutilized pending the site improvements for the Delta Sonic facility.

**6. The value to the community of the proposed use;**

Delta Sonic proposes to make significant improvements to its facility and property, including enhancement of landscaping, incorporation of pedestrian access, elimination of three Ogden Avenue driveways, addition of car wash stacking spaces, prep hut, vacuum stations, gas canopy and pumps and reconfiguration of on-site circulation. These improvements will beautify the property, provide safer circulation throughout the site and reduce car wash traffic congestion of Ogden Avenue, all of which are a value to the community.

**7. The comprehensive plan;**

The Village's Comprehensive Plan envisions the Ogden Avenue Commercial Corridor to provide a range of retail, service, office and business activities to serve the daily needs of local residents and to provide commercial goods and services to the larger region. The auto-oriented nature of the Delta Sonic facility is consistent with the suggested uses along this corridor. The Comprehensive Plan also encourages modernization of commercial properties. One of the Key Focus Areas is Ogden Avenue. The redevelopment of the Delta Sonic property incorporates several of the Key Concepts for the Central portion of Ogden Avenue by providing additional buffering, pedestrian access, improved signage, dumpster screening, additional Ogden Avenue and interior landscaping, and reduced number of Ogden Avenue curb cuts (5 existing / 2 proposed). The redevelopment of the Delta Sonic property fulfills several objectives of the Comprehensive Plan.

**Compliance with Section 28.12.040 Planned Unit Developments:**

As demonstrated by the materials enclosed with this application. Delta Sonic submits that the project meets the approval criteria contained in Municipal Code Section 28.12.040. for Planned Unit Development as follows:

**Part C6: Review and Approval Criteria:**

**a. The zoning map amendment review and approval criteria of Section 28.12.030 in the case of new Planned Unit Development proposals.**

The Delta Sonic facility is an existing car wash, auto detailing, gas station, convenience store and office. Because of the variety and interconnection of the uses, the Village staff recommended utilizing the Planned Unit Development. The use of the Planned Unit Development for this existing facility is appropriate and satisfies of the zoning map amendment review and approval criteria as set forth above.

**b. Whether the proposed PUD development plan and map amendment would be consistent with the comprehensive plan and any other adopted plans for the subject area.**

The Village's Comprehensive Plan encourages the Village to utilize zoning as a tool to protect an area's character. The use of a Planned Unit Development for Delta Sonic will control and enhance the redevelopment of the property so as to modernize the property as envisioned by the Comprehensive Plan.

**c. Whether PUD development plan complies with the PUD overlay district provisions of Section 28.4.030.**

**Section 28.4.030 (A):**

**a. Implementation of and consistency with the comprehensive plan and other relevant plans and policies.**

The existing Delta Sonic use and its renovation are consistent with the Village's Comprehensive Plan as set forth above.

**b. Flexibility and creativity in responding to changing social, economic and market conditions allowing greater public benefits than could be achieved using conventional zoning and development regulations.**

This Delta Sonic facility consists of various auto-oriented uses: car wash, gas station, auto detailing, convenience store and office. All of these uses are interconnected; however, they each have specific site requirements. The flexibility and creativity of the Planned Unit Development allow the site to be developed as a coherent whole, rather than several individual uses which would be required under conventional zoning and development regulations.

**c. Efficient and economical provisions of public facilities and services.**

Since the uses are concentrated, public facilities and services are more efficiently and economically provided to the property.

**d. Variety in housing types and sizes to accommodate households of all ages, sizes, incomes and lifestyle choices.**

Even though this is a commercial development, it provides services necessary to the residents of the community.

**e. Compact, mixed-use development patterns where residential, commercial, civic and open spaces are located in close proximity to one another.**

Even though this is a single-user commercial development, it is adjacent to other corridor commercial uses and residential neighborhoods. The redevelopment of the site is sensitive to the property's location by incorporating landscaping, buffering, pedestrian access and reduction of curb cuts as encouraged by the Village's Comprehensive Plan.

**f. A coordinated transportation that includes an inter-connected hierarchy of facilities for motorized and non-motorized travel.**

Even though, this is a single-user commercial development, the expansion of Delta Sonic onto the Burger King property and the redevelopment of the entire site allows the number of Ogden Avenue curb cuts to be reduced from five to two and pedestrian connectivity to be incorporated into the site.

**g. High-quality buildings and improvements that are compatible with surrounding areas, as determined by their arrangement, massing, form, character and landscaping.**

Surrounding properties are developed with one and two stories buildings. The Delta Sonic buildings are compatible in height and massing. The proposed renovation, landscape improvements, and incorporation of the Burger King property will improve the property so as to be compatible with surrounding uses and will enhance the Ogden Avenue Corridor as encouraged by the Comprehensive Plan.

**h. The protection and enhancement of open space amenities and natural resource features.**

Delta Sonic is located along the Ogden Avenue Commercial Corridor. This corridor developed with very few open space amenities or natural resource features. The redevelopment of this site will include additional landscaping and buffering, thereby enhancing the site as encouraged by the Village's Comprehensive Plan.

**i. The incorporation of sustainable development features including green infrastructure practices in landscapes and parking areas, to maximize the aesthetic and water quality benefits of best practices in stormwater management.**

Delta Sonic is an existing 27 year old facility, as part of the redevelopment of the site, the stormwater management system is being updated and BMPs, where required, will be utilized.

**j. Attractive, high-quality landscaping, lighting, architecture and signage, including the use of native landscaping that reflects the unique character of the village and the surrounding area.**

Major components of the redevelopment of the Delta Sonic property include additional landscaping and buffering, updated lighting and new signage. The renovation of the facilities and the landscape improvements will enhance Ogden Avenue and the character of the Village as contemplated by the Comprehensive Plan.

**d. Whether the proposed development will result in public benefits that are greater than or at least equal to those that would have resulted from development under conventional zoning regulations.**

Delta Sonic is a 27 year old facility in the Ogden Avenue Commercial Corridor. It includes various interconnected uses each with its own specific requirements. The Planned Unit Development allows the site to be redeveloped as a single coherent facility, rather than as separate uses under conventional zoning regulations. The proposed redevelopment incorporates several Key Concepts as outlined in the Village's Comprehensive Plan, thereby benefitting the Downers Grove community.

**e. Whether appropriate terms and conditions have been imposed on the approval to protect the interests of surrounding property owners and residents, existing and future residents of the PUD and the general public.**

Delta Sonic has worked with Village staff to optimize the redevelopment potential of the site as envisioned by the Comprehensive Plan, including elimination of three Ogden Avenue curb cuts, increased additional landscaping and buffering, increased pedestrian access and safety, more efficient on-site circulation and significant reduction of Ogden Avenue stacking. These elements of the site design protect the interests of the surrounding property owners, businesses, residents and the general public. Delta Sonic will work with the Village to establish appropriate terms and conditions in the required Village documents.

**Section 28.4.030 (D): Developers Statement of Intent:**

Delta Sonic is an existing auto-oriented facility located in the Ogden Avenue Commercial Corridor. It consists of various interconnected uses including car wash, auto detailing, gas station, convenience store and office, each of which has its own specific requirements. The Planned Unit Development allows the site to be redeveloped as a cohesive facility, rather than several individual uses.

The main focus of Delta Sonic's redevelopment proposal is to upgrade its facility and to address safety concerns. Presently, vehicles stack onto Ogden Avenue in order to access the car wash. With the acquisition and incorporation of the Burger King property into the Delta Sonic site, additional stacking spaces will be accommodated on-site and on-site circulation throughout the site will be improved, thereby increasing safety on-site and on Ogden Avenue

The Village's Comprehensive Plan envisions the Ogden Avenue Commercial Corridor to provide a range of retail, service, office and business activities to serve the daily needs of local residents and to provide commercial goods and services to the larger region. The auto-oriented nature of the Delta Sonic facility is consistent with the suggested uses along this corridor. The Comprehensive Plan also encourages modernization of commercial properties. Delta Sonic's proposal incorporates all four levels of modernization as outlined in the Comprehensive Plan. One of the Key Focus Areas is Ogden Avenue. The redevelopment of the Delta Sonic property incorporates several of the Key Concepts for the Central portion of Ogden Avenue by providing additional buffering, pedestrian access, improved signage, dumpster screening, additional Ogden Avenue and interior landscaping, and reduction in the number of Ogden Avenue curb cuts (5 existing / 2 proposed). The redevelopment of the Delta Sonic property fulfills several objectives of the Comprehensive Plan.

The Comprehensive Plan also recommends that the village use zoning as a tool to protect an area's character. The application of the Planned Unit Development to the Delta Sonic redevelopment is consistent with this Key Concept of the Plan.

**Compliance with Section 28.12.050H Special Uses:**

As demonstrated by the materials enclosed with this application, Delta Sonic submits that the project meets the approval criteria contained in Municipal Code Section 28.12.050H for Special Uses as follows:

**Part H: Review and Approval Criteria:**

- 1. That the proposed use is expressly authorized as a special use in the district in which it is to be located.**

The present zoning classification of the subject property is B-3 (General Services and Highway Business District). Pursuant to Table 5-1 of the Downers Grove Zoning Ordinance, the proposed uses (ie: Fueling Station/Personal Vehicle Repair and Maintenance) are authorized as special uses in the district.

- 2. That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.**

The existing property is presently being used as a Delta Sonic car wash, auto detailing, convenience store, gas station and offices. Delta Sonic is located along the Ogden Avenue Commercial Corridor. These auto-oriented uses serve the needs of the local residents and the larger region as encouraged by the Comprehensive Plan. Its location on Ogden Avenue is appropriate and the uses provided are in the interest of public convenience.

The proposed improvements to the property will enhance the existing facility; improving both safety on the adjoining roadway system and internal traffic circulation. The proposed improvements will increase the stacking for the existing car wash, which will result in cars having room to stack on-site instead of on Ogden Avenue when the property is busy. Additionally, the removal and replacement of the gas islands and installation of new UFTs will provide for safer fuel storage. The larger UFTs will also result in the need for fewer fuel deliveries. Additionally, removing the stormwater basin and placing those facilities underground will open more space on site for on-site circulation and improve traffic flow by making the property less congested. Finally, the improvement will allow for a new service to be provided, self-service vacuums, as well as allow for improved cleaning of vehicles by the installation of the proposed prep hut. All of these improvements will contribute to the general welfare of the community.

- 3. That the proposed use will not, in the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.**

The requested uses will not be detrimental to the health, safety, or welfare of persons working or residing in the area. The majority of the property is already being utilized by Delta Sonic and the other portion contains a vacant fast food restaurant with a drive-thru. As demonstrated by the application, the proposed project will actually result in improved safety. At the outset, the project will be reducing the number of driveways onto Ogden Avenue from five (5) existing driveways to two (2) proposed driveways. Further, the increased stacking for the car wash will significantly reduce the potential for cars to stack onto Ogden Avenue during peak car wash days. Finally, the other improvements, including both the new gas islands and removal of the detention pond, will open up the site and improve on-site circulation.

The proposed use will also not be injurious to property values or improvements in the vicinity. The project is replacing a vacant Burger King fast food restaurant with additional stacking area and vacuums, while closing three driveways on Ogden Avenue. The Burger King, when open, maintained significantly later hours than the Delta Sonic while also maintaining a drive-thru lane. The demolition of the vacant Burger King will remove the drive-thru fast food use and result in the site operating fewer hours than it previously did. Further, the existing evergreen landscaping to the south of the Burger King property will buffer adjacent properties.

Based on these considerations, the proposed redevelopment of the Delta Sonic facility will be an enhancement to the Ogden Avenue Commercial Corridor.

#### **Compliance with Section 28.12.090 Variations:**

As demonstrated by the materials enclosed with this application. Delta Sonic submits that the project meets the approval criteria contained in Municipal Code Section 28.12.090. for Variations as follows:

#### **Part G: Review and Approval Criteria:**

- 1. No variation may be approved unless the variation to be approved is consistent with the spirit and intent of this zoning ordinance and that strict compliance with the subject provisions would results in practical difficulties or particular hardships for the subject property owner.**

Delta Sonic is an established and existing commercial use which is constrained by its location. It is proposing to renovate its facility so as to enhance and improve the safety, landscaping, circulation and aesthetics of the site. The requested variations are necessitated by the constraints of the site.

- 2. The consideration of whether a variation request has met the standards of practical difficulties or particular hardships must include all of the following findings from the evidence presented:**

- a. **The subject property cannot yield a reasonable return if required to comply with the regulations that apply to it;**
- b. **The plight of the owners is due to unique circumstances**
- c. **The variation, if granted, will not alter the essential character of the locality.**

The current Delta Sonic facility does not comply with all of the regulations of the B-3 District. Likewise, in order to redevelop its property and to incorporate the Burger King property, certain variations are required due to the constraints of the site. If the variations are not granted, then the facility cannot be renovated, thereby decreasing its economic viability.

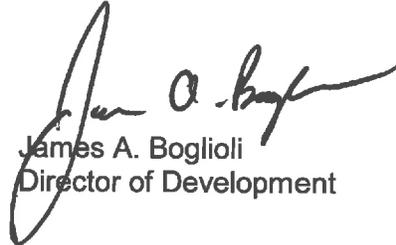
Through its renovation plans, Delta Sonic proposes to upgrade, enhance and make safer its existing facility. The proposed renovations will be a benefit to the community and upgrade the character of Ogden Avenue and will not alter the essential character of the Ogden Avenue Commercial Corridor.

- 3. **In addition, the hearing body must also take into consideration the extent to which the following facts, favorable to the property owner, have been established by the evidence presented.**
  - a. **That the physical surroundings, shape, or topographical conditions of the subject property would results in a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of the regulations were carried out**
  - b. **That the conditions leading to the need of the requested variation are not applicable, generally, to other properties within the same zoning classification**
  - c. **That the alleged difficulty or hardship was not created by the current property owner**
  - d. **That the proposed variation will not impair an adequate supply of air to adjacent property, or substantially diminish or impair property values within the neighborhood**
  - e. **That the proposed variation will not alter the essential character of the area**
  - f. **That the granting of this variation will not confer on the subject property owner any special privilege that is not available to other properties or structures in the same district;**

The Delta Sonic is an older existing facility. Over the years, it has experienced circulation and vehicle stacking issues. Through the purchase of the adjacent Burger King property and its proposed incorporation into the Delta Sonic facility, Delta Sonic anticipates that these issues will be minimized. Delta Sonic also wants to upgrade and enhance its facility. The requested variations are necessitated by the constraints of the site. The proposed renovations will not alter the character of the Ogden Avenue corridor. Therefore, the facts stated above have been established through the documents and information submitted with the application.

We are excited to upgrade the existing Delta Sonic and look forward to working with Downers Grove on this project. Should you require any additional information or wish to discuss this, please do not hesitate to contact me either at (716) 878-9626 or by e-mail at [JamesBoglioli@Benderson.com](mailto:JamesBoglioli@Benderson.com).

Sincerely,  
**Delta Sonic Car Wash Systems, Inc**



James A. Boglioli  
Director of Development

Enc.

James A. Boglioli  
Delta Sonic Carwash Systems, Inc.  
570 Delaware Avenue  
Buffalo, NY 14202

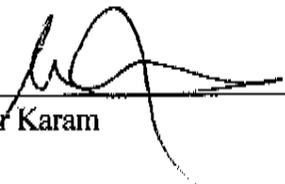
Re: 4340 Oakwood Avenue, Downers Grove

Dear Mr. Boglioli,

I am the owner of the property located at 4340 Oakwood Avenue, Downers Grove. I understand that Delta Sonic has applied to the Village of Downers Grove for certain zoning and subdivision approvals to allow Delta Sonic to renovate its existing Ogden Avenue facility. I also understand that (a) Delta Sonic has requested that the Village vacate the alley located between its property and my property, located at 4340 Oakwood Avenue, (b) that as an adjacent property owner, I have the right to acquire the south one-half of that portion of the alley as it abuts my property and (c) that Delta Sonic is proposing to maintain the alley in its current state with no changes being proposed as part of its renovation.

Please be advised that I do not have any interest in acquiring the alley and I waive any right I may have in and to the alley adjacent to my property. You are authorized to provide this letter to the Village of Downers Grove.

Sincerely,



\_\_\_\_\_

Samir Karam

DELTA SONIC  
SUMMARY OF NEIGHBORHOOD MEETING

1. ***How notification occurred:*** A letter was sent to the neighbors (letter attached).  
***When notification occurred:*** The letter was sent on June 10, 2016.  
***Who was notified:*** All people/entities within 250 feet (exclusive of right-of-way) of the Delta Sonic property as shown on the real estate tax record were notified (list of people/entities attached).
2. ***How information about the proposal was shared with neighbors:*** The letter outlined Delta Sonic's proposal with the site plan attached. The letter also invited the neighbors to an information meeting. The information meeting was held on Thursday June 23, 2016 at 7:00 p.m. at the Delta Sonic training facility.
3. ***Who was involved in the discussions:*** Three representatives of Delta Sonic were present at the information meeting. Two neighbors attended the meeting (list of attendees attached).
4. ***Suggestions and concerns raised by the neighbors:***
  - Mr. Defrates operates a business on Ogden Avenue. He asked two questions:
    - Do the existing underground fuel tanks leak?
    - What is the revenue of the Downers Grove facility?
  - Ms. Wojciechowski lives on the west side of Seeley Avenue adjacent to the Ogden Avenue commercial corridor. She had several questions/concerns:
    - How close to Seeley Avenue will the car wash lanes be located?
    - Will there be an increase in car wash traffic?
    - Will there be an increase in the noise level?
    - What is the lighting proposed for site?
    - What type of landscaping is proposed along Seeley Avenue?
    - Will there be access onto Seeley Avenue?
5. ***What specific changes to the proposal were considered and/or made as a result of the communications:*** To address Ms. Wojciechowski's concerns, Delta Sonic would be receptive to (i) installing a taller fence along the south half of the Seeley Avenue frontage and (ii) locating the landscaping on the outside of the fence. Delta Sonic will contact the Village to determine if these modifications would be acceptable to the Village.



MEMORANDUM TO: Michael Green  
Delta Sonic Car Wash Systems, Inc.

FROM: Robert A. Casiello  
Consultant

Michael A. Werthmann, PE, PTOE  
Principal

DATE: March 12, 2015  
*Revised June 7, 2016*

SUBJECT: Traffic Impact Study  
Proposed Delta Sonic Car Wash Site Improvements  
Downers Grove, Illinois

This memorandum summarizes the methodologies, results and findings of a traffic impact study (TIS) conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) regarding the proposed site improvements to the Delta Sonic Car Wash (car wash) located in Downers Grove, Illinois. Currently, the car wash is located at 1415 Ogden Avenue and is bounded by Ogden Avenue to the north, Oakwood Avenue to the east, residential homes to the south, and the former Burger King to the west. As proposed, the existing car wash is to include the former Burger King site located directly west of the car wash. The purpose of the proposed site improvements is to enhance access to and from and circulation through the site as well as to further minimize the impact of the car wash on Ogden Avenue. Other than self-service vacuum stations, no additional services will be added to the car wash.

The car wash is proposed to include the following modifications and improvements:

- *Car Wash.* The car wash prep hut will be expanded from one bay to two and the available stacking will be significantly increased by utilizing the Burger King site.
- *Fueling Positions.* The number of total fueling positions will be reduced from 16 to 10 and the fueling positions will be relocated so that they are all aligned parallel to one another at an approximate 45-degree (northeast-to-southwest) alignment to Ogden Avenue.
- *Access Drives.* The Ogden Avenue Middle Access Drive and the North Oakwood Avenue access drive serving the car wash will be eliminated and the other two Ogden Avenue access drives serving the car wash will be enhanced. In addition, the two access drives serving the former Burger King site will be eliminated.
- *Parking.* The number of parking spaces provided by the car wash will be increased from 23 to 52 spaces with 10 parking spaces provided along the front of the convenience store.

The purpose of this study was to examine background traffic conditions, assess the impact that the proposed site improvements will have on traffic conditions in the area, and determine the necessity of any improvements/modifications.

## **Existing Conditions**

Existing transportation conditions in the vicinity of the site were documented based on field visits conducted by KLOA, Inc. in order to obtain a database for projecting future conditions. The following provides a description of the geographical location of the site, physical characteristics of the area roadway system including lane usage and traffic control devices, and the existing peak hour traffic volumes.

### **Site Location**

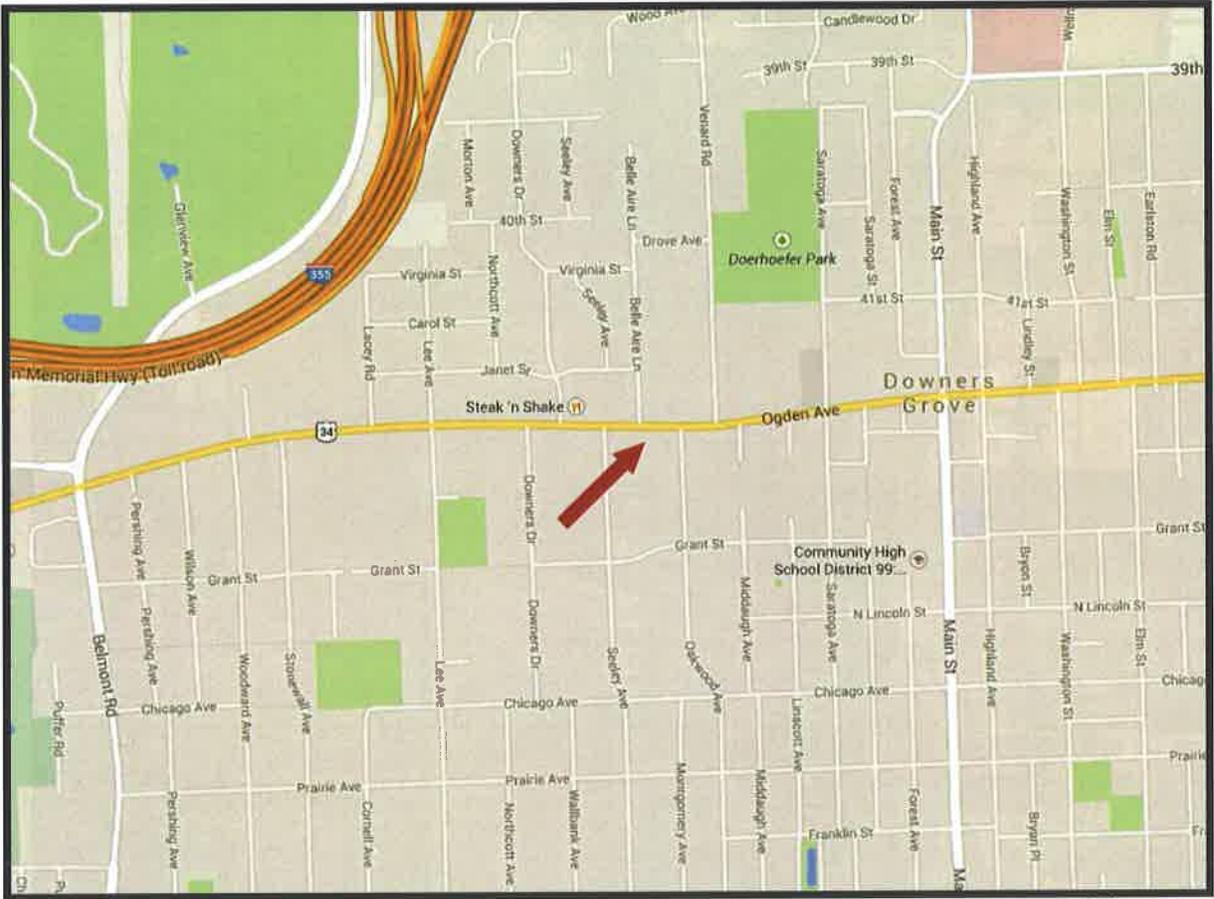
The existing car wash and the former Burger King site are located on the south side of Ogden Avenue between Seeley Avenue and Oakwood Avenue. Within the vicinity of the expanded site, land uses along Ogden Avenue generally consist of retail and commercial developments. To the north and south of Ogden Avenue, the primary use is residential. The location of the site in relation to the area roadway system is shown in **Figure 1**.

### **Existing Roadway System Characteristics**

**Figure 2** shows an aerial view of the site area and illustrates the locations of the existing car wash and the former Burger King site. The characteristics of the existing roadways serving the site are described below.

*Ogden Avenue (US Route 34)* is generally an east-west arterial roadway that has a five-lane cross-section with a two-way left-turn lane. It is under the jurisdiction of the Illinois Department of Transportation (IDOT), has a posted speed limit of 35 mph and an Average Daily Traffic (ADT) volume of 36,000 vehicles.

*Belle Aire Lane, Seeley Avenue, and Oakwood Avenue* are north-south local roadways. Belle Aire Lane is located north of Ogden Avenue, while Seeley Avenue and Oakwood Avenue are located south of Ogden Avenue. At their intersections with Ogden Avenue, the three local roads provide two-lane cross sections with their approaches to Ogden Avenue under stop sign control. A speed limit of 25 mph is posted for each road.



Site Location

Figure 1



PROJECT:  
Delta Sonic Car Wash  
Downers Grove, Illinois

TITLE:  
Aerial View of Site

**KLOAN**  
Job No: 15-017

Figure: 2

## Existing Traffic Volumes

In order to determine current traffic conditions in the vicinity of the site, KLOA, Inc. conducted peak period traffic counts at the following intersections:

- Ogden Avenue and Belle Aire Lane
- Ogden Avenue and Seeley Avenue
- Ogden Avenue and the West Car Wash Access Drive
- Ogden Avenue and the Middle Car Wash Access Drive
- Ogden Avenue and the East Car Wash Access Drive
- Ogden Avenue and Enterprise Rent-a-Car Access Drive
- Oakwood Avenue and the North Car Wash Access Drive
- Oakwood Avenue and the South Car Wash Access Drive

The traffic counts were generally conducted on Thursday, January 22, 2015 during the weekday morning (7:00 A.M. to 9:00 A.M.) and evening (4:00 P.M. to 6:00 P.M.) peak periods and on Saturday, January 24, 2015 during the midday peak period (12:00 P.M. to 2:00 P.M.). It should be noted that the Saturday counts were conducted on a mild, sunny, and dry day, which according to the operator, represented a peak day at the car wash. In addition, the Oakwood Avenue access drive traffic counts were performed on Thursday and Saturday, July 9 and 11, 2015. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:30 A.M. to 8:30 A.M., the weekday evening peak hour of traffic occurs from 4:45 P.M. to 5:45 P.M., and the Saturday midday peak hour occurs from 12:30 P.M. to 1:30 P.M. **Figure 3** illustrates the existing peak hour traffic volumes.

## Existing Characteristics and Operations of the Car Wash

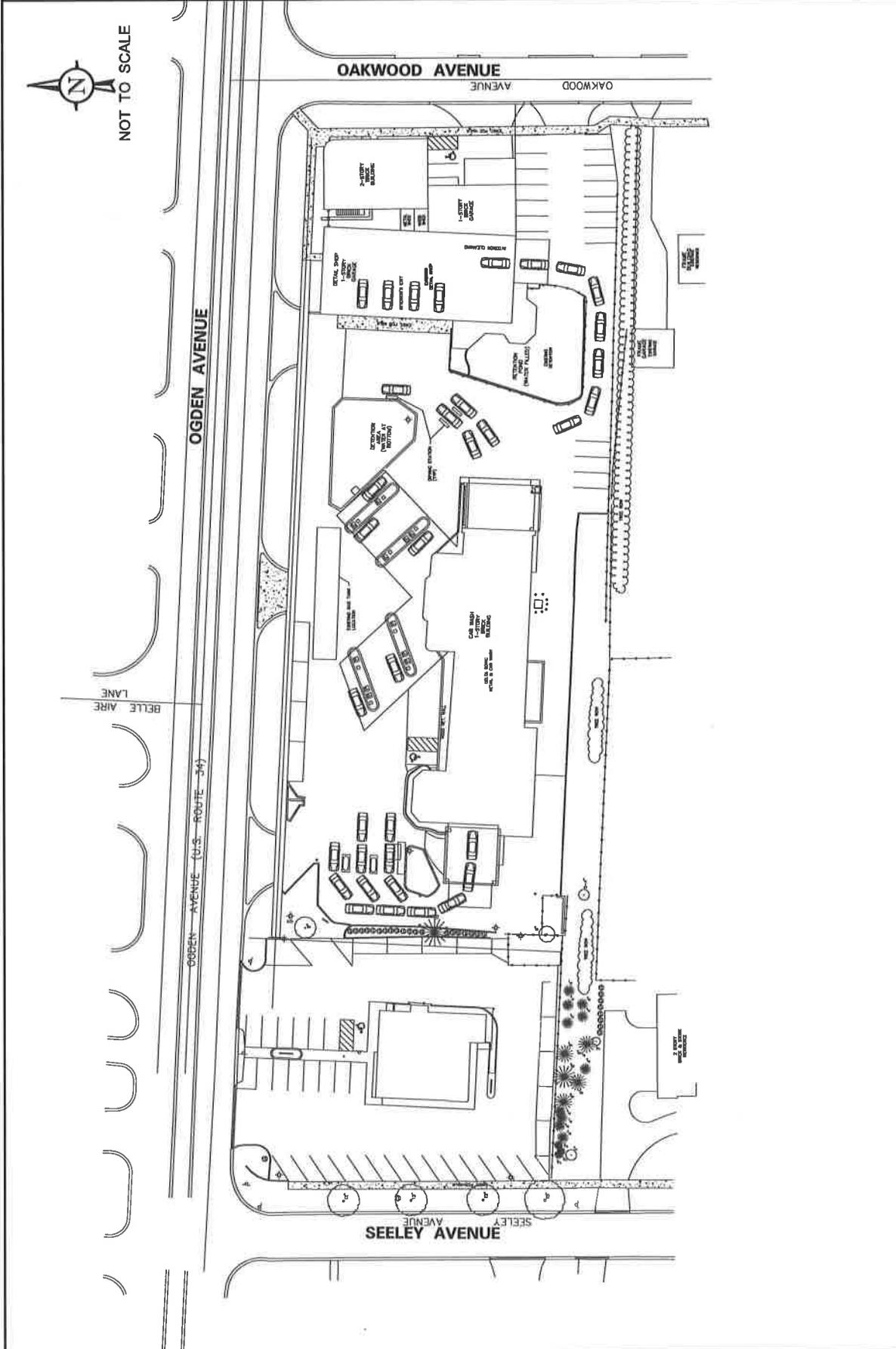
The Downers Grove Delta Sonic Car Wash provides multiple services and facilities, including car washes, detailing services, gas sales, and a convenience store. **Figure 4** illustrates the existing site plan. The following describes the existing operations of the car wash and **Figure 5** illustrates the primary existing circulation patterns through the site.

### *Existing Access*

Access to the existing car wash is provided via three access drives located on the south side of Ogden Avenue and two access drives on Oakwood Avenue. The following describes the design, location, and operation of each access drive:

- The *East Car Wash Access Drive* is located approximately 200 feet east of Belle Aire Lane (centerline to centerline) and provides full access to the site. The access drive is approximately 34 feet wide and provides one inbound lane and one outbound lane. As the traffic counts show, this access drive primarily serves outbound traffic from the car wash and the eastern set of fueling positions.





TITLE:  
**Existing Site Plan**

PROJECT:  
**Delta Sonic Car Wash  
 Downers Grove, Illinois**



- The *Middle Car Wash Access Drive* is located approximately 60 feet east of Belle Aire Lane (centerline to centerline) and provides full access to the site. The access drive is approximately 25 feet wide and provides one inbound lane and one outbound lane. As the traffic counts show, this access drive primarily serves inbound access to the furling positions and the car wash and provides secondary outbound access from the fueling positions and the convenience store.
- The *West Car Wash Access Drive* is located approximately 85 feet west of Belle Aire Lane (centerline to centerline) and provides full access to the site. The access drive is approximately 30 feet wide and provides one inbound lane and one outbound lane. As the traffic counts show, this access drive primarily provides inbound and outbound access to the western set of fueling positions as well as inbound access to the car wash. During peak periods, Delta Sonic will prohibit inbound movements at this access drive via the use of traffic cones due to the car wash queueing.
- The *Oakwood Avenue Access Drives* are located on the west side of the road at the south end of the site. The North Access Drive is approximately 31 feet wide and the South Access Drive is approximately 23 feet wide. Both access drives operate as two-way drives and primarily serve the employee parking.
- The *Former Burger King Access Drives* are located on the south side of Ogden Avenue at the east and west sides of the site. The east access drive is approximately 25 feet wide and the west access drive is approximately 32 feet wide. Both access drives operate as two-way access drives.

### ***Existing Car Wash***

The car wash operation extends along the south side of the site, with the entrance of the car wash located on the west side of the site and the exit located to the east. The majority of car wash customers enter the site via the Middle Access Drive and stacking for the car wash operation begins near the West Access Drive where three pay stations are provided. The southern pay station is the Super Lane and is an automated express lane for plan holders only. The other two pay stations are for customers who pay an attendant. According to Delta Sonic officials, approximately 40 percent of its customers at this site are plan holders. After paying, a single lane provides access to the car wash and can accommodate seven to eight vehicles between the car wash entrance and the pay stations. Given the proximity of the existing pay stations to the West Access Drive, any queueing from the pay stations has the potential to block the West Access Drive.

Upon exiting the car wash building, the customer has the following options:

- Customers have the option to have Delta Sonic employees towel dry their vehicle by traveling northeast when exiting the car wash building. Three drying stations are provided between the car wash building and the East Access Drive. (The location of the drying stations is shown on Figure 4.)

- In the event a customer paid for an interior cleaning or additional services, the customer exits the car wash building and continues slightly south and east to the detailing building located directly east of the car wash building. This building provides four drying bays along the west side of the building. These customers proceed to the detailing building by traversing a circulation road located along the south side of the site that extends to Oakwood Avenue. All vehicles enter the detailing building from the south side of the building and exit the building from one of the four bays on the west side of the building.

Given the proximity of the East Access Drive to the drying stations and as illustrated in the traffic counts, it can be seen that the majority of car wash customers exit via the East Access Drive. Based on the peak hour volumes, an average of 66 percent of the facility traffic exits the site via the East Access Drive.

### ***Existing Fueling Positions***

The car wash provides 16 fuel positions divided under two canopies located just south of the Middle Access Drive. Eight positions are provided under both the west and the east canopies. Given that both sets of positions are angled at approximately 45 degrees and the proximity of the access drives to the positions, it can be seen that the majority of the vehicles enter at the Middle Access Drive and exit via the East or West Access Drives. Based on the peak hour volumes, an average of 83 percent of the facility traffic enters the site via the Middle Access Drive.

### ***Existing Convenience Store***

The entrance to the convenience store is provided on the north side of the car wash building just south of the fueling positions. Four parallel parking spaces for the store are provided between the West Access Drive and Middle Access Drive and one handicap space is located just west of the convenience store's entrance. Customers parking in the four parking spaces must walk across the site to reach the convenience store.

### ***Existing Parking***

The existing car wash site provides a total of 23 parking spaces. Five spaces are provided along the north side of the car wash site and 18 spaces, used primarily by employees, are provided on the south side of the car wash site.

### **Observations of Existing Operations**

In addition to the peak hour traffic counts, observations of the peak hour operations were conducted at the car wash. Also, peak period usage data was obtained from the operator for the same time periods the traffic counts/observations were conducted and shown in **Table 1**.

Table 1  
 DELTA SONIC CAR WASH USAGE DURING PEAK PERIODS

Date	Time	Fuel Only	Car Wash Only	Fuel and Car Wash	Car Wash and Interior Cleaning
Thursday, January 22, 2015	7:00 A.M. to 9:00 A.M.	63	141	5	6
Thursday, January 22, 2015	4:00 P.M. to 6:00 P.M.	137	238	26	12
Saturday, January 24, 2015	Noon to 2:00 P.M.	148	364	27	29

Note: Peak hour usage data provided for hours when traffic counts were conducted.

The following summarizes the field observations:

- During high volume periods when longer queues from the car wash develop, access from the West Access Drive is often restricted via traffic cones to prevent congestion at this access drive. Typically the access drive is closed when the queueing of vehicles extends past the driveway.
- During the time of the traffic counts/observations, no closures of any inbound or outbound lanes at any of the three access drives were observed during the weekday peak period counts. However, during the Saturday midday count, the inbound lane from the West Access Drive was restricted from approximately 1:30 P.M. until the end of the count (2:00 P.M.).
- The typical queues at the car wash pay stations during the weekday peak periods were approximately two to three vehicles per lane. The longest queues observed during these periods were approximately four to five vehicles per lane, which occurred once during the morning peak hour and six times during the evening peak hour. A queue of two to three vehicles at the northern and middle pay stations blocks the West Access Drive.
- During the Saturday peak period, the queues from the car wash pay stations were between three and six vehicles per lane. It should be noted that the Saturday midday peak period counts and field observations were conducted on a relatively mild, sunny, and dry day and, as such, represented a peak day at the car wash.
- At times, paid customers were unable to advance beyond the pay stations, which impacted the queueing of vehicles waiting to pay. This is due to the processing speed of the car wash and limited stacking between the pay stations and car wash entrance.

- The car wash queue frequently extends beyond the West Access Drive. During these times, the West Access Drive was still utilized by some inbound and outbound vehicles, although some were forced to negotiate around the queued vehicles.
- Vehicles were observed queueing while waiting for gas. One or two vehicles were observed waiting during the weekday morning and evening peak periods while queueing of one to two vehicles was observed throughout the Saturday peak period. A maximum of four vehicles were observed waiting for gas during the Saturday peak period.
- On occasion, the queue of vehicles waiting for gas extended to or near the Middle Access Drive. At times, this queue impeded access from Ogden Avenue to the site.
- In order to expedite operations and minimize internal congestion, Delta Sonic utilizes several employees to assist in the operations, particularly the car wash and to direct and manage circulation through the site.
- Due to the lack of parking, a number of patrons parked their vehicles along the fueling positions even though they did not purchase gas.

## Traffic Characteristics of the Proposed Site Improvements

A description of the proposed site improvements and the modifications in access, circulation, and car wash queuing are provided below and shown in **Figure 6**. The proposed primary circulation patterns through the expanded site are illustrated in **Figure 7**.

### Proposed Development Plan

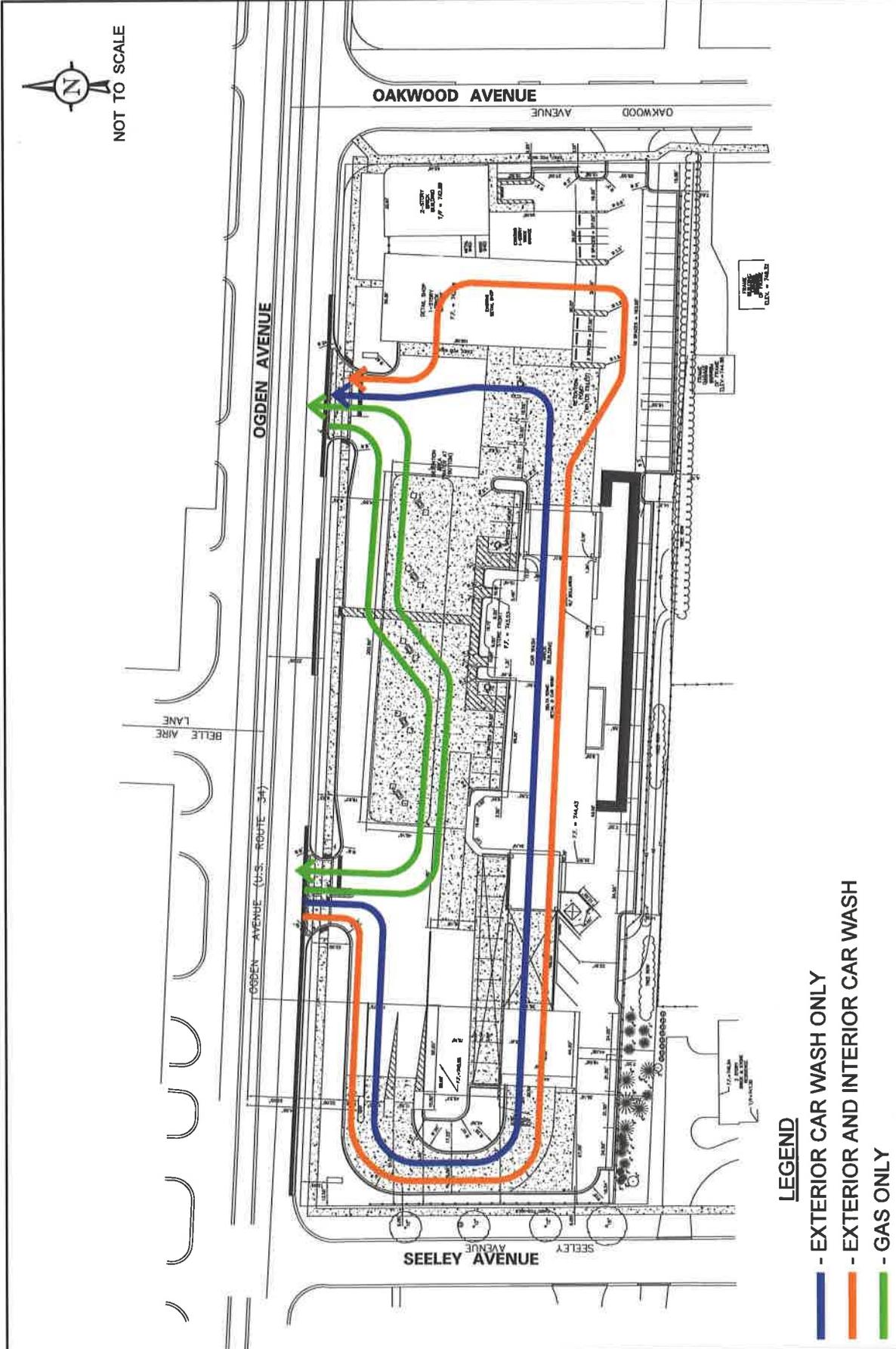
To enhance the existing vehicle access to and from and circulation through the car wash, the existing car wash is proposed to be expanded to include the former Burger King site located west of the car wash. As proposed, the former Burger King site will be used to provide a two-bay prep hut and additional stacking for the car wash. In addition, modifications are proposed to the fueling positions, access drives, and the internal circulation system. The following summarizes the proposed site improvements.

#### *Proposed Access*

As proposed, the Ogden Avenue Middle Access Drive and the North Oakwood Avenue Access Drive serving the car wash will be eliminated and the other two Ogden Avenue access drives serving the car wash will be enhanced. In addition, the two access drives serving the former Burger King site will be eliminated. The following summarizes the modifications to the car wash access system:

- The *Ogden Avenue East Access Drive* will be relocated approximately 20 feet west of its existing location in order to relocate it further from the detailing building and to improve the outbound flow of traffic from the detailing building. As proposed, this access drive is to be 39 feet wide and will provide one inbound lane and two outbound lanes striped for a separate right-turn lane and separate left-turn lane. The access drive will primarily provide inbound access to the fueling positions and outbound access from the car wash, detailing building, and the fueling positions.
- The *Ogden Avenue Middle Access Drive* is to be eliminated.
- The *Ogden Avenue West Access Drive* is to remain at the same location. As proposed, this access drive is to be 39 feet wide and will be striped for one inbound lane and one outbound lane separated by a five-foot mountable median. The access drive will primarily provide inbound access to the car wash and fueling positions and outbound access from the fueling positions.
- The *North Oakwood Avenue Access Drive* will be eliminated.
- The *South Oakwood Avenue Access Drive* is to remain in the same location. As proposed, this access drive will be approximately 25 feet wide and provide one inbound lane and one outbound lane. The access drive will primarily provide access to and from the employee parking.





Proposed Primary Circulation Patterns

PROJECT:  
 Delta Sonic Car Wash  
 Downers Grove, Illinois

TITLE:

All of the outbound lanes should be under stop sign control. The proposed site improvements will result in the elimination of three access drives on Ogden Avenue and one access drive on Oakwood Avenue.

### *Proposed Car Wash Operations*

The following modifications are proposed to the car wash operations and circulation:

- The primary purpose of the site improvements is to relocate and enhance the stacking and preparation space before vehicles enter the car wash. As proposed, the three pay stations will be relocated approximately 120 feet west of their current location, which will reduce the impact the car wash queueing has on the West Access Drive. The pay station lanes will be able to accommodate five vehicles each without blocking the West Access Drive. Furthermore, the northern lane closest to the West Access Drive will be an automated express lane while the other two lanes will be pay lanes. The automated lane generally operates quicker than the other lanes since the customers are plan holders and are familiar with what is offered, which will result in reduced queueing and less impact on the West Access Drive. As indicated previously, approximately 40 percent of the customers at this site are plan holders.
- After paying, the three pay station lanes will be tapered down to two lanes where it will continue until just after the proposed two-bay prep hut. The two lanes will then taper to one lane through the car wash. With the site improvements, the car wash will be able to accommodate approximately 19 vehicles between the pay stations and the car wash entrance.
- The purpose of the prep hut is to pre-spray each vehicle before entering the car wash to remove any major dirt and debris. The dirtier a vehicle is, the longer it will take to traverse the prep hut, which directly impacts the car wash queueing. Currently, a single vehicle that requires extra time to be properly pre-sprayed will hold up the rest of the vehicles waiting to enter the car wash. As such, the two-bay prep hut will greatly enhance the efficiency of the car wash and the subsequent vehicle queueing by allowing two vehicles to be pre-sprayed simultaneously.
- The vehicles will continue to exit the car wash via the eastern end of the site. To accommodate the realigned fueling positions and to enhance the circulation and flow of traffic from the car wash and detailing building, the existing water detention area will be eliminated and replaced with underground detention.

**Table 2** illustrates the increase in car wash stacking with the proposed site improvements. From the table it can be seen that the site improvements will significantly increase the total stacking available without impeding access to/from the West Access Drive or the internal circulation through the site. In addition, the stacking between each pay station and the West Access Drive will be increased from one to two to five vehicles per lane.

Table 2  
 CAR WASH STACKING  
 BETWEEN CAR WASH ENTRANCE AND WEST ACCESS DRIVE

	Existing Conditions	Proposed Conditions	Increase in Stacking
Between the Car Wash Entrance and the Pay Stations	9	19	10
Between the Pay Stations and the West Access Drive	<u>5</u>	<u>15</u>	<u>10</u>
Total	14	34	20

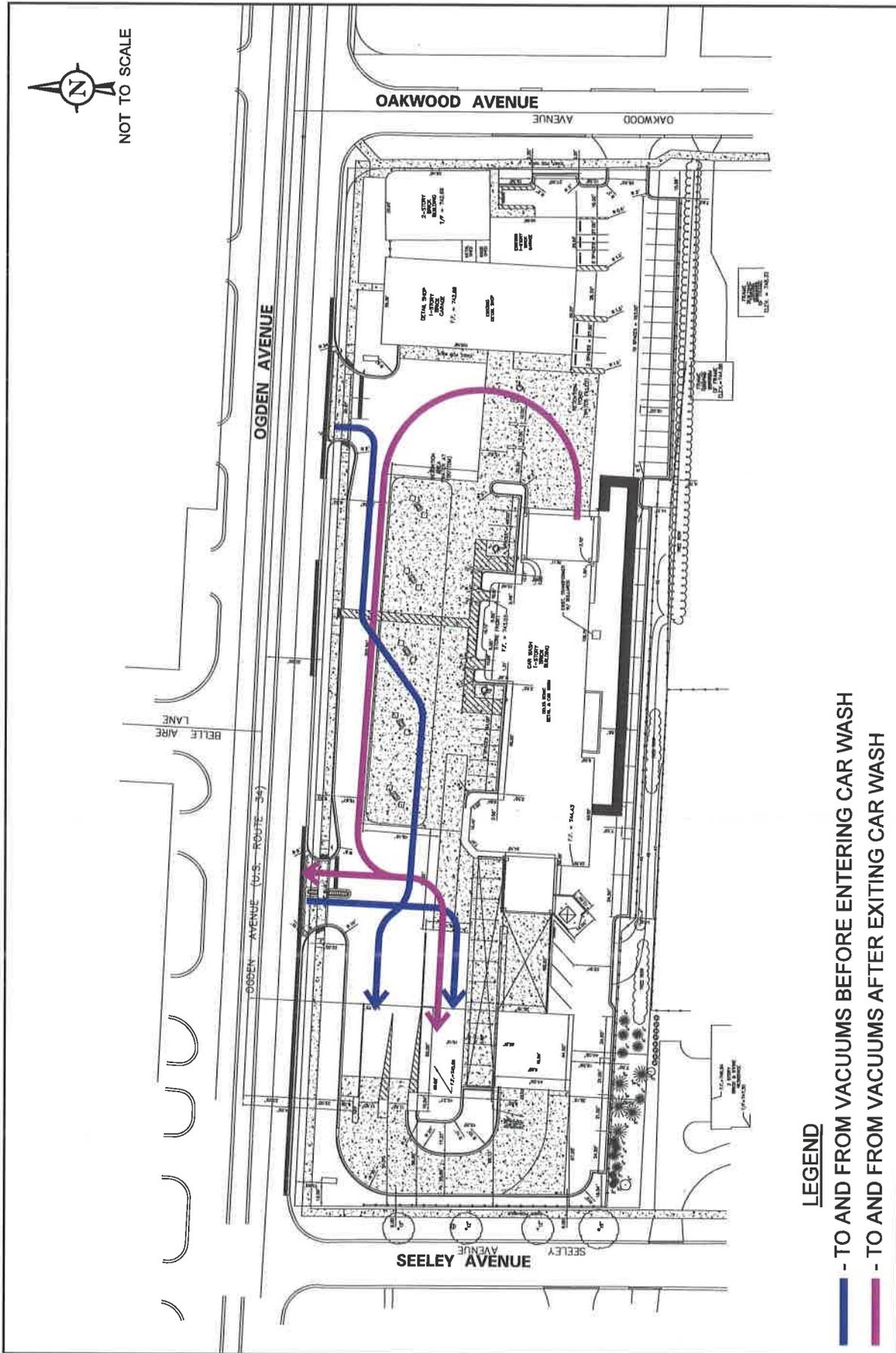
***Proposed Fueling Positions***

The total number of fueling positions will be reduced from 16 to 10. In addition, all of the fueling positions will be relocated so that they are all aligned parallel to one another at an approximate 45-degree (northeast-to-southwest) alignment to Ogden Avenue. All of the fueling positions will be located between the East and West Ogden Avenue Access Drives and between Ogden Avenue and the convenience store. The following describes the travel paths to/from the fueling positions:

- Customers entering the site from the East Ogden Avenue Access Drive will typically enter the fueling positions via the north east-west circulation road and exit the fueling positions via the south east-west circulation road and exit the site via the West Ogden Avenue Access Drive.
- Customers entering the site from the West Ogden Avenue Access Drive will typically enter the fueling positions via the south east-west circulation road and exit the fueling positions via the north east-west circulation road and exit the site via the East Ogden Avenue Access Drive.

***Proposed Self-Service Vacuum Stations***

Eleven self-service vacuum stations are proposed to be located west of the car wash building and will be the only addition to the existing services provided by the car wash. A minimum of 26 feet will be provided between where vehicles will park for the self-service vacuums and any potential queuing that may occur from the south pay station. A typical two-way parking lot drive aisle is between 20 and 24 feet. As such, it can be seen that more than sufficient space is provided between the self-service vacuums and the south pay station for vehicles to circulate to and from the self-service vacuums. Furthermore, Delta Sonic has indicated that based on their experience, a large percentage of customers use the self-service vacuums before the car wash. It should be noted that pavement striping will be used to clearly separate the vacuum station drive aisle from the drive lanes to the pay stations. **Figure 8** illustrates the likely circulation paths to and from the self-service vacuums.



**PROJECT:** Delta Sonic Car Wash  
Downers Grove, Illinois

**TITLE:** Circulation Patterns To The Proposed Vacuums

### ***Parking***

The proposed site will provide a total of 52 parking spaces with 10 spaces located along the front of the convenience store and 42 spaces on the south side of the site. As part of the 52 total parking spaces, three will consist of handicapped spaces located adjacent to the convenience store.

### **Additional Car Wash Traffic Generation**

The purpose of the proposed site improvements is to enhance the access to and from and circulation through the site and further minimize the impact of the car wash on Ogden Avenue. Other than the 11 self-service vacuum stations, the site improvements will not result in any additional services provided by the car wash. Any additional traffic due to the new self-service vacuum stations will be negligible. Further, the number of fueling positions will be reduced from 16 to 10. Therefore, the site improvements are projected to generate a limited, if any, amount of additional traffic.

While the site improvements will result in a limited increase in traffic, the elimination of the Middle Ogden Avenue Access Drive will result in the redistribution of the car wash traffic along the site access system. **Figure 9** illustrates the projected redistribution of traffic with the proposed site improvements.



TITLE:  
 REDISTRIBUTED EXISTING TRAFFIC VOLUMES

PROJECT:  
 Delta Sonic Car Wash  
 Downers Grove, Illinois

**LEGEND**  
 00 - AM PEAK HOUR (7:30-8:30 AM)  
 (00) - PM PEAK HOUR (4:45-5:45 PM)  
 [00] - SATURDAY MIDDAY PEAK HOUR (12:30-1:30 PM)

## Traffic Evaluation

As indicated previously, the primary purpose of the site improvements is to enhance vehicle access and circulation through the site as well as further minimize any impact on the surrounding roadway system. Other than the 11 additional self-service vacuum stations, the car wash is not expanding any of its current services. In fact, the site improvements will result in the reduction in the number of fueling positions from 16 to 10. As such, the additional traffic to be generated by the site improvements of the car wash is projected to be very limited, if any. The following summarizes the positive impacts that the proposed site improvements will have on the existing operations:

- *Car Wash Stacking.* The stacking for the car wash is proposed to be increased significantly. Currently, the site provides stacking for approximately 14 vehicles between the car wash entrance and the West Access Drive with stacking for only one to two vehicles at each pay station. With the relocation of the pay stations and the provision of a second lane between the pay stations and the entrance to the car wash, the proposed site improvements will provide stacking for approximately 34 vehicles between the car wash entrance and the West Access Drive. Stacking for five vehicles will be provided between each pay station and the West Access Drive for a total of 15 vehicles. As such, the site improvements will significantly increase the stacking available without impeding access to/from the West Access Drive or the internal circulation through the site.
- *Car Wash Operations.* The site improvements will allow for a two-bay prep hut which will greatly enhance the efficiency of the car wash as two vehicles will be able to be pre-sprayed simultaneously. The primary benefit of the two-bay prep hut is the fact that a single vehicle that requires extra time to be properly pre-sprayed before entering the car wash will not hold up the rest of the vehicles waiting to enter the car wash. As such, in addition to providing considerably more stacking, the car wash will be able to process more vehicles as they are prepped for the car wash, which will further reduce the car wash queue.
- *Site Access.* As part of the site improvements, the Middle Ogden Avenue Access Drive serving the car wash and the two Burger King access drives will be eliminated from Ogden Avenue. In addition, the North Oakwood Avenue Access Drive serving the car wash site will be eliminated. Further, the two Ogden Avenue access drives are proposed to be modified to enhance the flow of traffic to/from the site and the capacity of the access drives. The East Access Drive will be widened to provide one inbound lane and two outbound lanes striped for a separate left-turn lane and a separate right-turn lane. In addition, the West Access Drive will operate significantly better given the improvements to this drive and the fact that it will generally no longer be blocked or closed given the significant increase in car wash stacking and the improved car wash operations.

- *Circulation to/from the Fueling Positions.* The relocation and realignment of the fueling positions, the elimination of the Middle Ogden Avenue Access Drive, and the increase in car wash stacking will provide for more efficient circulation to and from the fueling positions as follows:
  - ❖ Currently, the West Access Drive is often blocked or closed due to the existing car wash queue which can make exiting the western set of fueling positions difficult as vehicles must make a U-turn within the site to reach the Middle Access Drive. With the proposed site improvements, the West Access Drive should generally no longer be blocked/closed allowing quicker and more direct access between Ogden Avenue and the realigned fueling positions.
  - ❖ The elimination of the Middle Access Drive and the realignment of the fueling positions will eliminate the number of existing turning conflicts within the site and provide for a more direct and less confusing circulation route to and from the fueling positions.
  - ❖ Two-way, east-west circulation roads will be provided both north and south of the fueling positions.
- *Reduced Impact on Ogden Avenue.* The impact of the car wash on Ogden Avenue and the external roadway system will be reduced due to the following:
  - ❖ The Middle Ogden Avenue Access Drive serving the car wash and the two existing Ogden Avenue access drives serving the former Burger King site will be eliminated, reducing the number of access drives along Ogden Avenue.
  - ❖ Traffic will be able to enter the site more quickly and efficiently given the various modifications to the access system. In addition, the increased stacking and enhanced circulation of traffic through the site will minimize the internal stacking within proximity to the West Access Drive which potentially restricts access into the site.
- *Parking.* The parking within the site will be increased from 23 to 52 parking spaces.

## Conclusion

This study was performed to determine the impact of the site improvements proposed for the Delta Sonic Car Wash located in Downers Grove, Illinois. As proposed, the existing car wash will be expanded to include the former Burger King site which will be used primarily to provide a two-bay prep hut and additional stacking for the car wash. In addition, modifications are proposed to the access drives and the internal circulation. Primary access to the site will continue to be provided via two of the existing Ogden Avenue access drives.

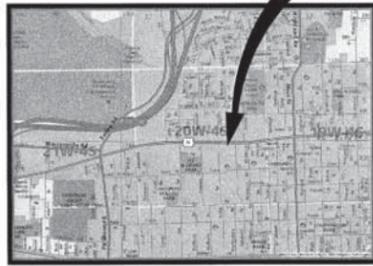
The primary purpose of the site improvements is to enhance vehicle access to and from and circulation through the site as well as further minimize any impact on the surrounding roadway system. Other than 11 additional vacuum stations, the car wash is not expanding any of its current services. Furthermore, based on Delta Sonic officials' experience, a large percentage of customers use the vacuums prior to entering the car wash. In addition, the number of fueling positions will be reduced from 16 to 10. As such, the additional traffic to be generated by the site improvements of the car wash is projected to be very limited, if any. The following summarizes the positive impacts that the proposed site improvements will have on the existing operations:

- The proposed site improvements will significantly increase the car wash stacking.
- The proposed site improvements will enhance the car wash operations which will further reduce the car wash queues.
- The proposed site improvements and associated access modifications will enhance the flow of traffic to/from the site.
- The proposed site improvements and associated access and internal circulation modifications will result in a more efficient and orderly circulation of traffic through the site.
- The proposed site improvements will provide more on-site parking.
- The proposed site improvements will eliminate the Middle Ogden Avenue Access Drive serving the car wash and two existing access drives serving the former Burger King site, reducing the number of access drives along Ogden Avenue.
- The proposed site improvements and associated access and circulation modifications will result in quicker and more efficient access from Ogden Avenue to the site.
- With the site improvements the car wash will provide a total of 52 parking spaces while generating minimal, if any, increase in parking demand over existing conditions.

# ALTA/ACSM LAND TITLE SURVEY

OF

PARCEL A: 09-06-405-010  
PARCEL B: 09-06-405-007  
PARCEL C: 09-06-405-008  
PARCEL D: 09-06-405-009  
PARCEL E: 09-06-405-008  
PARCEL F: 09-06-405-001



LOCATION MAP  
NOT TO SCALE

### PARCEL A

TICOR TITLE INSURANCE COMPANY COMMITMENT 2000.000749618 DW (LOT 1 BLOCK 2).  
THE LAND REFERRED TO IN THIS POLICY IS DESCRIBED AS FOLLOWS:

LOT 1 BLOCK 2 (EXCEPT THE SOUTH 20 FEET THEREOF) IN BLOCK 2 IN ARTHUR T. MCINTOSH AND CO'S ADDITION TO DOWNERS GROVE, BEING A SUBDIVISION OF LOT 2 OF THE PLAT OF DOWNERS GROVE, IN THE SOUTHWEST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID ARTHUR T. MCINTOSH AND CO'S ADDITION TO DOWNERS GROVE, RECORDED JUNE 26, 1924 AS DOCUMENT 179462, IN DUPAGE COUNTY, ILLINOIS.

### PARCEL B

UNNAMED TITLE COMPANY, POLICY 51161905 (LOTS 2 AND 3 BLOCK 2).  
THE LAND REFERRED TO IN THIS POLICY IS DESCRIBED AS FOLLOWS:

PARCEL ONE: LOT 2 (EXCEPT THE WEST 20 FEET THEREOF) IN BLOCK 2 IN ARTHUR T. MCINTOSH AND COMPANY'S ADDITION TO DOWNERS GROVE, BEING A SUBDIVISION OF LOT 2 OF THE PLAT OF DOWNERS GROVE, IN THE SOUTHWEST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID ARTHUR T. MCINTOSH AND COMPANY'S ADDITION TO DOWNERS GROVE, RECORDED JUNE 26, 1924 AS DOCUMENT 179462, IN DUPAGE COUNTY, ILLINOIS.

PARCEL TWO: THE WESTERLY PORTION OF LOT 2 IN BLOCK 2 IN ARTHUR T. MCINTOSH AND COMPANY'S ADDITION TO DOWNERS GROVE, BEING A SUBDIVISION OF LOT 2 OF THE PLAT OF DOWNERS GROVE, IN THE SOUTHWEST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, SAID WESTERLY PORTION BEING APPROXIMATELY 20 FEET IN WIDTH AND BEING ALL OF THAT OF SAID LOT 2 WHICH LIES WESTERLY OF A STRAIGHT LINE MARKING THE WEST FACE OF THE CONCRETE BLOCK WALL OF THE REAR OF NARROW PORTION OF THE "L" SHAPED ONE-STORY BRICK AND BLOCK BUILDING LOCATED ON SAID LOT 2, ALL IN DUPAGE COUNTY, ILLINOIS.

### PARCEL C

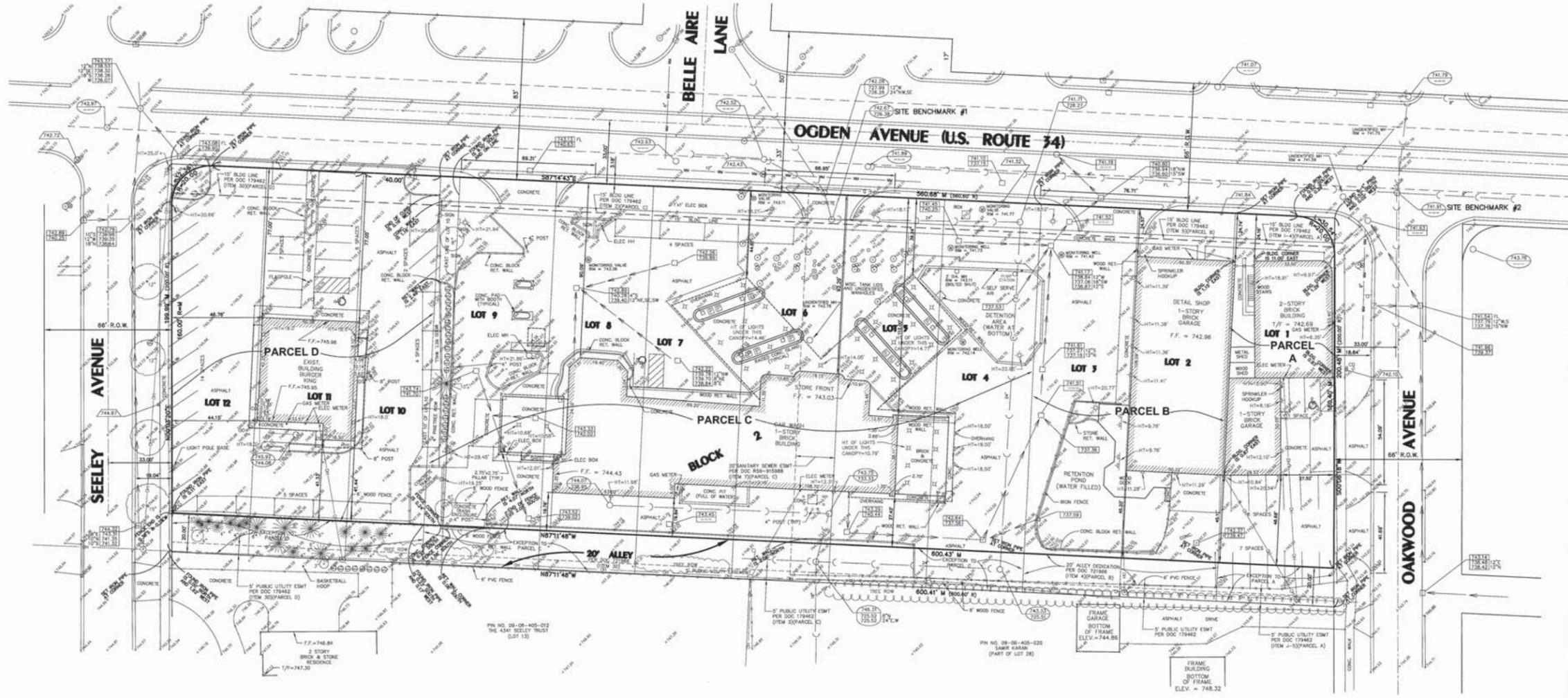
TICOR TITLE INSURANCE COMPANY COMMITMENT 0-99156 (LOTS 4-9 AND EAST 1/2 LOT 10 BLOCK 2).  
THE LAND REFERRED TO IN THIS POLICY IS DESCRIBED AS FOLLOWS:

LOTS 4, 5, 6, 7, 8, 9 AND THE EAST 1/2 OF LOT 10 (EXCEPT THE SOUTH 20 FEET OF EACH LOT HERETOFORE DEDICATED FOR ALLEY PURPOSES) IN BLOCK 2 IN ARTHUR T. MCINTOSH AND COMPANY'S ADDITION TO DOWNERS GROVE, BEING A SUBDIVISION OF LOT 2 OF THE PLAT OF DOWNERS GROVE, IN THE SOUTHWEST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID ARTHUR T. MCINTOSH AND COMPANY'S ADDITION TO DOWNERS GROVE, RECORDED JUNE 26, 1924 AS DOCUMENT 179462, IN DUPAGE COUNTY, ILLINOIS.

### PARCEL D

STEWART TITLE GUARANTY COMPANY, POLICY NO. 0-9301-001572690 (LOTS 11, 12 AND WEST 4/2 LOT 10 BLOCK 2).  
THE LAND REFERRED TO IN THIS POLICY IS DESCRIBED AS FOLLOWS:

THE WEST 4/2 OF LOT 10, AS MEASURED ON THE NORTH AND SOUTH LINES, TOGETHER WITH LOTS 11 AND 12 (EXCEPT THEREFROM THAT PART OF THE AFORESAID LOTS THAT HAVE BEEN DEDICATED FOR ALLEY PURPOSES AND RECORDED AS DOCUMENT NO. 721966 IN DUPAGE COUNTY, ILLINOIS) IN BLOCK 2 IN ARTHUR T. MCINTOSH AND CO'S ADDITION TO DOWNERS GROVE, BEING A SUBDIVISION OF LOT 2 OF THE PLAT OF DOWNERS GROVE, IN THE SOUTHWEST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY, ILLINOIS.



### LEGEND

EXISTING	SANITARY SEWER
W	WATER MAIN
S	STORM SEWER
U	UNDERGROUND ELECTRIC LINE
OH	OVERHEAD WIRES
G	GAS LINE
T	TELEPHONE LINE
F	FENCE
△	FIRE HYDRANT
○	VALVE IN VAULT
□	VALVE BOX
○	INLET
○	CATCH BASIN
○	MANHOLE WITH CLOSED LID
○	MANHOLE WITH OPEN LID
○	FLARED END SECTION
○	LIGHT POLE
○	CANOPY LIGHT
○	BUILDING MOUNTED LIGHT
○	LIGHT POLE (MAST ARM)
○	POWER POLE
○	GUY WIRE
○	PEDESTAL SIGN
○	CLIP & CUTTER
○	SURF ELEVATION
○	CONTOUR
○	TREE
○	PINE
○	SHRUB
○	BUSHES
○	CALLOUT FOR SANITARY MANHOLE
○	CALLOUT FOR WATER STRUCTURE
○	CALLOUT FOR STORM STRUCTURE

SITE AREA: 107,307 S.F. = 2.4779 ACRES

### SURVEYOR NOTES:

- THIS SURVEY IS BASED ON THE FOLLOWING TITLE COMMITMENTS:  
LOT 1, BLOCK 2, TICOR TITLE INSURANCE COMPANY, POLICY NO. 2000.000749618, EFFECTIVE DATE MAY 1, 2000 (PARCEL A)  
LOTS 2 AND 3, BLOCK 2, UNNAMED TITLE COMPANY, POLICY 51161905, EFFECTIVE DATE DECEMBER 11, 1987 (PARCEL B)  
LOTS 4-9 AND THE EAST 1/2 OF LOT 10, BLOCK 2, TICOR TITLE INSURANCE COMPANY, POLICY NO. 0-99156, EFFECTIVE DATE OCTOBER 2, 1987 (PARCEL C)  
LOTS 11, 12 AND THE WEST 4/2 OF LOT 10, BLOCK 2, STEWART TITLE GUARANTY COMPANY, POLICY NO. 0-9301-001572690, EFFECTIVE DATE JANUARY 16, 2014 (PARCEL D)  
ALL EASEMENTS AND ENCUMBRANCES SHOWN ON THIS SURVEY ARE LIMITED TO THOSE NAMED IN THESE COMMITMENTS.
- UTILITIES SHOWN ARE BASED ON VISUAL INSPECTION AND A J.U.L.I.E. DESIGN DIG ORDERED JULY 24, 2014, DIG NO. A2050371.  
THE FOLLOWING UTILITY COMPANIES RESPONDED TO THE REQUEST FOR INFORMATION:  
1. ATT DISTRIBUTION RYAN KENNELLY 630-673-5460 (COULD NOT SEND INFO)  
2. COMED 630-576-7094  
3. DOWNERS GROVE SANITARY DISTRICT TED CHERWAK 630-969-0664 X 108  
4. VILLAGE OF DOWNERS GROVE ANDY BIRCH 630-434-5494  
5. MCI DEAN BOYERS 972-729-8222  
6. NICOR GAS 630-387-2262
- THE FOLLOWING UTILITY COMPANIES DID NOT RESPOND TO THE REQUEST FOR INFORMATION:  
1. COMCAST MARTHA GIERAS 630-600-6332  
2. DUPAGE WATER COMMISSION MICHAEL SCHWEIZER 630-634-0160  
3. SIDERA NETWORKS MATT COPE 312-432-0076
- WE ARE NOT AWARE OF ANY PROPOSED CHANGES IN STREET HIGH-OF-WAY LINES, THERE IS NO OBSERVED EVIDENCE OF RECENT STREET OR SIDEWALK CONSTRUCTION OR REPAIR.

### NOTES:

- TICOR TITLE INSURANCE COMPANY COMMITMENT 2000.000749618 DW (LOT 1 BLOCK 2).  
1. REFERENCE IS MADE TO A 15' BUILDING SETBACK LINE SHOWN ON DOC. 179462 (ITEM 1-4) (PLOTTED)  
2. REFERENCE IS MADE TO A 5' WIDE EASEMENT FOR PUBLIC UTILITIES PER DOC. 179462 (ITEM A-5) (PLOTTED)
- UNNAMED TITLE COMPANY, POLICY 51161905 (LOTS 2 AND 3 BLOCK 2).  
1. REFERENCE IS MADE TO A DEDICATION FOR ALLEY ALONG THE SOUTH 20 FEET OF THE LAND PER DOC. 721966 (ITEM 4) (PLOTTED)  
2. REFERENCE IS MADE TO A 15' BUILDING SETBACK LINE SHOWN ON DOC. 179462 (ITEM 5) (PLOTTED)  
3. REFERENCE IS MADE TO THE PROPERTY AS PART OF THE DOWNERS GROVE SANITARY DISTRICT SUBJECT TO PROVISIONS IN A CERTAIN ORDINANCE NO. 14 PER DOC. 75-4376 (ITEM 6) (NOT PLOTTABLE)
- STEWART TITLE GUARANTY COMPANY, POLICY NO. 0-9301-001572690 (LOTS 11, 12 AND WEST 4/2 LOT 10 BLOCK 2).  
1. REFERENCE IS MADE TO DEDICATION OF ALLEY PER DOC. 721966 (ITEM 3) (PLOTTED)  
2. REFERENCE IS MADE TO DOC. 179462 FOR SETBACK LINE (ITEM 30) (PLOTTED)

### ZONING:

THIS PROPERTY IS ZONED B-3 GENERAL SERVICES AND HIGHWAY BUSINESS  
REFER TO VILLAGE OF DOWNERS GROVE WEB SITE FOR SPECIFICS (WWW.DOWNERS.US)

### FLOOD ZONE:

UPON INVESTIGATION OF THE NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP, DUPAGE COUNTY, ILLINOIS, MAP NUMBER 17483001H, EFFECTIVE DATE DECEMBER 16, 2004, THIS PROPERTY FALLS IN FLOOD ZONE "X" UNHAZARDED, BEING DEFINED AS "AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOOD PLAIN."

### CONTROL BENCHMARKS:

- DUPAGE COUNTY GEODETIC SURVEY MONUMENT 0166 (PID. DK 3214)  
3.5" BRASS DISK EAST SIDE OF FINLEY ROAD SOUTH OF INTERSTATE 66 AT THE OVERPASS  
ELEVATION 771.91 NAVD 88
- DUPAGE COUNTY GEODETIC SURVEY MONUMENT 0006 (PID. DK 3312)  
3.5" BRASS DISK NORTHEAST CORNER WASHINGTON STREET AND WARREN AVE.  
ELEVATION 716.78 NAVD 88

### SITE BENCHMARK:

- SANITARY MANHOLE RM AT ENTRANCE NEAR MIDDLE OF SITE  
ELEVATION 742.87
- SANITARY MANHOLE RM, CENTERLINE OF OAKWOOD AVENUE AT THE SOUTH EDGE OF OGDEN AVENUE  
ELEVATION 741.91

### PARKING SPACES:

REGULAR SPACES 56  
HANDICAP SPACES 2

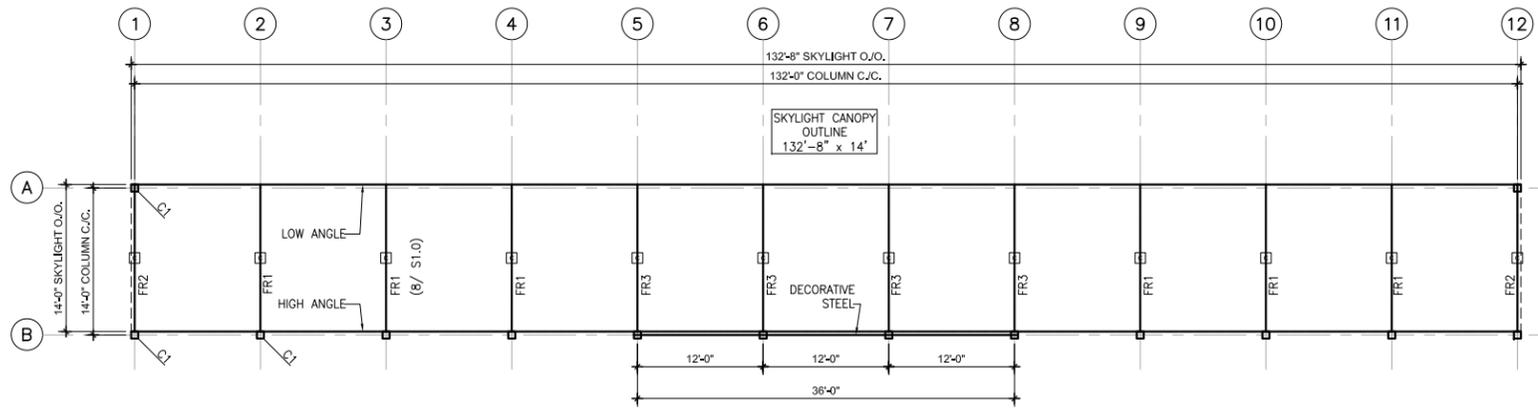
### SURVEYOR'S CERTIFICATE:

STATE OF ILLINOIS )  
COUNTY OF DUPAGE )  
I, MICHAEL SCHWEIZER, being duly sworn, depose and say that I am a duly licensed Professional Land Surveyor in the State of Illinois, License No. 35-2587, and that I am the author of the above and foregoing map, and that the same is a true and correct copy of the original map as the same appears on my files, and that the same was completed on 11/13/2014.

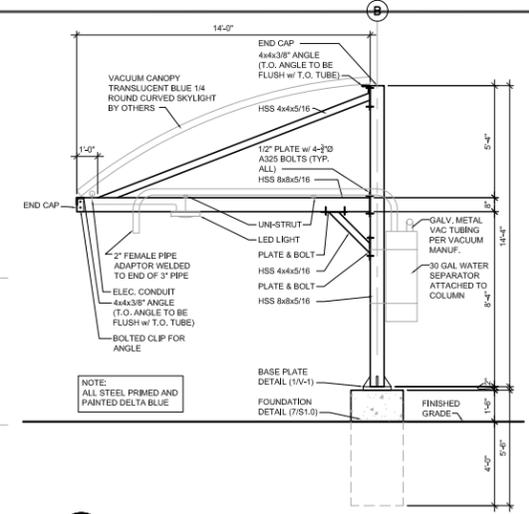
DATE OF MAP: 11/13/2014  
BY: MICHAEL SCHWEIZER  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2587  
MY LICENSE EXPIRES/RENEWS 11-30-2014

**INTECH CONSULTANTS, INC.**  
ENGINEERS / SURVEYORS  
1989 UNIVERSITY LANE, SUITE D U.S.E. ILLINOIS  
TEL: (630) 964-2656 FAX: (630) 964-5052  
E-MAIL: CAD@INTECHCONSULTANTS.COM  
ILLINOIS REGISTRATION NO. 184-001040

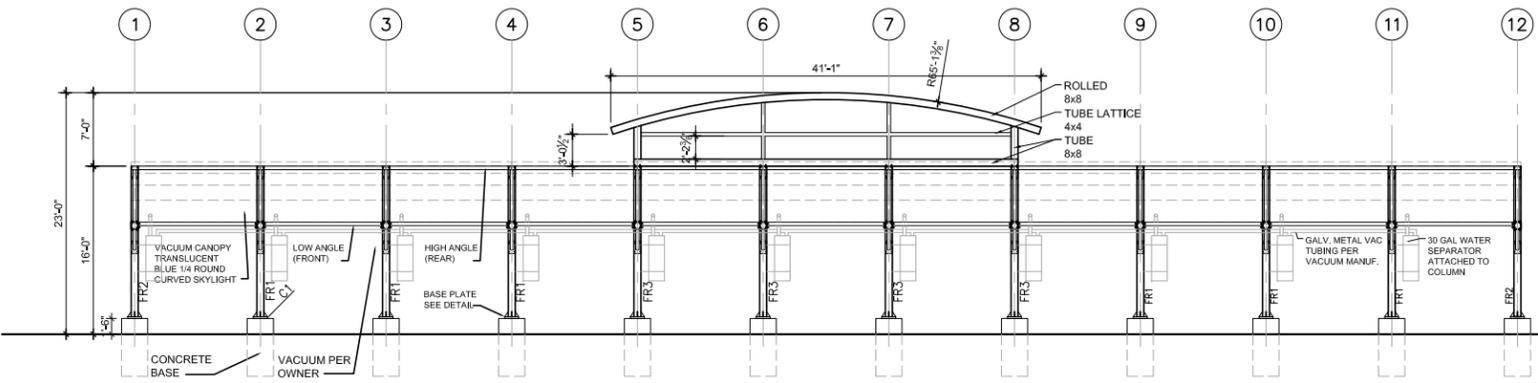
ALTA/ACSM LAND TITLE SURVEY



**2 WEST ELEVATION (CARWASH/ C-STORE)**  
SCALE: 1/16" = 1'-0"

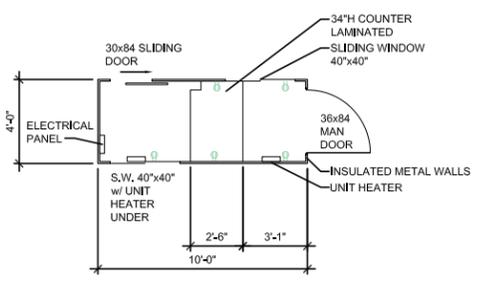


**4 FR1**  
SCALE: 1/4" = 1'-0"

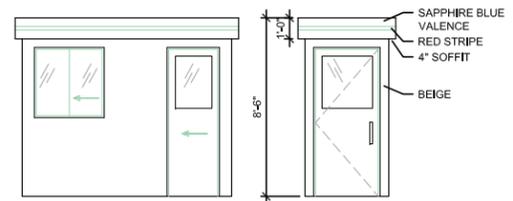


**3 FRONT ELEVATION (NORTH)**  
SCALE: 1/16" = 1'-0"

CONSTRUCTION TYPE:  
VACUUM CANOPIYS: II-B  
(UNPROTECTED STEEL)

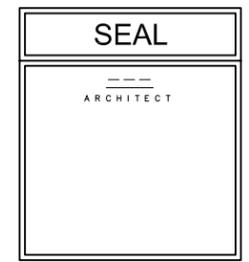


**2 CASHIER BOOTH PLAN**  
SCALE: 1/4" = 1'-0"



**2 CASHIER BOOTH ELEVATIONS**  
SCALE: 1/4" = 1'-0"

CONSTRUCTION TYPE:  
CASHIER BOOTH: II-B  
(UNPROTECTED STEEL)



**Special Information:**  
IT IS UNDERSTOOD THAT ANY WARRANTY INFORMATION CONCERNING EQUIPMENT INSTALLED MUST BE FORWARDED TO THE OWNER AND THAT ANY AND ALL CONTRACTORS SHALL GUARANTEE THEIR WORK FOR A PERIOD OF ONE YEAR FROM THE DATE OF OWNERS ACCEPTANCE.  
  
ARCHITECT IS NOT RESPONSIBLE FOR MATERIALS, METHODS, INSTALLATION, AND CONSTRUCTION WHICH DEVIATE FROM CONSTRUCTION DOCUMENTS AND SPECIFICATIONS.

Drawing Revisions:	No.	DATE	BY	REMARKS

Property No: **1820**  
Area: -  
Status: **REVIEW & APPROVAL**

Property:  
**Delta Sonic**  
1415 Ogden Ave  
Downers Grove, IL 60515

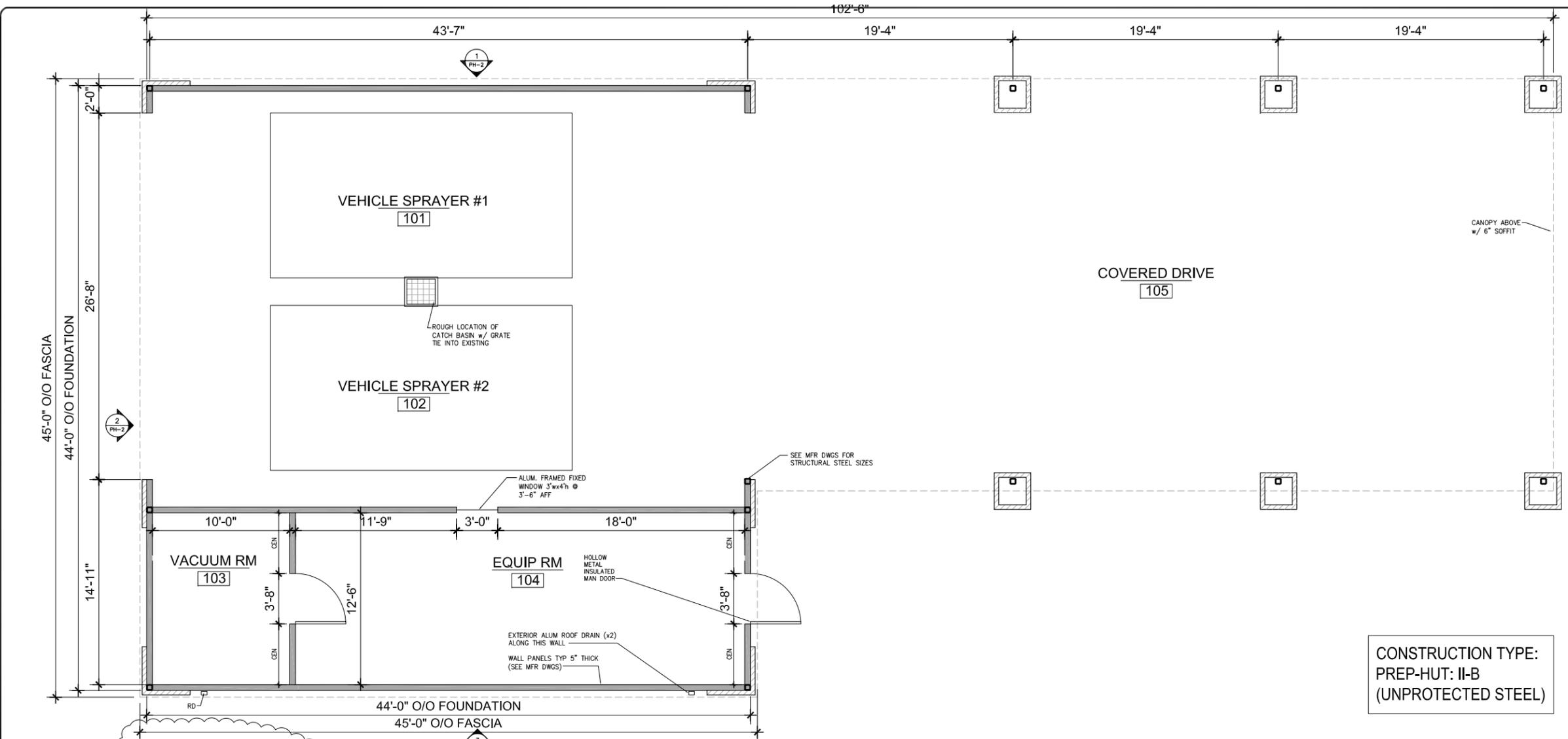
Tenant:  
**Delta Sonic Car Wash**

Title:  
**PLANS & ELEVATIONS**  
**VACUUM & BOOTHS**



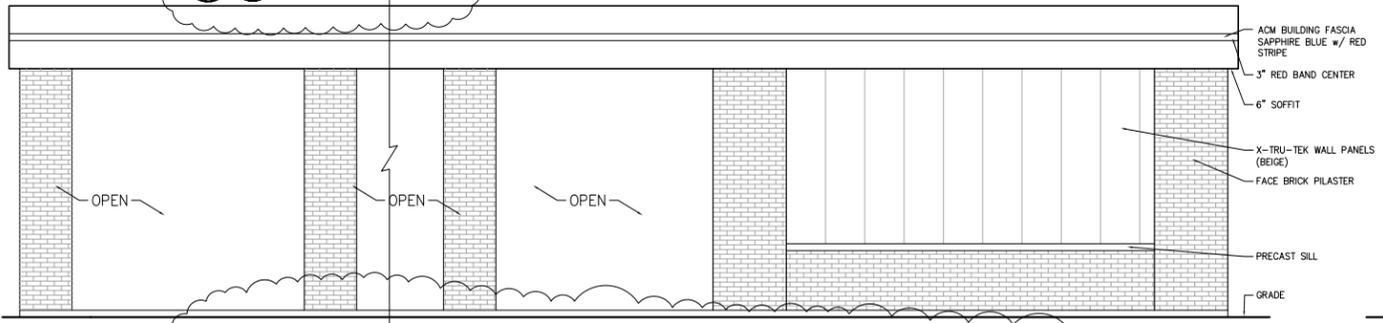
Drawn By: **MG**  
Checked By: **JAR**  
Date: **10-14-14**

Drawing No.: **A-1.2**

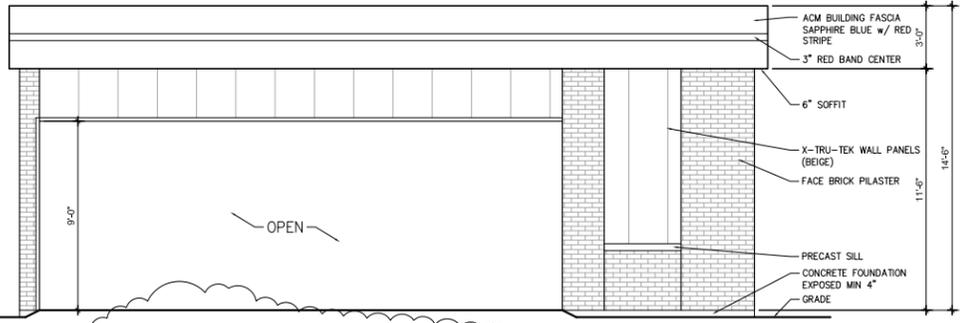


CONSTRUCTION TYPE:  
PREP-HUT: II-B  
(UNPROTECTED STEEL)

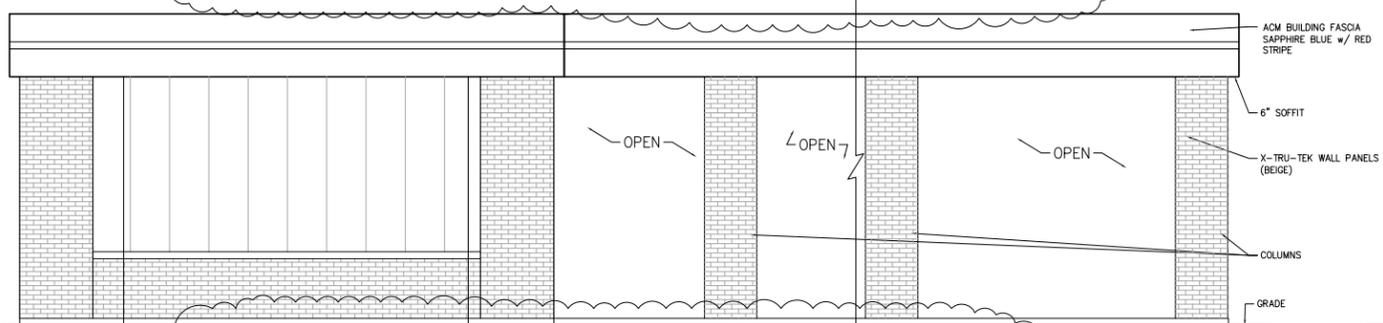
1 FLOOR PLAN  
SCALE: 1/4"=1'-0"



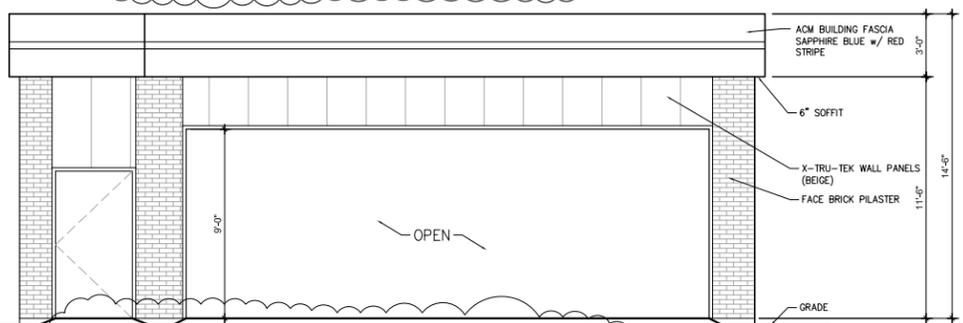
2 NORTH ELEVATION  
SCALE: 1/4"=1'-0"



3 WEST ELEVATION (ENTRANCE)  
SCALE: 1/4"=1'-0"



4 SOUTH ELEVATION  
SCALE: 1/4"=1'-0"



5 EAST ELEVATION (EXIT)  
SCALE: 1/4"=1'-0"



Special Information:  
IT IS UNDERSTOOD THAT ANY WARRANTY INFORMATION CONCERNING EQUIPMENT INSTALLED MUST BE FORWARDED TO THE OWNER AND THAT ANY AND ALL CONTRACTORS SHALL GUARANTEE THEIR WORK FOR A PERIOD OF ONE YEAR FROM THE DATE OF OWNERS ACCEPTANCE.  
ARCHITECT IS NOT RESPONSIBLE FOR MATERIALS, METHODS, INSTALLATION, AND CONSTRUCTION WHICH DEVIATE FROM CONSTRUCTION DOCUMENTS AND SPECIFICATIONS.

Drawing Revisions:		REMARKS	BY	DATE
No.	1	FOR PERMIT	CRW	1-15-16
	2	PER TOWN COMMENTS	CRW	4-29-16

Property No:  
**1820**

Status:  
**REVIEW & APPROVAL**

Property:  
**Delta Sonic**  
1415 Ogden Ave  
Downers Grove, IL 60515

Tenant:  
**Delta Sonic Car Wash**

Title:  
**PLAN & ELEVATION  
PREP-HUT**



Drawn By:  
MG

Checked By:  
JAR

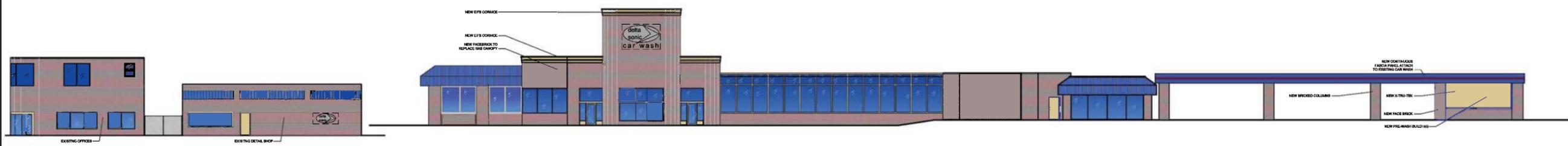
Date:  
10-14-14

Drawing No.  
**A-1.3**

**Special Information:**

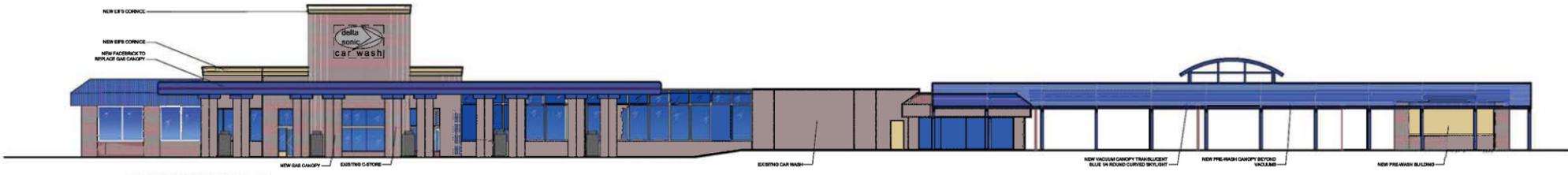
IT IS UNDERSTOOD THAT ANY WARRANTY INFORMATION CONCERNING EQUIPMENT INSTALLED MUST BE FORWARDED TO THE OWNER AND THAT ANY AND ALL CONTRACTORS SHALL GUARANTEE THEIR WORK FOR A PERIOD OF ONE YEAR FROM THE DATE OF OWNERS ACCEPTANCE.

ARCHITECT IS NOT RESPONSIBLE FOR MATERIALS, METHODS, INSTALLATION AND CONSTRUCTION WHICH DEVIATE FROM CONSTRUCTION DOCUMENTS AND SPECIFICATIONS.

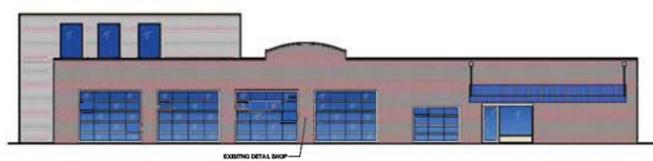


1 NORTH ELEVATION (OFFICE/DETAIL SHOP)  
SCALE: 1/16" = 1'-0"

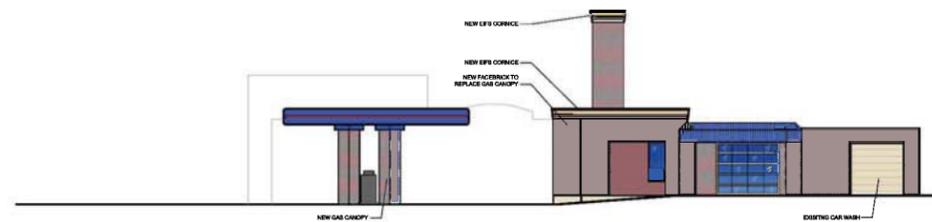
2 NORTH ELEVATION (C-STORE/ PREP HUT)  
SCALE: 1/16" = 1'-0"



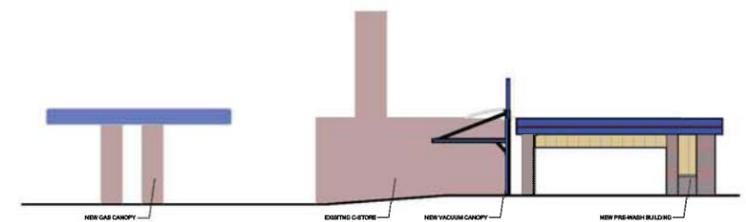
2.1 NORTH ELEVATION (GAS/ C-STORE/ VACUUMS)  
SCALE: 1/16" = 1'-0"



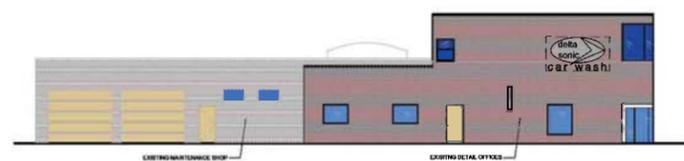
3 WEST ELEVATION (CARWASH/ C-STORE)  
SCALE: 1/16" = 1'-0"



4 WEST ELEVATION (CARWASH/ C-STORE)  
SCALE: 1/16" = 1'-0"



5 WEST ELEVATION (CARWASH/ C-STORE)  
SCALE: 1/16" = 1'-0"



6 EAST ELEVATION (FREE VACUUMS)  
SCALE: 1/16" = 1'-0"



7 EAST ELEVATION (FREE VACUUMS)  
SCALE: 1/16" = 1'-0"

Drawing Revisions:

No.	DATE	BY	REMARKS
1	1-18-16	CRW	FOR PERMIT
	4-28-16	CRW	PER TOWN COMMENTS

Property No: 1820  
Status: REVIEW & APPROVAL  
Area: -

Property: Delta Sonic  
1415 Ogden Ave  
Downers Grove, IL 60515

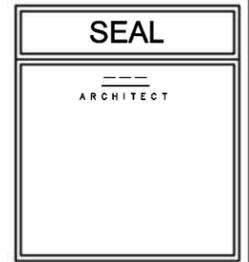
Tenant: Delta Sonic Car Wash

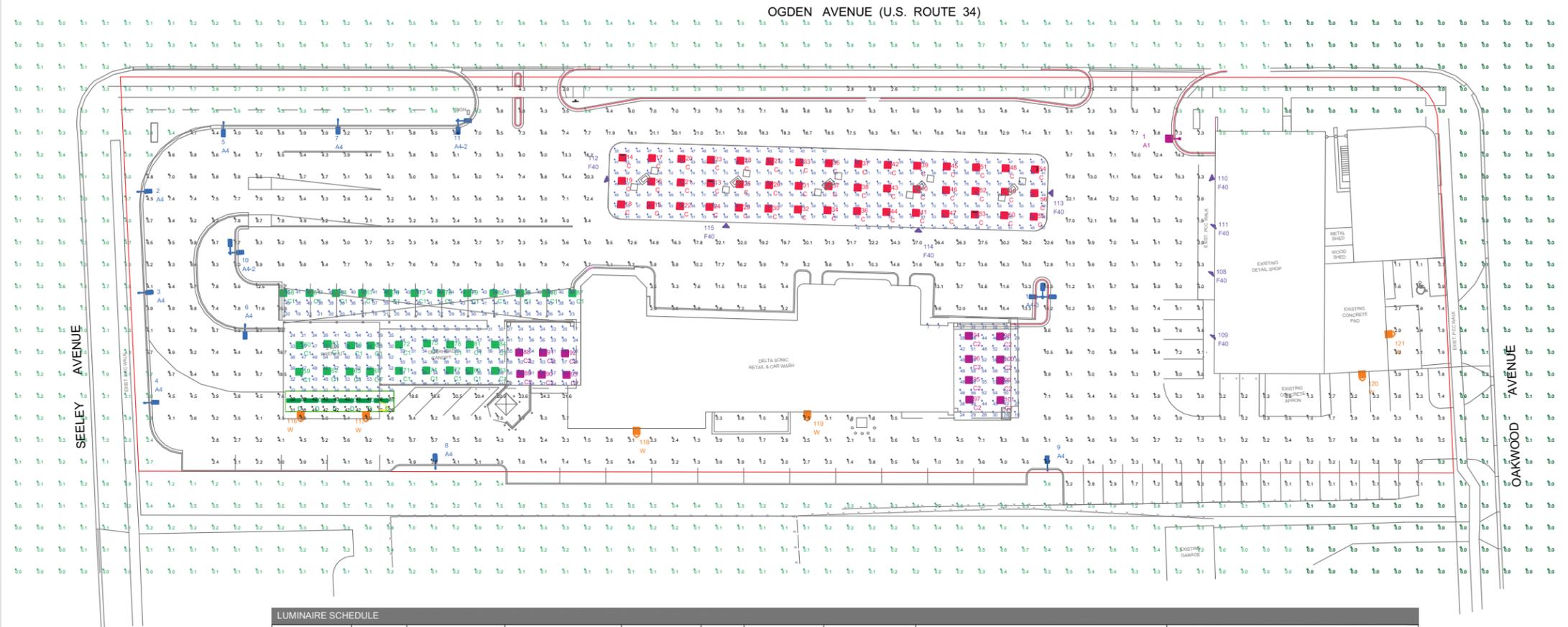
Title: OVERALL SITE ELEVATIONS

**DELTA SONIC CAR WASH**  
570 Delaware Ave.  
Buffalo, N.Y. 14202  
716.541.2100

Drawn By: MG  
Checked By: JAR  
Date: 10-14-14

Drawing No. A-2.0





SYMBOL	QTY	LABEL	ARRANGEMENT	LUMENS	LLF	ARR. WATTS	TOTAL WATTS	MANUFACTURER	DESCRIPTION
	1	A1	SINGLE	8800	1.000	134	134	CREE, INC.	STR-LWY-2MB-AA-06-E-UL-XX-700-57K
	8	A4	SINGLE	11683	1.040	134	1072	Cree Inc.	STR-LWY-4M-AA-06-E-UL-XX-700
	2	A4-2	2 @ 90 DEGREES	11683	1.040	268	536	Cree Inc.	STR-LWY-4M-AA-06-E-UL-XX-700
	1	A4-3	3 @ 90 DEGREES	11683	1.040	402	402	Cree Inc.	STR-LWY-4M-AA-06-E-UL-XX-700
	45	C	SINGLE	13246	1.040	125.5	5647.5	Cree Lighting	CPY250-A-DM-D-B-UL + XA-BXCC9002&
	30	C1	SINGLE	8356	1.040	81.6	2448	Cree Lighting	CPY250-A-DM-D-A-UL
	14	C2	SINGLE	8356	1.040	81.6	1142.4	Cree Lighting	CPY250-A-DM-D-A-UL + XA-BXCC9002&
	6	D	SINGLE	6593	1.040	63.1	378.6	Cree Inc.	WS4-63L-57K-10V-FD
	8	F40	SINGLE	13160	1.040	134	1072	CREE, INC.	FLD-EDG-40-AA-06-E-UL-XX-700
	6	W	SINGLE	5893	1.040	68	408	CREE, INC.	SEC-EDG-3M-WM-04-E-UL-XX-525

FOOTCANDLE LEVELS CALCULATED AT GRADE USING INITIAL LUMEN VALUES					
LABEL	AVG	MAX	MIN	AVG/MIN	MAX/MIN
CANOPY	59.75	79	23	2.60	3.43
CANOPY 1	41.14	58	7	5.88	8.29
CANOPY 2	44.14	63	17	2.60	3.71
EQUIPMENT ROOM	38.35	43	26	1.48	1.65
OBERHEAD CANOPY	47.57	63	25	1.90	2.52
PAVED AREA	4.87	31.7	0.0	N.A.	N.A.
PREP HUT	45.62	63	22	2.07	2.86
UNDEFINED AREA	0.70	6.5	0.0	N.A.	N.A.
VACUUM CANOPY	36.35	49	19	1.91	2.58

NOTE:  
 - FOOTCANDLE LEVELS CALCULATED AT GRADE USING INITIAL LUMEN VALUES  
 - POLE MOUNTED FIXTURES ARE MOUNTED ON A 20FT POLE ATOP A 1 FT HIGH CONCRETE BASE.

LUM. NO.	LABEL	MTG. HT.	TILT
1	A1	21	0
2	A4	21	0
3	A4	21	0
4	A4	21	0
5	A4	21	0
6	A4	21	0
7	A4	21	0
8	A4	21	0
9	A4	21	0
10	A4-2	21	0
11	A4-2	21	0
12	A4-3	21	0
13	C	15.5	0
14	C	15.5	0
15	C	15.5	0
16	C	15.5	0
17	C	15.5	0
18	C	15.5	0
19	C	15.5	0
20	C	15.5	0
21	C	15.5	0
22	C	15.5	0
23	C	15.5	0
24	C	15.5	0
25	C	15.5	0
26	C	15.5	0
27	C	15.5	0
28	C	15.5	0
29	C	15.5	0
30	C	15.5	0
31	C	15.5	0
32	C	15.5	0
33	C	15.5	0
34	C	15.5	0
35	C	15.5	0
36	C	15.5	0
37	C	15.5	0
38	C	15.5	0
39	C	15.5	0
40	C	15.5	0
41	C	15.5	0
42	C	15.5	0
43	C	15.5	0
44	C	15.5	0
45	C	15.5	0
46	C	15.5	0
47	C	15.5	0
48	C	15.5	0
49	C	15.5	0
50	C	15.5	0
51	C	15.5	0
52	C	15.5	0
53	C	15.5	0
54	C	15.5	0
55	C	15.5	0
56	C	15.5	0
57	C	15.5	0
58	C1	10	0
59	C1	12	0
60	C1	12	0

LUM. NO.	LABEL	MTG. HT.	TILT
61	C1	10	0
62	C1	12	0
63	C1	12	0
64	C1	10	0
65	C1	12	0
66	C1	12	0
67	C1	10	0
68	C1	12	0
69	C1	12	0
70	C1	10	0
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74	C1	12	0
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83	C1	12	0
84	C1	12	0
85	C1	10	0
86	C1	10	0
87	C1	10	0
88	C2	12	0
89	C2	12	0
90	C2	12	0
91	C2	12	0
92	C2	12	0
93	C2	12	0
94	C2	12	0
95	C2	12	0
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97	C2	12	0
98	C2	12	0
99	C2	12	0
100	C2	12	0
101	C2	12	0
102	D	12	0
103	D	12	0
104	D	12	0
105	D	12	0
106	D	12	0
107	D	12	0
108	F40	21	73.159
109	F40	21	73.159
110	F40	21	59.174
111	F40	21	73.159
112	F40	21	59.174
113	F40	21	59.174
114	F40	21	59.174
115	F40	21	59.174
116	W	15	0
117	W	15	0
118	W	15	0
119	W	15	0
120	W	15	0
121	W	15	0

REV.	BY	DATE	DESCRIPTION
R1	TAS	11/13/15	UPDATED SITE PLAN AND REVISED FIXTURE LOCATIONS
R2	TAS	4/22/16	UPDATED SITE PLAN

PERSONS USING THIS PROGRAM ARE ADVISED THAT THIS PROGRAM MAY CONTAIN ERRORS WHICH RED LEONARD ASSOCIATES, INC. OR ITS SOFTWARE PROVIDER HAVE NOT OBSERVED. IN ADDITION, THE USE OF THIS PROGRAM TO AID IN LAYOUT OF LIGHTING AND ESTIMATING MATERIAL QUANTITIES IS NOT INTENDED TO REMOVE THE RESPONSIBILITY OF THE USER TO VERIFY THE COMPLETENESS OF ANY BILL OF MATERIALS AND FULL ACCORDANCE WITH ALL LOCAL, STATE, OR FEDERAL STATUTES, REGULATIONS OR OTHER REQUIREMENTS, OR THE REQUIREMENTS OF ANY INSURANCE GROUP, ORGANIZATION OR CARRIER REGARDING LUMINAIRES AND THEIR APPLICATION.

FOR INFORMATIONAL AND ILLUSTRATION PURPOSES ONLY. ALL PRODUCT, SERVICE AND CORPORATE NAMES ARE THE PROPERTY OF THEIR RESPECTIVE OWNERS. PRODUCT SPECIFICATIONS AND QUANTITIES MAY VARY. THESE DOCUMENTS ARE THE PROPERTY OF RED LEONARD ASSOCIATES, INC. ANY USE OF THESE DOCUMENTS WITHOUT THE WRITTEN CONSENT OF JAYNE A. LEONARD OF RED LEONARD ASSOCIATES, INC. IS STRICTLY PROHIBITED.

ILLUMINATION RESULTS SHOWN ON THIS LIGHTING APPLICATION ARE BASED ON PROJECT PARAMETERS PROVIDED BY THE MANUFACTURER LISTED UNDER "MANUFACTURER" AND THE LUMINAIRE TEST PROCEDURES CONDUCTED UNDER THESE PARAMETERS. THESE PARAMETERS MAY AFFECT FIELD RESULTS. THE CUSTOMER IS RESPONSIBLE FOR VERIFYING COMPLIANCE WITH ANY APPLICABLE ELECTRICAL, LIGHTING, OR ENERGY CODE.

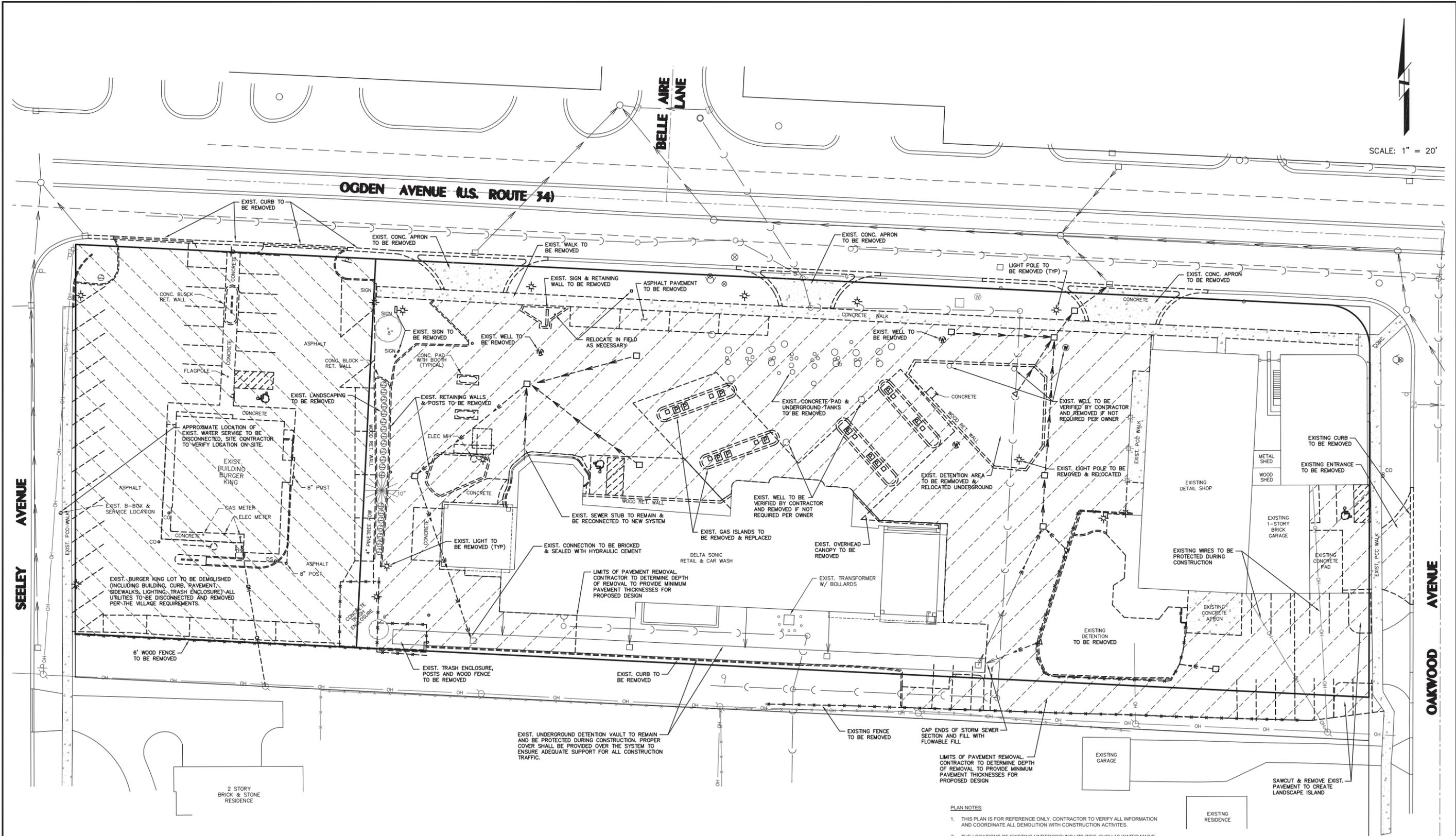
SCALE: 1" = 30'  
 LAYOUT BY: TAS  
 DWG SIZE: D  
 DATE: 1/22/15

PROJECT NAME:  
**DELTA SONIC**  
**DOWNERS GROVE, IL**

DRAWING NUMBER:  
**RL-2553-S1-R2**

**rla**

SCALE: 1" = 20'



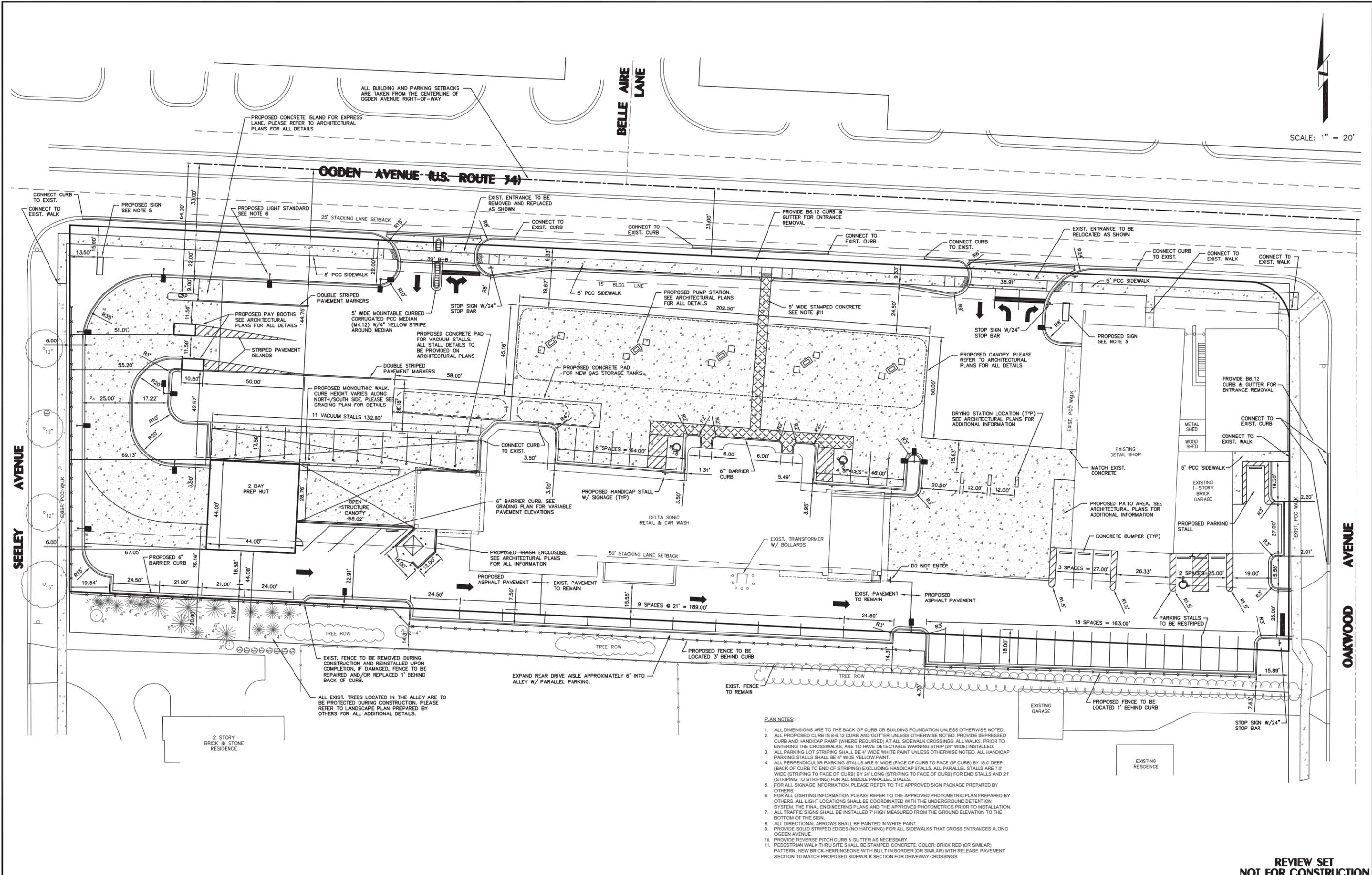
- PLAN NOTES:**
1. THIS PLAN IS FOR REFERENCE ONLY. CONTRACTOR TO VERIFY ALL INFORMATION AND COORDINATE ALL DEMOLITION WITH CONSTRUCTION ACTIVITIES.
  2. THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES, SUCH AS WATER MAINS, SEWERS, GAS LINES, ELECTRIC LINES, TELEPHONE LINES, ETC. AS SHOWN ON THE PLANS HAVE BEEN DETERMINED FROM THE BEST AVAILABLE INFORMATION AND IS GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. HOWEVER, THE ENGINEER AND OWNER DO NOT ASSUME RESPONSIBILITY FOR THE ACCURACY OF THE LOCATIONS SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT ALL UTILITY COMPANIES AND THEIR FACILITIES SHALL BE LOCATED PRIOR TO ANY WORK.
  3. ALL DEMOLITION TO BE COORDINATED WITH THE VILLAGE OF DOWNERS GROVE AND SHALL BE IN ACCORDANCE WITH THE VILLAGE REQUIREMENTS.
  4. EXISTING BUILDING SANITARY SEWER SERVICE @ 1445 OGDEN AVENUE MUST BE DISCONNECTED FROM THE PUBLIC SEWER MAIN PRIOR TO THE DEMOLITION OF THE BUILDING. CONTRACTOR TO SUBMIT SEWER PERMIT APPLICATION FOR DISCONNECTION.

**REVIEW SET  
NOT FOR CONSTRUCTION**

<p><b>INTECH CONSULTANTS, INC.</b> 1889 UNIVERSITY LANE, SUITE D LISLE, ILLINOIS ILLINOIS REGISTRATION No. 184-001040</p>	<p><b>ENGINEERS SURVEYORS</b></p>	<p><b>BENDERSON DEVELOPMENT CO.</b> 570 DELAWARE AVENUE BUFFALO, NY 14202</p>	<p><b>DELTA SONIC CAR WASH</b> 145 OGDEN AVENUE DOWNERS GROVE, ILLINOIS</p>	<p><b>REMOVAL PLAN</b></p>	<p>6-7-16 3</p>	<p>VILLAGE REVIEW COMMENT SET #1 (6-2-16) VILLAGE/IDOT REVIEW / NEW LAYOUT PER SANITARY/VILLAGE/IDOT REVIEWS</p>	<p>S.P. S.P. S.P.</p>	<p><b>DRAWN</b> SJP</p>	<p><b>SCALE</b> 1"=20'</p>	<p><b>SHEET</b> 3</p>	<p><b>OF</b> 20</p>	
					<p>4-22-16 2</p>							<p><b>DESIGNED</b> LGG</p>
					<p><b>DATE</b></p>	<p><b>NO.</b></p>	<p><b>REVISION</b></p>	<p><b>BY</b></p>				

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SCALE: 1" = 20'



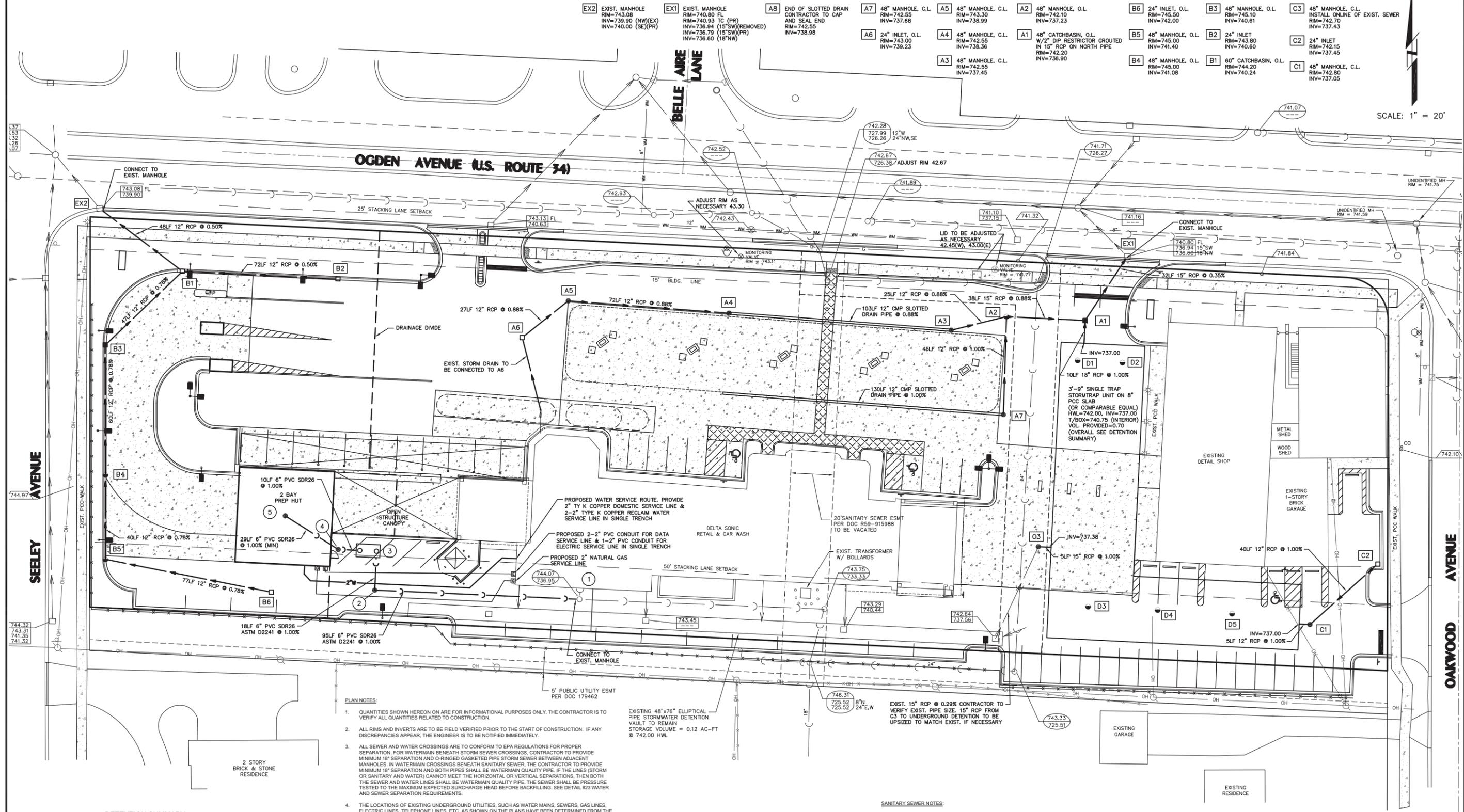
- PLAN NOTES:**
1. ALL DIMENSIONS ARE TO THE BACK OF CURB OR BUILDING FOUNDATION UNLESS OTHERWISE NOTED.
  2. ALL PROPOSED CURB IS 6-12 CURB AND GUTTER UNLESS OTHERWISE NOTED. PROVIDE DEPRESSED CURB AND HANDICAP RAMP (WHERE REQUIRED) AT ALL SIDEWALK CROSSINGS. ALL WALKS, PRIOR TO ENTERING THE CROSSWALKS, ARE TO HAVE DETECTABLE WARNING STRIP (24" WIDE) INSTALLED.
  3. ALL PARKING LOT STRIPING SHALL BE 4" WIDE WHITE PAINT UNLESS OTHERWISE NOTED. ALL HANDICAP PARKING STALLS SHALL BE 4" WIDE YELLOW PAINT.
  4. ALL PERPENDICULAR PARKING STALLS ARE 9' WIDE (FACE OF CURB TO FACE OF CURB) BY 18.0' DEEP (BACK OF CURB TO END OF STRIPING) EXCLUDING HANDICAP STALLS. ALL PARALLEL STALLS ARE 7.0' WIDE (STRIPING TO FACE OF CURB) BY 24' LONG (STRIPING TO FACE OF CURB) FOR END STALLS AND 21' (STRIPING TO STRIPING) FOR ALL MIDDLE PARALLEL STALLS.
  5. FOR ALL SIGNAGE INFORMATION, PLEASE REFER TO THE APPROVED SIGN PACKAGE PREPARED BY OTHERS.
  6. FOR ALL LIGHTING INFORMATION PLEASE REFER TO THE APPROVED PHOTOMETRIC PLAN PREPARED BY OTHERS. ALL LIGHT LOCATIONS SHALL BE COORDINATED WITH THE UNDERGROUND DETENTION SYSTEM, THE FINAL ENGINEERING PLANS AND THE APPROVED PHOTOMETRICS PRIOR TO INSTALLATION.
  7. ALL TRAFFIC SIGNS SHALL BE INSTALLED 7' HIGH MEASURED FROM THE GROUND ELEVATION TO THE BOTTOM OF THE SIGN.
  8. ALL DIRECTIONAL ARROWS SHALL BE PAINTED IN WHITE PAINT.
  9. PROVIDE SOLID STRIPED EDGES (NO HATCHING) FOR ALL SIDEWALKS THAT CROSS ENTRANCES ALONG OGDEN AVENUE.
  10. PROVIDE REVERSE PITCH CURB & GUTTER AS NECESSARY.
  11. PEDESTRIAN WALK THRU SITE SHALL BE STAMPED CONCRETE. COLOR: BRICK RED (OR SIMILAR) PATTERN: NEW BRICK-HERRINGBONE WITH BUILT IN BORDER (OR SIMILAR) WITH RELEASE. PAVEMENT SECTION TO MATCH PROPOSED SIDEWALK SECTION FOR DRIVEWAY CROSSINGS.

REVIEW SET  
NOT FOR CONSTRUCTION

<p><b>INTECH CONSULTANTS, INC.</b> 1889 UNIVERSITY LANE, SUITE D LISLE, ILLINOIS ILLINOIS REGISTRATION No. 184-001040</p>	<p><b>ENGINEERS SURVEYORS</b></p>	<p><b>BENDERSON DEVELOPMENT CO.</b> 570 DELAWARE AVENUE BUFFALO, NY 14202</p>	<p><b>DELTA SONIC CAR WASH</b> 1415 OGDEN AVENUE DOWNERS GROVE, ILLINOIS</p>	<p><b>GEOMETRIC PLAN</b></p>	<p>6-7-16 3 VILLAGE REVIEW COMMENT SET #1 (6-2-16)</p>	<p>DESIGNED DATE BY</p>	<p>SCALE 1"=20'</p>	<p>SHEET 4 OF 20</p>
					<p>4-22-16 2 VILLAGE/IDOT REVIEW / NEW LAYOUT</p>			

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**STORM SEWER** (ALL RIMS ARE PAVEMENT ELEVATIONS)



SCALE: 1" = 20'

**PLAN NOTES:**

- QUANTITIES SHOWN HEREON ARE FOR INFORMATIONAL PURPOSES ONLY. THE CONTRACTOR IS TO VERIFY ALL QUANTITIES RELATED TO CONSTRUCTION.
- ALL RIMS AND INVERTS ARE TO BE FIELD VERIFIED PRIOR TO THE START OF CONSTRUCTION. IF ANY DISCREPANCIES APPEAR, THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY.
- ALL SEWER AND WATER CROSSINGS ARE TO CONFORM TO EPA REGULATIONS FOR PROPER SEPARATION. FOR WATERMAIN BENEATH STORM SEWER CROSSINGS, CONTRACTOR TO PROVIDE MINIMUM 18" SEPARATION AND O-RINGED GASKETED PIPE STORM SEWER BETWEEN ADJACENT MANHOLES. IN WATERMAIN CROSSINGS BENEATH SANITARY SEWER, THE CONTRACTOR TO PROVIDE MINIMUM 18" SEPARATION AND BOTH PIPES SHALL BE WATERMAIN QUALITY PIPE. IF THE LINES (STORM OR SANITARY AND WATER) CANNOT MEET THE HORIZONTAL OR VERTICAL SEPARATIONS, THEN BOTH THE SEWER AND WATER LINES SHALL BE WATERMAIN QUALITY PIPE. THE SEWER SHALL BE PRESSURE TESTED TO THE MAXIMUM EXPECTED SURCHARGE HEAD BEFORE BACKFILLING. SEE DETAIL #23 WATER AND SEWER SEPARATION REQUIREMENTS.
- THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES, SUCH AS WATER MAINS, SEWERS, GAS LINES, ELECTRIC LINES, TELEPHONE LINES, ETC. AS SHOWN ON THE PLANS HAVE BEEN DETERMINED FROM THE BEST AVAILABLE INFORMATION AND IS GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. HOWEVER, THE ENGINEER AND OWNER DO NOT ASSUME RESPONSIBILITY FOR THE ACCURACY OF THE LOCATIONS SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT ALL UTILITY COMPANIES AND THEIR FACILITIES SHALL BE LOCATED PRIOR TO ANY WORK.
- PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL EXPOSE ANY UTILITIES OR OTHER OBSTRUCTIONS TO BE CROSSED BY THE PROPOSED SEWER, AND SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THERE ARE ANY CONFLICTS WITH THE PROPOSED SEWER GRADE.
- STORM INFORMATION FOR STRUCTURES D1-D5 WILL BE DETAILED ON THE CONSTRUCTION DOCUMENTS PREPARED FOR THE UNDERGROUND DETENTION SYSTEM AND WILL BE AVAILABLE UNDER SEPARATE COVER.
- FOR ALL LIGHTING INFORMATION PLEASE REFER TO THE APPROVED PHOTOMETRIC PLAN PREPARED BY OTHERS. ALL LIGHT LOCATIONS SHALL BE COORDINATED WITH THE UNDERGROUND DETENTION SYSTEM. THE FINAL ENGINEERING PLANS AND THE APPROVED PHOTOMETRICS PRIOR TO INSTALLATION.

EXISTING 48"x76" ELLIPTICAL PIPE STORMWATER DETENTION VAULT TO REMAIN STORAGE VOLUME = 0.12 AC-FT @ 742.00 HWL.

**SANITARY SEWER NOTES:**

- ALL SANITARY STRUCTURES SHALL COMPLY WITH THE SANITARY DISTRICTS STANDARDS FOR MANHOLE CONSTRUCTION AS FOLLOWS:
- THE STRUCTURES SHALL HAVE WATER TIGHT, RUBBER PIPE SEALS.
  - THE STRUCTURES SHALL HAVE THE APPROPRIATE FRAME & COVER.
  - FRAMES AND ADJUSTING RINGS SHALL BE SEALED WITH MASTIC GASKET AND EXTERNAL CHIMNEY SEALS.
  - ADJUSTING RINGS SHALL NOT EXCEED 8" IN HEIGHT.
  - THE STRUCTURES SHALL PASS THE REQUIRED MANHOLE VACUUM TEST.

**DETENTION SUMMARY**

TOTAL VOLUME REQUIRED = 0.64 AC-FT  
 VOLUME PROVIDED STORMTRAP = 0.58 AC-FT  
 VOL. PROVIDED ELLIPTICAL PIPE = 0.12 AC-FT  
 TOTAL VOLUME PROVIDED = 0.70 AC-FT  
 HWL = 742.00  
 RESTRICTOR PROVIDED = 2" DIP @ A1

**SANITARY SEWER**

- 1 EXIST. SANITARY MANHOLE RIM=744.07 INV=737.00 (PR) RIM=745.60 INV=736.95 (EX)
- 2 48" MANHOLE RIM=745.00 INV=737.95
- 3 OIL SEPARATOR KISTNER MODEL OS-1000T-N-0 (OR APPROVED EQUAL) RIM=745.20 INV=738.38 (W) INV=738.13 (S)
- 4 48" MANHOLE (GRIT CHAMBER) RIM=745.60 INV=745.40 INV=738.48
- 5 SEDIMENT TRAP (VERIFY W/ ARCHITECTURAL PLANS) (SEE STD. DETAIL 29) RIM=745.40 INV=738.78

**REVIEW SET NOT FOR CONSTRUCTION**

**INTECH CONSULTANTS, INC.**  
 1889 UNIVERSITY LANE, SUITE D  
 LISLE, ILLINOIS  
 ILLINOIS REGISTRATION No. 184-001040

**ENGINEERS SURVEYORS**

**BENDERSON DEVELOPMENT CO.**  
 570 DELAWARE AVENUE  
 BUFFALO, NY 14202

**DELTA SONIC CAR WASH**  
 145 OGDEN AVENUE  
 DOWNERS GROVE, ILLINOIS

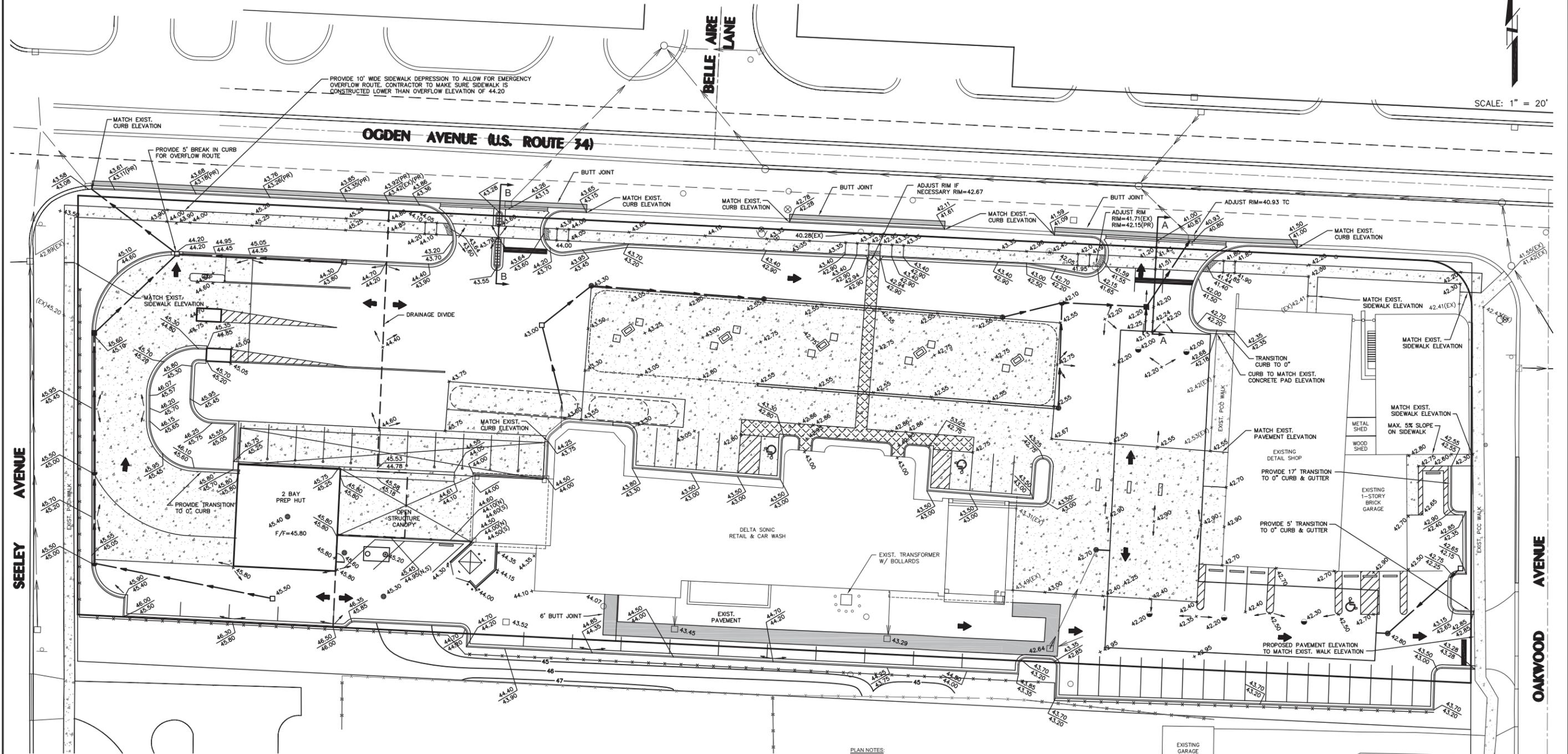
UTILITY PLAN

DATE	NO.	REVISION
6-7-16	3	VILLAGE REVIEW COMMENT SET #1 (6-2-16)
4-22-16	2	VILLAGE/IDOT REVIEW / NEW LAYOUT
1-27-16	1	PER SANITARY/VILLAGE/IDOT REVIEWS

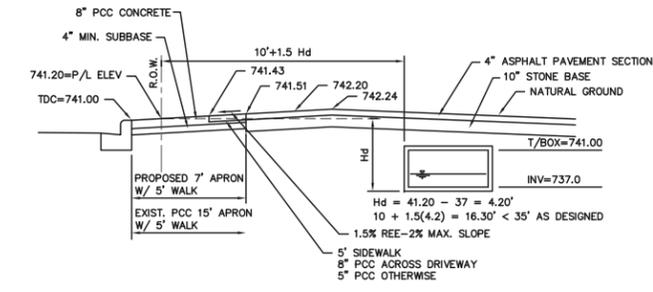
DRAWN	SCALE	SHEET	OF
SJP	1"=20'	5	20
DESIGNED	DATE	PROJECT NO.	
LGG	1-28-15	2002-028A	

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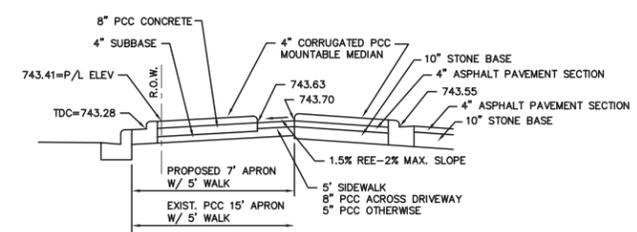
SCALE: 1" = 20'



- PLAN NOTES:**
- EXISTING GRADES AND IMPROVEMENTS ARE SHOWN FROM THE BEST INFORMATION AVAILABLE AND MUST BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. ANY DISCREPANCY WITH THE PLAN SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER.
  - ALL GRADES ARE FINISHED GRADE ELEVATIONS.
  - REVERSE PITCH CURB & GUTTER AS NECESSARY TO PROVIDE PROPER DRAINAGE. FOR FURTHER INFORMATION REGARDING REVERSE PITCH GUTTER, PLEASE REFER TO "CONCRETE CURB AND GUTTER DETAIL".
  - QUANTITIES SHOWN HEREON ARE FOR INFORMATIONAL PURPOSES ONLY. THE CONTRACTOR IS TO VERIFY ALL QUANTITIES RELATED TO CONSTRUCTION.
  - ALL RIMS AND INVERTS ARE TO BE FIELD VERIFIED PRIOR TO THE START OF CONSTRUCTION. IF ANY DISCREPANCIES APPEAR, THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY.
  - ALL SEWER AND WATER CROSSINGS ARE TO CONFORM TO EPA REGULATIONS FOR PROPER SEPARATION.
  - THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES, SUCH AS WATER MAINS, SEWERS, GAS LINES, ELECTRIC LINES, TELEPHONE LINES, ETC. AS SHOWN ON THE PLANS HAVE BEEN DETERMINED FROM THE BEST AVAILABLE INFORMATION AND IS GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. HOWEVER, THE ENGINEER AND OWNER DO NOT ASSUME RESPONSIBILITY FOR THE ACCURACY OF THE LOCATIONS SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT ALL UTILITY COMPANIES AND THEIR FACILITIES SHALL BE LOCATED PRIOR TO ANY WORK.
  - PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL EXPOSE ANY UTILITIES OR OTHER OBSTRUCTIONS TO BE CROSSED BY THE PROPOSED SEWER, AND SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THERE ARE ANY CONFLICTS WITH THE PROPOSED SEWER GRADE.
  - ALL FACILITIES FOR THIS DEVELOPMENT SHALL CONFORM TO THE ILLINOIS ACCESSIBILITY CODE.



SECTION A-A (CASE VI IDOT DETAIL)



SECTION B-B

**INTECH CONSULTANTS, INC.**  
 1889 UNIVERSITY LANE, SUITE D  
 LISLE, ILLINOIS  
 ILLINOIS REGISTRATION No. 184-001040

**ENGINEERS  
 SURVEYORS**

**BENDERSON DEVELOPMENT CO.**  
 570 DELAWARE AVENUE  
 BUFFALO, NY 14202

**DELTA SONIC CAR WASH**  
 145 OGDEN AVENUE  
 DOWNERS GROVE, ILLINOIS

**GRADING PLAN**

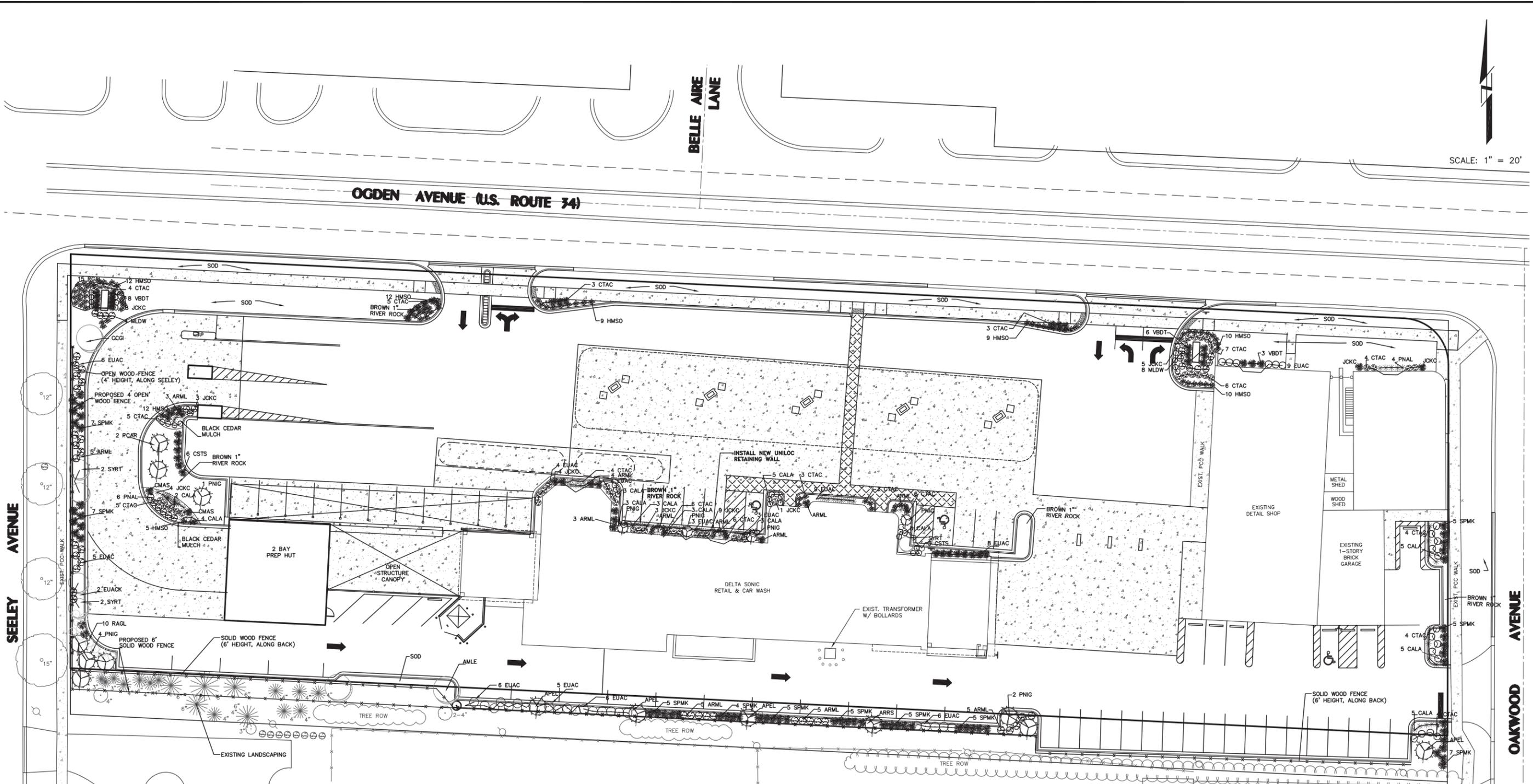
6-7-16	3	VILLAGE REVIEW COMMENT SET #1 (6-2-16)	SJP
4-22-16	2	VILLAGE/IDOT REVIEW / NEW LAYOUT	SJP
1-27-16	1	PER SANITARY/VILLAGE/IDOT REVIEWS	SJP
<b>DATE</b>	<b>NO.</b>	<b>REVISION</b>	<b>BY</b>

<b>DRAWN</b>	<b>SCALE</b>	<b>SHEET</b>
SJP	1"=20'	6
<b>DESIGNED</b>	<b>DATE</b>	<b>PROJECT NO.</b>
LGG	1-28-15	2002-028A

**REVIEW SET  
 NOT FOR CONSTRUCTION**

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SCALE: 1" = 20'



PLANT LIST

KEY	NAME	BOTANICAL/Common	SIZE	QUANTITY
APEL	ACER PLATANOIDES 'EMERALD LUSTRE'	EMERALD LUSTRE NORWAY MAPLE	2.5"	6
ARRS	ACER RUBRUM 'RED SUNSET'	RED SUNSET RED MAPLE	2.5"	2
COGI	CRATAEGUS CRUSGALLI INERMIS	THORNLESS COCKSPUR HAWTHORN	8" MS	2
SYRT	SYRINGA RETICULATA	JAPANESE TREE LILAC	8" MS	5
CELT	CELTIS OCCIDENTALIS	COMMON HACKBERRY	2.5"	0
FAAP	FRAXINUS AMERICANA 'AUTUMN PURPLE'	AUTUMN PURPLE WHITE ASH	2.5"	0
AMLE	AMELANCHIER LAEVIS	ALLEGHENY SERVICEBERRY	8"	1
FPPA	FRAXINUS PENNSYLVANICA 'PATMORE'	PATMORE GREEN ASH	2.5"	0
TCGS	TILLIA CORDATA 'GREENSPIRE'	GREENSPIRE LINDEN	2.5"	0
PCAR	PYRUS CALLERYANA 'ARISTOCRAT'	ARISTOCRAT PEAR	2.5"	3
MLDW	XXXXXXXXXXXX	DENSE YEW	24"	13
	CORNUS MAS	CORNELLANCHERRY DOGWOOD	6"	5
PNIG	PINUS NIGRA	AUSTRIAN PINE	6"	12
EUAC	EUONYMUS ALATUS COMPACTUS	ARROWWOOD VIBURNUM	3.5'	88
CSTS	CHAMENOMIS SPECIOSA 'TEXAS SCARLET'	TEXAS SCARLET QUINCE	24"	10
RAGL	RHUS AROMATICA 'GRO-LOW'	GRO-LOW SUMAC	24"	25
JCKC	JUNIPERUS CHINENSIS 'MALLAYS COMPACT'	MALLAY COMPACT JUNIPER	24"	50
VBDT	VIBURNUM DENTATUM	ARROWWOOD VIBURNUM	3'	32
CTAC	XXXXXXXXXXXX	CONCORD BARBERRY	24"	83
ARML	ARONIA MELANOCARPA	BLACK CHOKEBERRY	3'	41
SPMK	SYRINGA PATULA 'MISS KIM'	MISS KIM LILAC	36"	85
HMSO	HEMEROCALLIS 'STELLA DE ORO'	STELLA DE ORO DAYLILY	1 GAL	112
PNAL	XXXXXXXXXXXX	GOLD FLAME SPIREA	2 GAL	21
CALA	CALAMAGROSTIS STRICTA	FEATHER REED GRASS	3 GAL	33

NOTES:  
 1. THIS LANDSCAPE PLAN HAS BEEN PROVIDED TO INTECH CONSULTANTS AND HAS BEEN INCLUDED IN THE ENGINEERING PLANS FOR REFERENCE ONLY. PLEASE CONSULT WITH BENDERSON DEVELOPMENT COMPANY DIRECTLY WITH ANY QUESTIONS. IF ANY MODIFICATIONS ARE MADE INTECH CONSULTANTS SHOULD BE NOTIFIED IMMEDIATELY.  
 2. LANDSCAPE TO BE IN ACCORDANCE WITH VODG SECTION 28.8.020

SITE AREA - 108,110 SF  
 ALLEY AREA - 12,000 SF  
 TOTAL SITE AREA 120,110 SF  
 TOTAL STREET YARD AREA

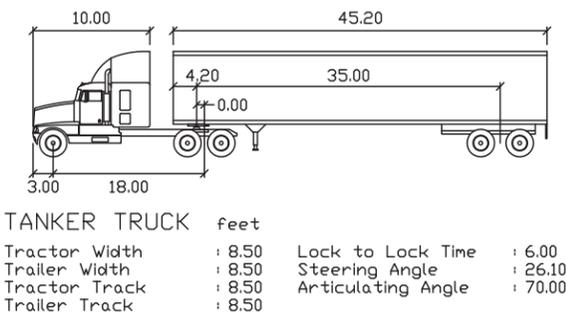
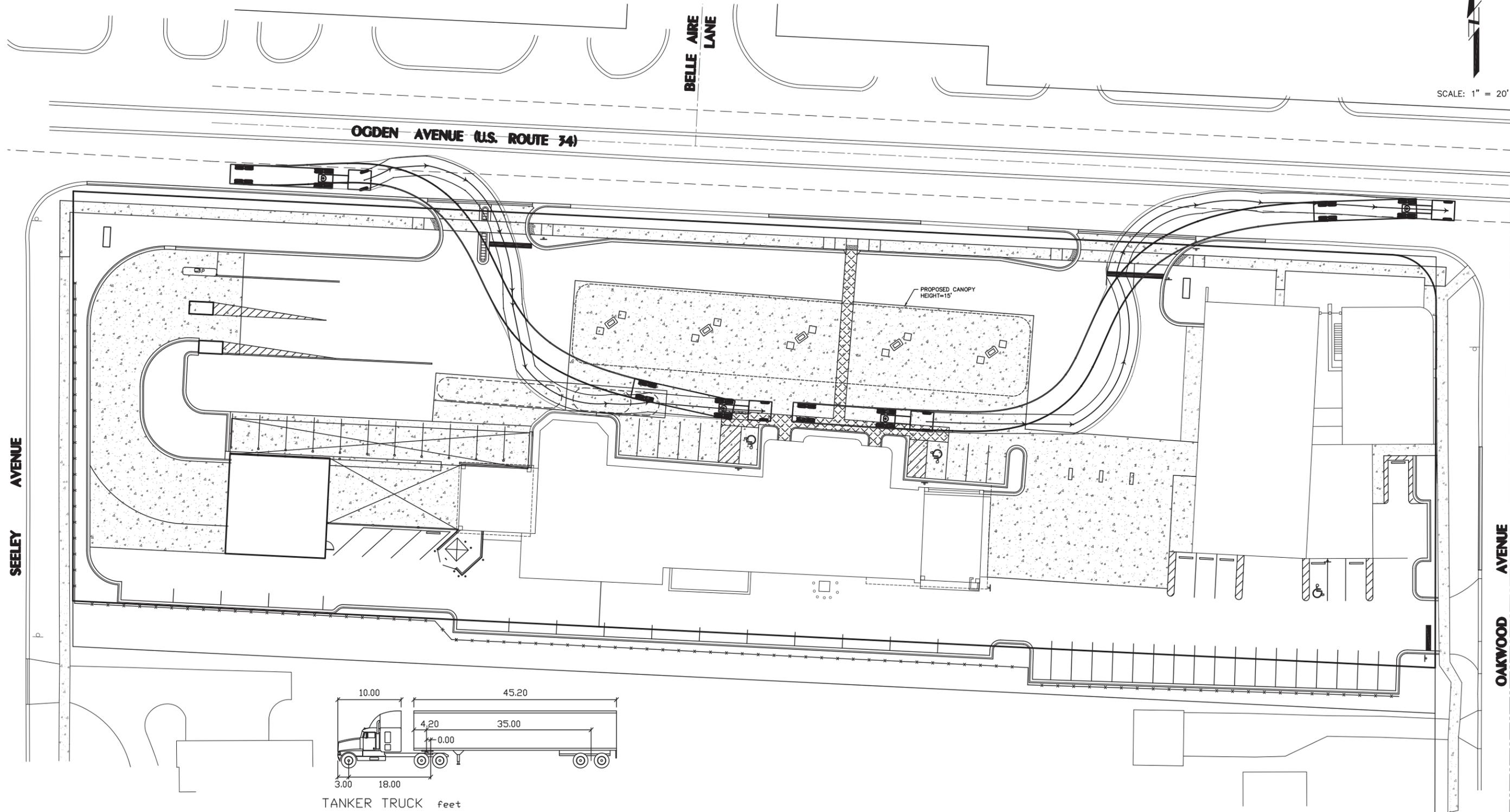
GREENSPACE = 9.5%  
 GREENSPACE = 70%  
 GREENSPACE = 15.5%  
 GREENSPACE = 8%

REVIEW SET  
 NOT FOR CONSTRUCTION

<p><b>INTECH CONSULTANTS, INC.</b>          1889 UNIVERSITY LANE, SUITE D          Lisle, Illinois          ILLINOIS REGISTRATION No. 184-001040</p>	<p><b>ENGINEERS          SURVEYORS</b></p>	<p>BENDERSON DEVELOPMENT CO.          570 DELAWARE AVENUE          BUFFALO, NY 14202</p>	<p><b>DELTA SONIC CAR WASH</b>          1475 OGDEN AVENUE          DOWNERS GROVE, ILLINOIS</p>	<p>LANDSCAPE          PLAN</p>	<p>6-7-16 3 VILLAGE REVIEW COMMENT SET #1 (6-2-16)</p>	<p>S.P.          S.P.          S.P.</p>	<p>SCALE          1"=20'</p>	<p>SHEET          12 OF 20</p>
					<p>4-22-16 2 VILLAGE/IDOT REVIEW / NEW LAYOUT</p>			
					<p>1-27-16 1 PER SANITARY/VILLAGE/IDOT REVIEWS</p>	<p>BY          LGG</p>	<p>DATE          1-28-15</p>	

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SCALE: 1" = 20'



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**INTECH CONSULTANTS, INC.**  
 1989 UNIVERSITY LANE, SUITE D  
 LISLE, ILLINOIS  
ILLINOIS REGISTRATION No. 184-001040

**ENGINEERS  
 SURVEYORS**

**BENDERSON DEVELOPMENT CO.**  
 570 DELAWARE AVENUE  
 BUFFALO, NY 14202

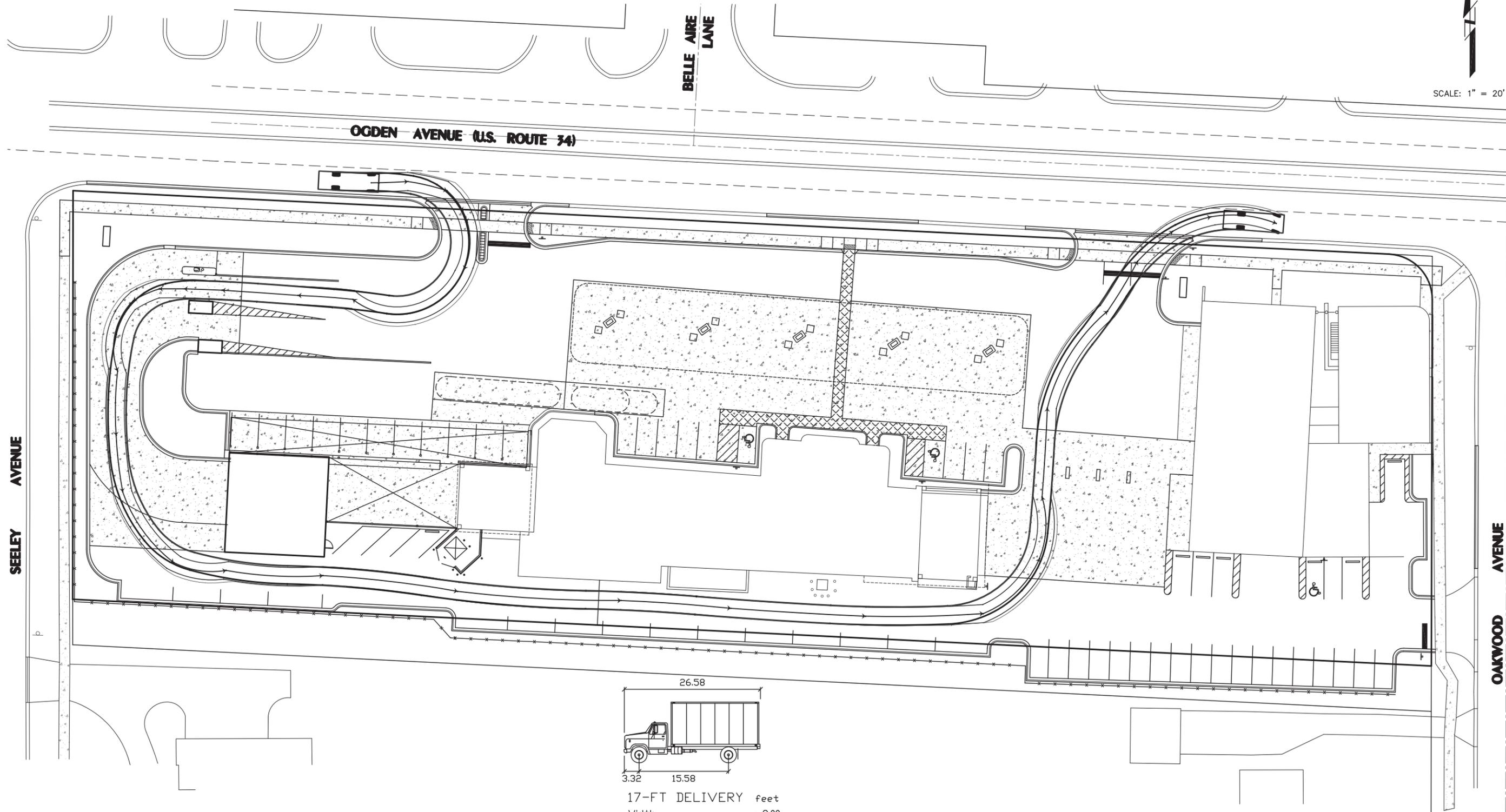
**DELTA SONIC CAR WASH**  
 1415 OGDEN AVENUE  
 DOWNERS GROVE, ILLINOIS

**AUTOTURN  
 EXHIBIT A  
 TANKER TRUCK**

DATE	NO.	REVISION
6-7-16	3	VILLAGE REVIEW COMMENT SET #1 (6-2-16)
4-22-16	2	VILLAGE/IDOT REVIEW / NEW LAYOUT
1-27-16	1	PER SANITARY/VILLAGE/IDOT REVIEWS

DRAWN	SCALE	SHEET	OF
SJP	1"=20'	14	20
DESIGNED	DATE	PROJECT NO.	
LGG	1-28-15	2002-028A	

SCALE: 1" = 20'



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**INTECH CONSULTANTS, INC.**  
 1989 UNIVERSITY LANE, SUITE D  
 LISLE, ILLINOIS  
ILLINOIS REGISTRATION No. 184-001040

**ENGINEERS  
 SURVEYORS**

**BENDERSON DEVELOPMENT CO.**  
 570 DELAWARE AVENUE  
 BUFFALO, NY 14202

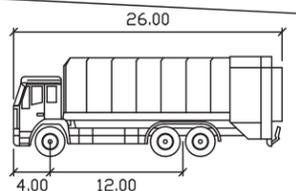
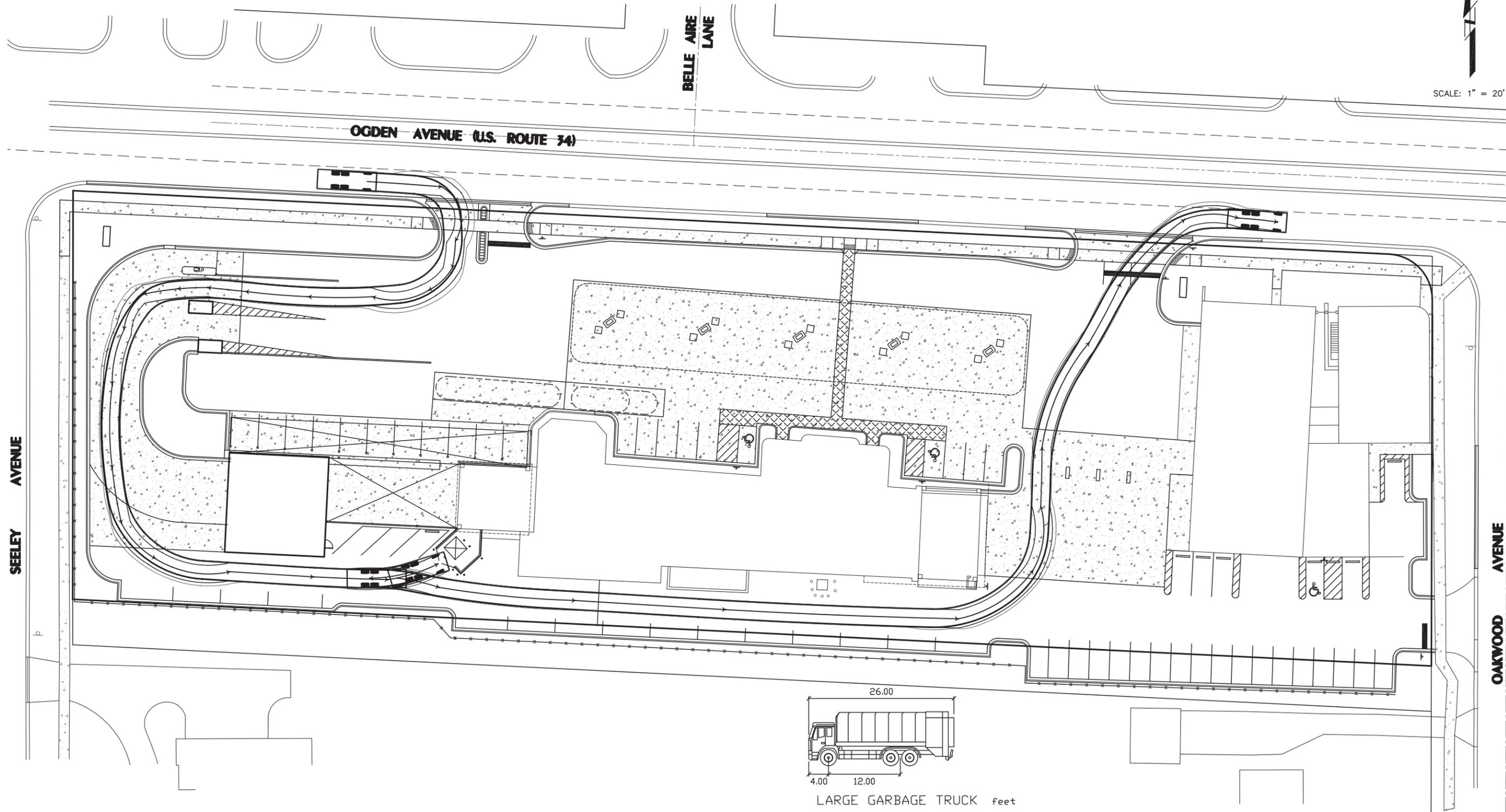
**DELTA SONIC CAR WASH**  
 145 OGDEN AVENUE  
 DOWNERS GROVE, ILLINOIS

**AUTOTURN  
 EXHIBIT B  
 DELIVERY TRUCK**

DATE	NO.	REVISION
6-7-16	3	VILLAGE REVIEW COMMENT SET #1 (6-2-16)
4-22-16	2	VILLAGE/IDOT REVIEW / NEW LAYOUT
1-27-16	1	PER SANITARY/VILLAGE/IDOT REVIEWS

DRAWN	SCALE	SHEET	OF
SJP	1"=20'	15	20
DESIGNED	DATE	PROJECT NO.	
LGG	1-28-15	2002-028A	

SCALE: 1" = 20'



LARGE GARBAGE TRUCK      feet  
 Width                               : 8.50  
 Track                                 : 8.00  
 Lock to Lock Time               : 6.00  
 Steering Angle                    : 40.00

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**INTECH CONSULTANTS, INC.**  
 1989 UNIVERSITY LANE, SUITE D      LISLE, ILLINOIS  
ILLINOIS REGISTRATION No. 184-001040

**ENGINEERS  
 SURVEYORS**

**BENDERSON DEVELOPMENT CO.**  
 570 DELAWARE AVENUE  
 BUFFALO, NY 14202

**DELTA SONIC CAR WASH**  
 145 OGDEN AVENUE  
 DOWNERS GROVE, ILLINOIS

**AUTOTURN  
 EXHIBIT C  
 GARBAGE TRUCK**

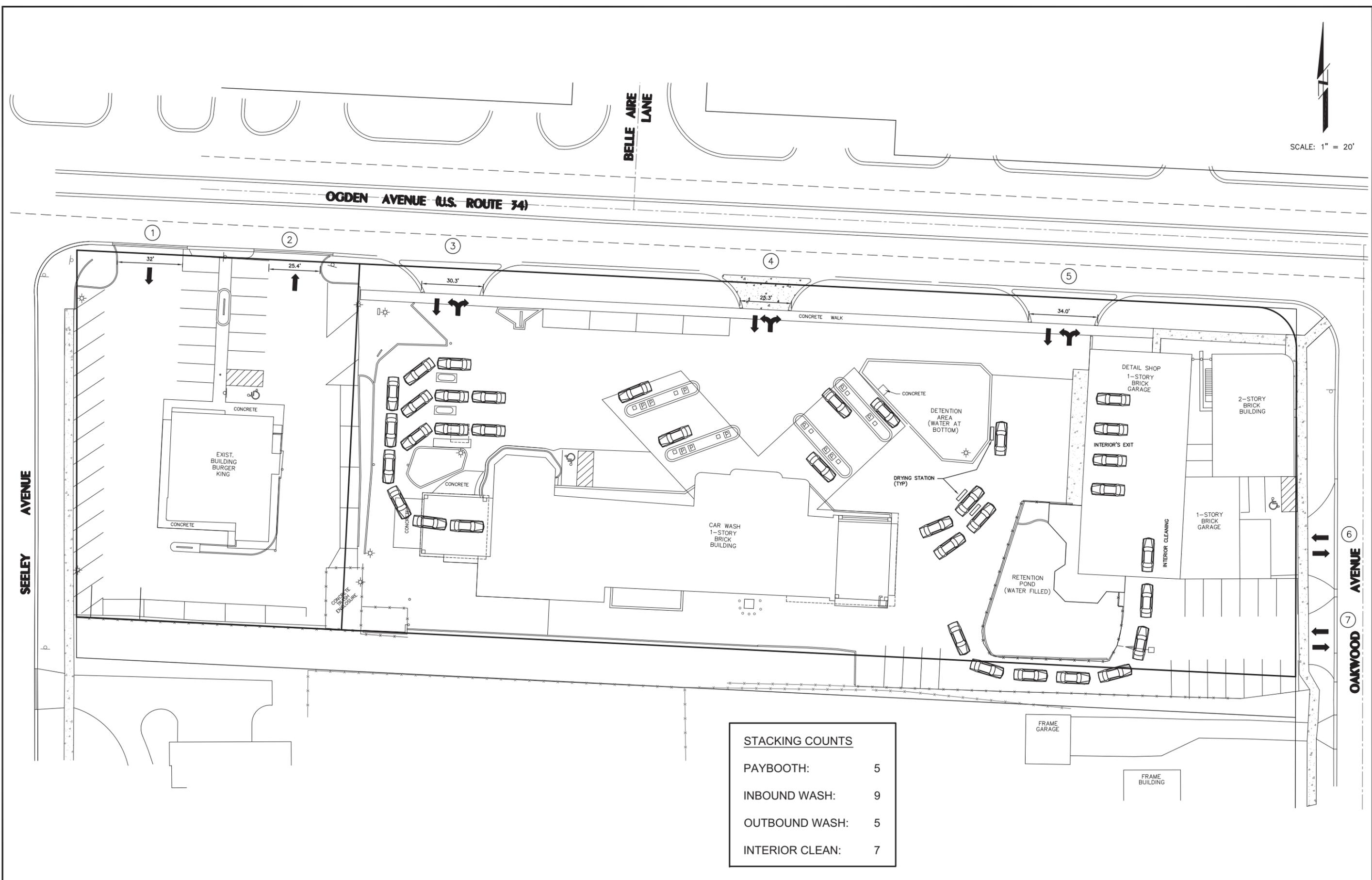
DATE	NO.	REVISION
6-7-16	3	VILLAGE REVIEW COMMENT SET #1 (6-2-16)
4-22-16	2	VILLAGE/IDOT REVIEW / NEW LAYOUT
1-27-16	1	PER SANITARY/VILLAGE/IDOT REVIEWS

**DRAWN**  
 SJP  
**DESIGNED**  
 LGG  
**BY**

**SCALE**  
 1"=20'  
**DATE**  
 1-28-15

**SHEET**    **OF**  
 16        20  
**PROJECT NO.**  
 2002-028A

SCALE: 1" = 20'

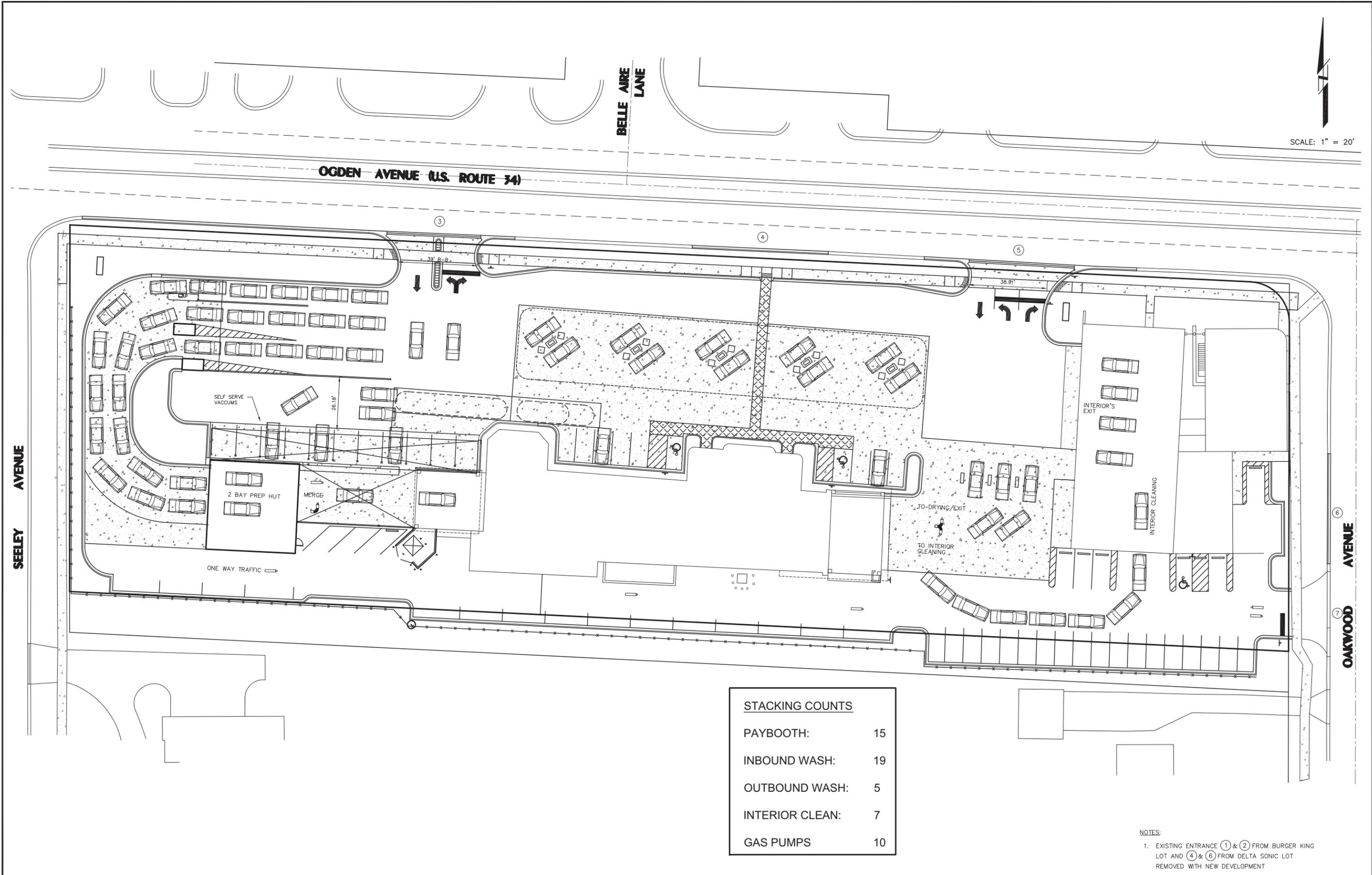


STACKING COUNTS	
PAYBOOTH:	5
INBOUND WASH:	9
OUTBOUND WASH:	5
INTERIOR CLEAN:	7

<b>INTECH CONSULTANTS, INC.</b> 1989 UNIVERSITY LANE, SUITE D <small>ILLINOIS REGISTRATION No. 184-001040</small> LISLE, ILLINOIS	<b>ENGINEERS SURVEYORS</b>	BENDERSON DEVELOPMENT CO. 570 DELAWARE AVENUE BUFFALO, NY 14202	<b>DELTA SONIC CAR WASH</b> 145 OGDEN AVENUE DOWNERS GROVE, ILLINOIS	<b>EXISTING TRAFFIC FLOW PLAN</b>	6-7-16    3 4-22-16    2 1-27-16    1 <b>DATE</b> <b>NO.</b>	VILLAGE REVIEW COMMENT SET #1 (6-2-16) VILLAGE/IDOT REVIEW / NEW LAYOUT PER SANITARY/VILLAGE/IDOT REVIEWS	S.P. S.P. S.P. <b>BY</b>	<b>DRAWN</b> SJP	<b>SCALE</b> 1"=20'	<b>SHEET</b> 18	<b>OF</b> 20
					<b>DESIGNED</b> LGG	<b>DATE</b> 1-25-15	<b>PROJECT NO.</b> 2002-028A				

M:\CAD Projects\2002-028A\2002-028A.dwg EXH-REV2.dwg EXIST-C 6/7/2016 10:11:30 AM SJP

SCALE: 1" = 20'



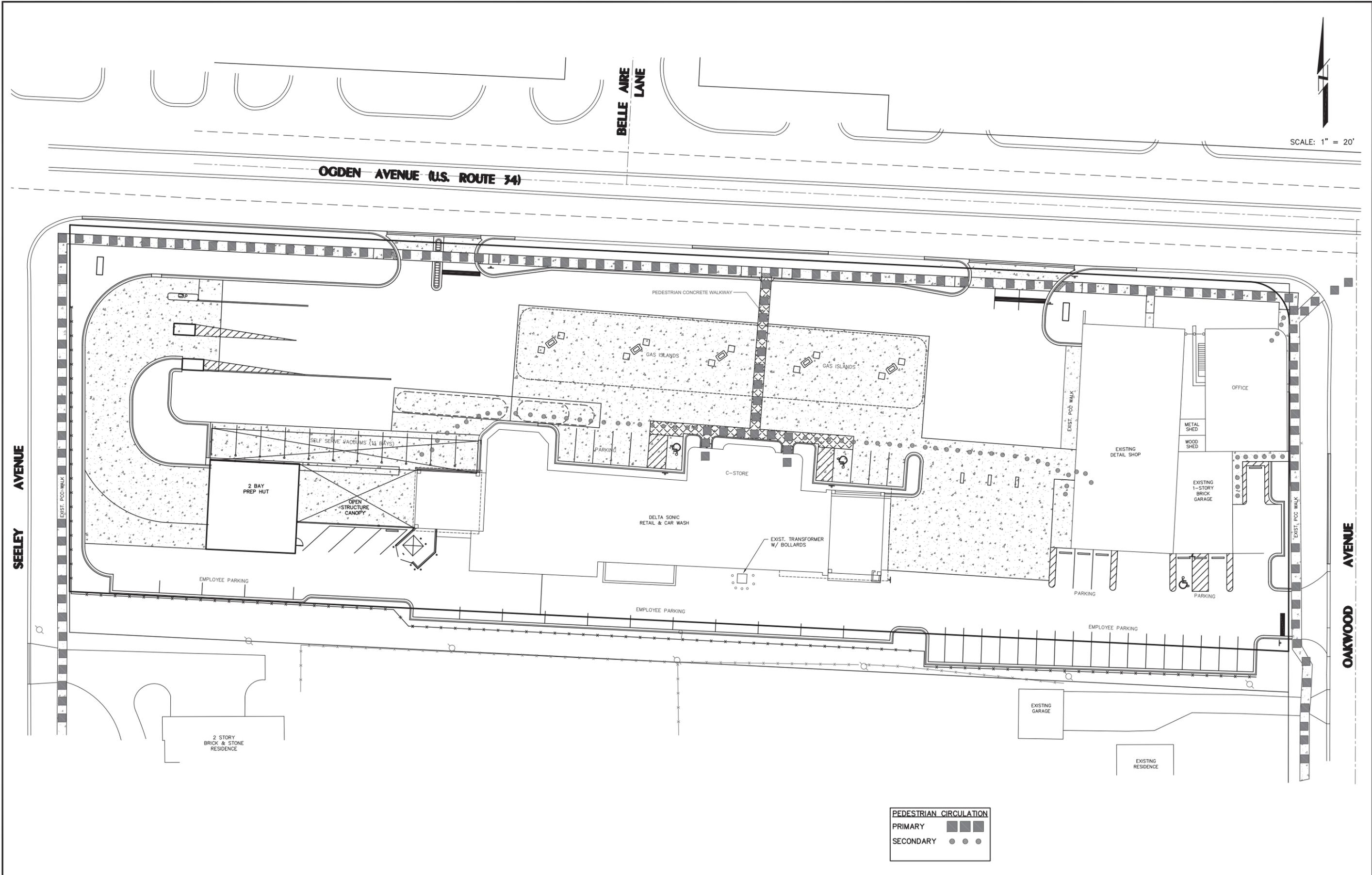
STACKING COUNTS	
PAYBOOTH:	15
INBOUND WASH:	19
OUTBOUND WASH:	5
INTERIOR CLEAN:	7
GAS PUMPS	10

NOTES:  
 1. EXISTING ENTRANCE ① & ② FROM BURGER KING LOT AND ④ & ⑥ FROM DELTA SONIC LOT REMOVED WITH NEW DEVELOPMENT

M:\CAD Projects\2002-028A\2002-028A\2002-028A Traffic, Red, Green, Land\REV2007.dwg, Proposed Traffic, 6/8/2016 5:12:57 AM, SJP

<p><b>INTECH CONSULTANTS, INC.</b>          1989 UNIVERSITY LANE, SUITE D          Lisle, Illinois  <small>ILLINOIS REGISTRATION No. 184-001040</small></p>	<p><b>ENGINEERS SURVEYORS</b></p>	<p>BENDERSON DEVELOPMENT CO.          570 DELAWARE AVENUE          BUFFALO, NY 14202</p>	<p><b>DELTA SONIC CAR WASH</b>          145 OGDEN AVENUE          DOWNERS GROVE, ILLINOIS</p>	<p><b>PROPOSED TRAFFIC FLOW PLAN</b></p>	<p>6-7-16 3          4-22-16 2          1-27-16 1</p>	<p>VILLAGE REVIEW COMMENT SET #1 (6-2-16)          VILLAGE/IDOT REVIEW / NEW LAYOUT          PER SANITARY/VILLAGE/IDOT REVIEWS</p>	<p>SJP SJP SJP</p>	<p><b>DRAWN</b> SJP</p>	<p><b>SCALE</b> 1"=20'</p>	<p><b>SHEET</b> 19</p>	<p><b>OF</b> 20</p>
					<p><b>DATE</b> NO.</p>	<p><b>REVISION</b></p>	<p><b>BY</b></p>	<p><b>DESIGNED</b> LGG</p>	<p><b>DATE</b> 1-28-15</p>	<p><b>PROJECT NO.</b> 2002-028A</p>	

SCALE: 1" = 20'



**PEDESTRIAN CIRCULATION**

PRIMARY

SECONDARY

<p><b>INTECH CONSULTANTS, INC.</b> 1989 UNIVERSITY LANE, SUITE D LISLE, ILLINOIS <small>ILLINOIS REGISTRATION No. 184-001040</small></p>	<p><b>ENGINEERS SURVEYORS</b></p>	<p>BENDERSON DEVELOPMENT CO. 570 DELAWARE AVENUE BUFFALO, NY 14202</p>	<p><b>DELTA SONIC CAR WASH</b> 1415 OGDEN AVENUE DOWNERS GROVE, ILLINOIS</p>	<p><b>PEDESTRIAN CIRCULATION PLAN</b></p>	6-7-16	3	<p>VILLAGE REVIEW COMMENT SET #1 (6-2-16) VILLAGE/IDOT REVIEW / NEW LAYOUT PER SANITARY/VILLAGE/IDOT REVIEWS</p>	SJP	<p><b>DRAWN</b> SJP</p>	<p><b>SCALE</b> 1"=20'</p>	<p><b>SHEET</b> 20</p>	<p><b>OF</b> 20</p>
					4-22-16	2		DATE				
					1-27-16	1		SJP	LGG	1-28-15	2002-028A	

M:\CAD Projects\2002-028A\2002-028A\2002-028A Traffic\_Plan\_Pedestrian.dwg, Pedestrian, 6/8/2016 5:13:30 AM, SJP

# FINAL PLAT OF SUBDIVISION TO CONSOLIDATE LOTS OF DELTA SONIC DOWNERS GROVE

PIN NUMBERS: 09-06-405-001  
09-06-405-007  
09-06-405-008  
09-06-405-009  
09-06-405-010  
09-06-405-026



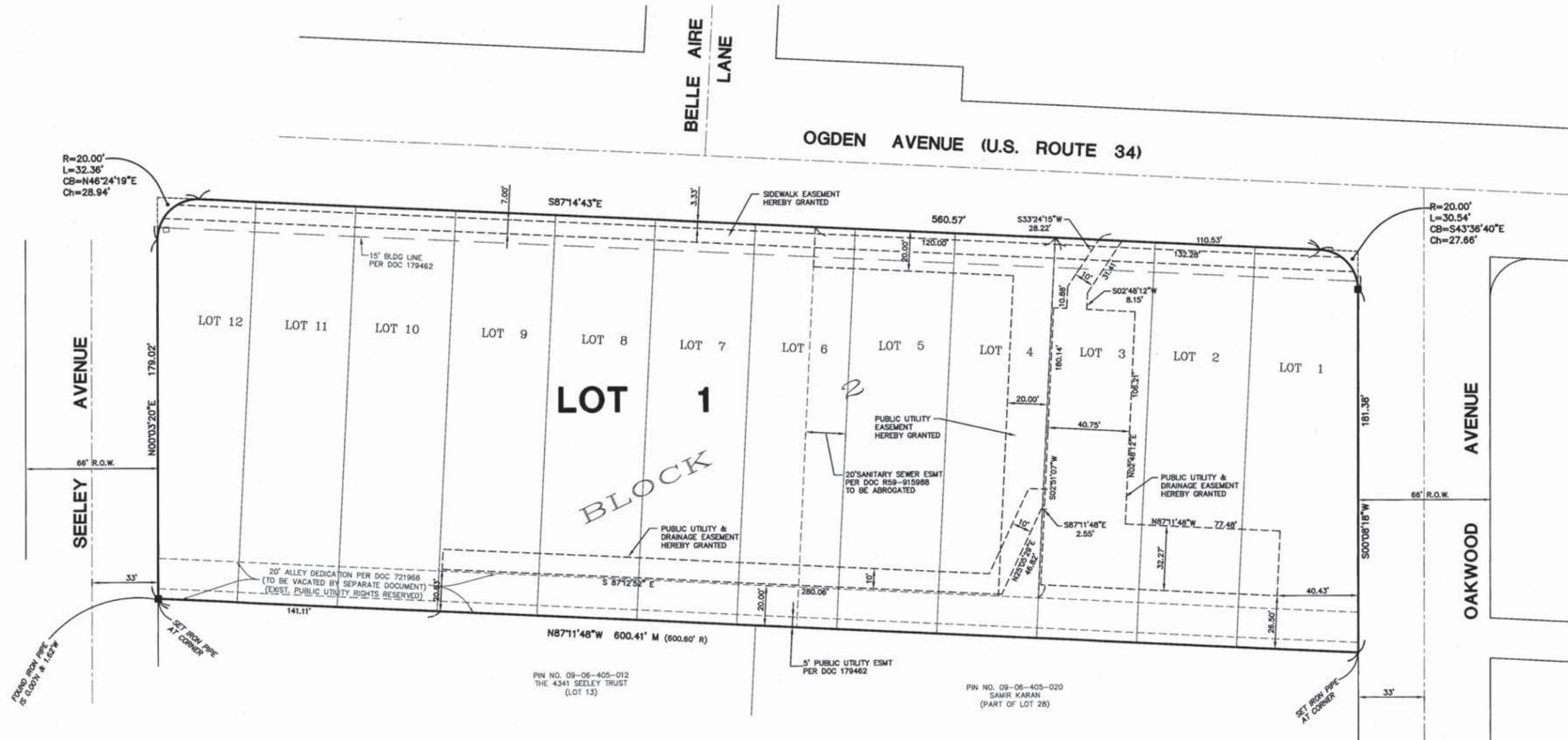
SCALE: 1" = 30'  
BASIS OF BEARING - ASSUMED

**LEGEND**

- CONCRETE MONUMENT SET
- - - EASEMENT LINE
- SUBDIVISION BOUNDARY LINE

SITE AREA = 119,946 S.F. = 2.7536 AC.

PART OF THE SOUTHEAST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH,  
RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY,  
ILLINOIS.



SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS )  
                          ) SS  
COUNTY OF DU PAGE)

THIS IS TO STATE THAT I, MARK STIMAC, ILLINOIS PROFESSIONAL LAND SURVEYOR #35-2587, HAVE SURVEYED AND SUBDIVIDED THE FOLLOWING DESCRIBED PROPERTY AS SHOWN BY THIS PLAT, WHICH IS A CORRECT REPRESENTATION OF SAID SURVEY AND SUBDIVISION; ALL DIMENSIONS ARE IN FEET OR DECIMALS THEREOF:

LOTS 1 THROUGH 12, INCLUSIVE, IN BLOCK 2 IN ARTHUR T. MCINTOSH AND COMPANY'S ADDITION TO DOWNERS GROVE, BEING A SUBDIVISION OF LOT 2 OF THE PLAT OF DOWNER ESTATE IN PART OF THE SOUTHEAST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID ARTHUR T. MCINTOSH AND COMPANY'S ADDITION TO DOWNERS GROVE, RECORDED JUNE 26, 1924 AS DOCUMENT 179462, IN DUPAGE COUNTY, ILLINOIS.

DATED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_\_\_.

ILLINOIS PROFESSIONAL LAND SURVEYOR #35-2587  
LICENSE EXPIRATION/RENEWAL DATE 11-30-2016

M:\CAD Projects\2002-028A\dwg\2002-028A FINAL PLAT OF SUB.DWG SUB PLAT SHIT 1, 04/20/16 8:25:56 AM, S.P.

FINAL PLAT OF SUBDIVISION

**INTECH CONSULTANTS, INC.**

1989 UNIVERSITY LANE, SUITE D ENGINEERS - SURVEYORS  
LISLE, ILLINOIS 60532  
PHONE: 630-964-5656 ILLINOIS REGISTRATION No. 184-001040

REVISED: 4-22-16  
REVISED: 12-16-15  
PREPARED: 1-21-2015

SHEET No. 1 of 2 JOB No.: 2002-028A

DECLARATION OF RESTRICTIVE COVENANTS

THE UNDERSIGNED OWNER HEREBY DECLARES THAT THE REAL PROPERTY DESCRIBED IN AND DEPICTED ON THIS PLAT OF SUBDIVISION SHALL BE HELD, TRANSFERRED, SOLD, CONVEYED AND OCCUPIED SUBJECT TO THE FOLLOWING COVENANTS AND RESTRICTIONS: (A) ALL PUBLIC UTILITY STRUCTURES AND FACILITIES, WHETHER LOCATED ON PUBLIC OR PRIVATE PROPERTY, SHALL BE CONSTRUCTED WHOLLY UNDERGROUND, EXCEPT FOR TRANSFORMERS, TRANSFORMER PADS, LIGHT POLES, REGULATORS, VALVES, MARKERS AND SIMILAR STRUCTURES APPROVED BY THE VILLAGE ENGINEER OF THE VILLAGE OF DOWNERS GROVE PRIOR TO RECORDING OF THIS PLAT OF SUBDIVISION. (B) AN EASEMENT FOR SERVING THE SUBDIVISION, AND OTHER PROPERTY WITH STORM DRAINAGE, SANITARY SEWER, STREET LIGHTING, POTABLE WATER SERVICE AND OTHER PUBLIC UTILITY SERVICES, IS HEREBY RESERVED FOR AND GRANTED TO THE VILLAGE OF DOWNERS GROVE AND DOWNERS GROVE SANITARY DISTRICT, THEIR RESPECTIVE SUCCESSORS AND ASSIGNS, JOINTLY AND SEPARATELY, TO INSTALL, OPERATE AND MAINTAIN AND REMOVE, FROM TIME TO TIME, FACILITIES AND EQUIPMENT USED IN CONNECTION WITH THE PUBLIC WATER SUPPLY, TRANSMISSION LINES, SANITARY SEWERS, STORM DRAINAGE SYSTEM, STREET LIGHTING SYSTEM, OR OTHER PUBLIC UTILITY SERVICE, AND THEIR APPURTENANCES, EITHER ON, OVER, ACROSS, BELOW OR THROUGH THE GROUND SHOWN WITHIN THE DOTTED LINES ON THE PLAT MARKED "PUBLIC UTILITY AND DRAINAGE EASEMENT", OR SIMILAR LANGUAGE, AND THE PROPERTY DESIGNATED ON THE PLAT FOR STREETS AND ALLEYS, TOGETHER WITH THE RIGHT TO CUT, TRIM OR REMOVE TREES, BUSHES AND ROOTS AS MAY BE REASONABLY REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON THE SUBDIVIDED PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER GRANTEE'S FACILITIES OR IN, UPON OR OVER, THE PROPERTY WITHIN THE STORMWATER OR SEWER EASEMENT WITHOUT THE PRIOR WRITTEN CONSENT OF GRANTEEES. AFTER INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF. WHEREAS, SAID LOTS WILL BE CONVEYED TO PURCHASERS SUBJECT TO THIS DECLARATION TO THE END THAT THE RESTRICTIONS IMPOSED SHALL INURE TO THE BENEFIT OF EACH AND ALL OF THE PURCHASERS OF SUCH LOTS WHETHER THEY SHALL HAVE BECOME SUCH BEFORE OR AFTER THE DATE THEREOF, AND THEIR RESPECTIVE HEIRS AND ASSIGNS, AND WHEREAS, THE AFORESAID PROPERTY DESCRIBED ON THE ATTACHED PLAT IS LOCATED ENTIRELY WITHIN THE CORPORATE LIMITS OF THE VILLAGE OF DOWNERS GROVE, ILLINOIS, AND WHEREAS, ALL OF THE PROVISIONS, RESTRICTIONS, CONDITIONS, COVENANTS, AGREEMENTS, AND CHARGES HEREIN CONTAINED SHALL RUN WITH AND BIND ALL OF SAID LOTS AND LAND AND SHALL INURE TO THE BENEFIT OF, AND BE ENFORCEABLE BY THE VILLAGE OF DOWNERS GROVE, ILLINOIS, AND THE OWNERS OR OWNER OF ANY OF THE LOTS OF LAND COMPRISED WITHIN SAID PLAT, AND THEIR RESPECTIVE HEIRS, EXECUTORS, ADMINISTRATORS, SUCCESSORS AND ASSIGNS. NOW, THEREFORE, ALL PERSONS, FIRMS OR CORPORATIONS NOW OWNING THE AFORESAID PROPERTY DO COVENANT AND AGREE THAT THEY OR ANY PERSON, FIRM OR CORPORATION HEREAFTER ACQUIRING ANY PROPERTY OR LOTS SHOWN UPON THE ATTACHED PLAT OF SUBDIVISION ARE HEREBY SUBJECT TO THE FOLLOWING RESTRICTIONS RUNNING WITH SAID PROPERTY TO WHOMSOEVER OWNED, TO WIT:

- 1. NO IMPROVEMENTS SHALL BE MADE IN OR UPON THE PUBLIC UTILITY AND DRAINAGE EASEMENT, INCLUDING DETENTION OR RETENTION AREAS, AS DESCRIBED IN THE PLAT OF SUBDIVISION, EXCEPT FOR LANDSCAPE INSTALLATION OF TREES, SHRUBS, BUSHES AND GRASS AND THE INSTALLATION OF UNDERGROUND UTILITY LINES AND DRIVEWAYS AND ONGRADE PATIOS.
2. EACH OWNER OR PURCHASER SHALL BE RESPONSIBLE FOR MAINTAINING THE PUBLIC UTILITY AND DRAINAGE EASEMENT, INCLUDING DETENTION OR RETENTION AREAS, APPLICABLE TO HIS LOT IN SUCH MANNER AS TO INSURE THE FREE AND UNINTERRUPTED FLOW OF STORM WATER THROUGH THE DRAINAGE SYSTEM OF THE SUBDIVISION, AND SHALL NOT DESTROY OR MODIFY GRADES OR SLOPES WITHOUT HAVING FIRST RECEIVED PRIOR WRITTEN APPROVAL OF THE VILLAGE OF DOWNERS GROVE, ILLINOIS.
3. IN THE EVENT ANY OWNER OR PURCHASER FAILS TO PROPERLY MAINTAIN THE PUBLIC UTILITY AND DRAINAGE EASEMENT, INCLUDING DETENTION OR RETENTION AREAS, THE VILLAGE OF DOWNERS GROVE, ILLINOIS, SHALL UPON TEN DAYS' PRIOR WRITTEN NOTICE, RESERVE THE RIGHT TO PERFORM, OR HAVE PERFORMED ON ITS BEHALF, ANY MAINTENANCE WORK TO OR UPON THE PUBLIC UTILITY AND DRAINAGE EASEMENT, INCLUDING DETENTION OR RETENTION AREAS, REASONABLY NECESSARY TO INSURE ADEQUATE STORMWATER STORAGE AND FREE FLOW OF STORMWATER THROUGH THE PUBLIC UTILITY AND DRAINAGE EASEMENT, INCLUDING DETENTION OR RETENTION AREAS.
4. IN THE EVENT THE VILLAGE OF DOWNERS GROVE, ILLINOIS, SHALL BE REQUIRED TO PERFORM, OR HAVE PERFORMED ON ITS BEHALF, ANY MAINTENANCE WORK TO OR UPON THE PUBLIC UTILITY AND DRAINAGE EASEMENT, INCLUDING DETENTION OR RETENTION AREAS, THE COST TOGETHER WITH THE ADDITIONAL SUM OF TEN PERCENT SHALL, UPON RECORDATION OF A NOTICE OF LIEN WITHIN SIXTY DAYS OF COMPLETION OF THE WORK, CONSTITUTE A LIEN AGAINST HIS LOT WHICH MAY BE FORECLOSED BY AN ACTION BROUGHT BY OR ON BEHALF OF THE VILLAGE OF DOWNERS GROVE, ILLINOIS.
5. THE AFORESAID RESTRICTIONS AND COVENANTS, AND EACH AND EVERY ONE OF THEM, ARE HEREBY EXPRESSLY MADE AN ESSENTIAL PART OF THIS INSTRUMENT, AND SHALL BE AND REMAIN OF PERPETUAL EFFICACY AND OBLIGATION IN RESPECT TO THE SAID PREMISES AND THE PARTIES HEREIN DESIGNATED, THEIR AND EACH OF THEIR SUCCESSORS, HEIRS, AND ASSIGNS.

IN WITNESS WHEREOF, THE OWNERS HAVE SET THEIR HANDS UPON THE ATTACHED PLAT THE DAY AND DATE FIRST WRITTEN HEREON.

BY: \_\_\_\_\_

NOTARY CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )
I, \_\_\_\_\_, A NOTARY PUBLIC IN AND FOR SAID COUNTY IN THE STATE AFORESAID DO HEREBY CERTIFY THAT

ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGE THAT THEY SIGNED AND DELIVERED THIS INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT, AS GIVEN UNDER MY HAND AND NOTARIAL SEAL.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

NOTARY PUBLIC
MY COMMISSION EXPIRES \_\_\_\_\_

ILLINOIS DEPARTMENT OF TRANSPORTATION

STATE OF ILLINOIS )
SS
COUNTY OF )

THIS PLAT HAS BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION WITH RESPECT TO ROADWAY ACCESS PURSUANT TO ILLINOIS COMPILED STATUTES CH. 765, SEC. 205/2; HOWEVER, A HIGHWAY PERMIT IS REQUIRED OF THE OWNER OF THE PROPERTY, A PLAN THAT MEETS REQUIREMENTS CONTAINED IN THE DEPARTMENT'S "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS" WILL BE REQUIRED.

APPROVED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

ILLINOIS DEPARTMENT OF TRANSPORTATION

FINAL PLAT OF SUBDIVISION TO CONSOLIDATE LOTS OF DELTA SONIC DOWNERS GROVE

PART OF THE SOUTHEAST QUARTER OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY, ILLINOIS.

EASEMENT PROVISIONS

AN EASEMENT FOR SERVING THE SUBDIVISION AND OTHER PROPERTY WITH ELECTRIC AND COMMUNICATION SERVICE IS HEREBY RESERVED FOR AND GRANTED TO COMMONWEALTH EDISON COMPANY AND AT&T, ILLINOIS A.K.A. ILLINOIS BELL TELEPHONE COMPANY, GRANTEEES, THEIR RESPECTIVE LICENSEES, SUCCESSORS AND ASSIGNS JOINTLY AND SEVERALLY, TO CONSTRUCT, OPERATE, REPAIR, MAINTAIN, MODIFY, RECONSTRUCT, REPLACE, SUPPLEMENT, RELOCATE AND REMOVE, FROM TIME TO TIME, POLES GUYS, ANCHORS, WIRES, CABLES, CONDUITS, MANHOLES, TRANSFORMERS, PEDESTALS, EQUIPMENT CABINETS OR OTHER FACILITIES USED IN CONNECTION WITH OVERHEAD AND UNDERGROUND TRANSMISSION AND DISTRIBUTION OF ELECTRICITY, COMMUNICATIONS, SOUNDS AND SIGNALS IN, OVER, UNDER, ACROSS, ALONG AND UPON THE SURFACE OF THE PROPERTY SHOWN WITHIN THE DASHED OR DOTTED LINES (OR SIMILAR DESIGNATION) ON THE PLAT AND MARKED "EASEMENT", "UTILITY EASEMENT", "PUBLIC UTILITY EASEMENT", "P.U.E." (OR SIMILAR DESIGNATION), THE PROPERTY DESIGNATED IN THE DECLARATION OF CONDOMINIUM AND/OR ON THIS PLAT AS "COMMON ELEMENTS", AND THE PROPERTY DESIGNATED ON THE PLAT AS "COMMON AREA OR AREAS", AND THE PROPERTY DESIGNATED ON THE PLAT FOR STREETS AND ALLEYS, WHETHER PUBLIC OR PRIVATE, TOGETHER WITH THE RIGHTS TO INSTALL REQUIRED SERVICE CONNECTIONS OVER OR UNDER THE SURFACE OF EACH LOT AND COMMON AREA OR AREAS TO SERVE IMPROVEMENTS THEREON, OR ON ADJACENT LOTS, AND COMMON AREA OR AREAS, THE RIGHT TO CUT, TRIM OR REMOVE TREES, BUSHES, ROOTS AND SAPLINGS AND TO CLEAR OBSTRUCTIONS FROM THE SURFACE AND SUBSURFACE AS MAY BE REASONABLY REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON THE SUBDIVIDED PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER GRANTEEES' FACILITIES OR IN, UPON OR OVER THE PROPERTY WITHIN THE DASHED OR DOTTED LINES (OR SIMILAR DESIGNATION) MARKED "EASEMENT", "UTILITY EASEMENT", "PUBLIC UTILITY EASEMENT", "P. U. E." (OR SIMILAR DESIGNATION) WITHOUT THE PRIOR WRITTEN CONSENT OF GRANTEEES. AFTER INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF. THE TERM "COMMON ELEMENTS" SHALL HAVE THE MEANING SET FORTH FOR SUCH TERM IN THE "CONDOMINIUM PROPERTY ACT", CHAPTER 765 ILCS 605/2, AS AMENDED FROM TIME TO TIME.

THE TERM "COMMON AREA OR AREAS" IS DEFINED AS A LOT, PARCEL OR AREA OF REAL PROPERTY, THE BENEFICIAL USE AND ENJOYMENT OF WHICH IS RESERVED IN WHOLE OR AS AN APPURTENANCE TO THE SEPARATELY OWNED LOTS, PARCELS OR AREAS WITHIN THE PLANNED DEVELOPMENT, EVEN THOUGH SUCH BE OTHERWISE DESIGNATED ON THE PLAT BY TERMS SUCH AS "OUTLOTS", "COMMON ELEMENTS", "OPEN SPACE", "OPEN AREA", "COMMON GROUND", "PARKING" AND "COMMON AREA". THE TERM "COMMON AREA OR AREAS", AND "COMMON ELEMENTS" INCLUDE REAL PROPERTY SUCFACED WITH INTERIOR DRIVEWAYS AND WALKWAYS, BUT EXCLUDES REAL PROPERTY PHYSICALLY OCCUPIED BY A BUILDING, SERVICE BUSINESS DISTRICT OR STRUCTURES SUCH AS A POOL, RETENTION POND OR MECHANICAL EQUIPMENT.

RELOCATION OF FACILITIES WILL BE DONE BY GRANTEEES AT COST OF THE GRANTOR/LOT OWNER, UPON WRITTEN REQUEST.

PUBLIC SIDEWALK EASEMENT

THE UNDERSIGNED OWNER HEREBY DECLARES THAT THE REAL PROPERTY DESCRIBED AND DEPICTED ON THIS PLAT SHALL BE HELD, TRANSFERRED, SOLD, CONVEYED AND OCCUPIED SUBJECT TO THE FOLLOWING EASEMENT RESTRICTIONS:

AN EASEMENT IS HEREBY GRANTED TO THE VILLAGE OF DOWNERS GROVE, ILLINOIS AND OTHER SERVICE PROVIDERS OPERATING UNDER FRANCHISES OR OTHER AUTHORITY FROM SAID VILLAGE, INCLUDING, BUT NOT LIMITED TO THEIR SUCCESSORS AND ASSIGNS, OVER ALL OF THE AREAS ON THIS PLAT, MARKED PUBLIC ACCESS EASEMENT, FOR THE PERPETUAL RIGHT, PRIVILEGE AND AUTHORITY TO CONSTRUCT, RECONSTRUCT, REPAIR, INSPECT AND MAINTAIN A PUBLIC SIDEWALK AND OTHER NECESSARY STRUCTURES AND APPURTENANCES AS MAY BE DEEMED NECESSARY BY SAID VILLAGE, OVER, UPON, ALONG, UNDER AND THROUGH SAID INDICATED EASEMENTS, TOGETHER WITH RIGHT OF ACCESS ACROSS THE PROPERTY FOR NECESSARY MEN AND EQUIPMENT TO DO ANY OF THE ABOVE WORK. THE RIGHT IS ALSO GRANTED TO CUT DOWN, TRIM OR REMOVE ANY TREES, SHRUBS OR OTHER PLANTS THAT INTERFERE WITH ABOVE MENTIONED WORK AND PUBLIC ACCESS. NO PERMANENT BUILDINGS OR STRUCTURES SHALL BE PLACED ON SAID EASEMENTS. EASEMENTS ARE HEREBY GRANTED TO THE VILLAGE OF DOWNERS GROVE AND OTHER GOVERNMENTAL AUTHORITIES HAVING JURISDICTION OF THE LAND, OVER THE ENTIRE EASEMENT AREA FOR PUBLIC ACCESS, INGRESS, EGRESS AND THE PERFORMANCE OF MUNICIPAL AND OTHER GOVERNMENTAL SERVICES.

WHEREAS, SAID PROPERTY DESCRIBED WILL BE CONVEYED TO PURCHASERS SUBJECT TO THIS DECLARATION TO THE END THAT THE RESTRICTIONS IMPOSED SHALL INURE TO THE BENEFIT OF EACH AND ALL OF THE PURCHASERS OF SUCH PROPERTY WHETHER THEY SHALL HAVE BECOME SUCH BEFORE OR AFTER THE DATE THEREOF, AND THEIR RESPECTIVE HEIRS AND ASSIGNS, AND

WHEREAS, THE AFORESAID PROPERTY DESCRIBED ON THE THIS PLAT IS LOCATED ENTIRELY WITHIN THE CORPORATE LIMITS OF THE VILLAGE OF DOWNERS GROVE, ILLINOIS, AND

WHEREAS, ALL OF THE PROVISIONS, RESTRICTIONS, CONDITIONS, COVENANTS, AGREEMENTS, AND CHARGES HEREIN CONTAINED SHALL RUN WITH AND BIND ALL OF SAID PROPERTY AND LAND AND SHALL INURE TO THE BENEFIT OF, AND BE ENFORCEABLE BY THE VILLAGE OF DOWNERS GROVE, ILLINOIS, AND THE OWNERS OR OWNER OF ANY OF THE PROPERTY COMPRISED WITHIN THIS PLAT, AND THEIR RESPECTIVE HEIRS, EXECUTORS, ADMINISTRATORS, SUCCESSORS AND ASSIGNS.

NOW, THEREFORE, ALL PERSONS, FIRMS OR CORPORATIONS NOW OWNING THE AFORESAID PROPERTY DO COVENANT AND AGREE THAT THEY OR ANY PERSON, FIRM OR CORPORATION HEREAFTER ACQUIRING ANY PROPERTY SHOWN UPON THIS PLAT ARE HEREBY SUBJECT TO SAID EASEMENT PROVISIONS AND RESTRICTIONS RUNNING WITH SAID PROPERTY BY WHOMSOEVER OWNED.

IN WITNESS WHEREOF, THE OWNERS HAVE SET THEIR HANDS UPON THIS PLAT THE DAY AND DATE FIRST WRITTEN THEREON.

THIS CERTIFICATE SHALL BE DATED AND SHALL HAVE LINES FOR SIGNATURE OF THE OWNER (OR OWNERS) AND THE SIGNATURE AND JURAT OF A NOTARY PUBLIC.

DOWNERS GROVE SANITARY DISTRICT CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

I, \_\_\_\_\_, COLLECTOR FOR THE DOWNERS GROVE SANITARY DISTRICT, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE NOT BEEN APPORTIONED AGAINST THE TRACT OF LAND INCLUDED IN THIS PLAT.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

COLLECTOR

VILLAGE COLLECTOR'S CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

I, \_\_\_\_\_, COLLECTOR FOR THE VILLAGE OF DOWNERS GROVE DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE NOT BEEN APPORTIONED AGAINST THE TRACT OF LAND, INCLUDED IN THIS PLAT.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

BY: \_\_\_\_\_

DU PAGE COUNTY CLERK'S CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

I, \_\_\_\_\_, COUNTY CLERK OF DU PAGE COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT GENERAL TAXES, NO UNPAID FORFEITED TAXES AND NO REDEEMABLE TAX SALES AGAINST ANY OF THE LAND INCLUDED IN THIS PLAT. I FURTHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THIS PLAT, GIVEN UNDER MY HAND AND SEAL OF THE COUNTY CLERK OF DU PAGE COUNTY, ILLINOIS.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

COUNTY CLERK

COUNTY RECORDER'S CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

THIS PLAT WAS FILED FOR RECORD IN THE RECORDERS OFFICE OF DUPAGE COUNTY,

ILLINOIS, ON THE \_\_\_ DAY OF \_\_\_\_\_, 20\_\_ AT \_\_\_\_\_ O'CLOCK \_\_\_ M. AS DOCUMENT NUMBER \_\_\_\_\_.

DUPAGE COUNTY RECORDER

SCHOOL DISTRICT CERTIFICATION

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

THE UNDERSIGNED DO HEREBY CERTIFY THAT, AS OWNERS OF THE PROPERTY DESCRIBED IN THE SURVEYOR'S CERTIFICATE, AND KNOWN AS \_\_\_\_\_ TO THE BEST OF THEIR KNOWLEDGE, IS LOCATED WITHIN THE BOUNDARIES OF THE \_\_\_\_\_ HIGH SCHOOL DISTRICT, AND \_\_\_\_\_ ELEMENTARY SCHOOL DISTRICT IN DUPAGE COUNTY, ILLINOIS.

DATED AT \_\_\_\_\_, ILLINOIS THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

BY: \_\_\_\_\_

VILLAGE OF DOWNERS GROVE DIRECTOR OF COMMUNITY DEVELOPMENT CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

APPROVED BY THE VILLAGE OF DOWNERS GROVE DIRECTOR OF COMMUNITY DEVELOPMENT,

THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

DIRECTOR OF COMMUNITY DEVELOPMENT

VILLAGE CLERK

PIN NUMBERS: 09-06-405-001
09-06-405-007
09-06-405-008
09-06-405-009
09-06-405-010
09-06-405-026

OWNER'S CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

DELTA SONIC CAR WASH SYSTEMS, INC. (LOTS 2 THRU 9 INCLUSIVE) HEREBY CERTIFIES THAT THEY ARE THE OWNERS OF THE ABOVE DESCRIBED PROPERTY AND THEY HAVE CAUSED THE SAME TO BE SURVEYED AND SUBDIVIDED AS SHOWN ON THE PLAT HEREON DRAWN.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

BY: \_\_\_\_\_

NOTARY CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

I, \_\_\_\_\_, A NOTARY PUBLIC IN AND FOR SAID COUNTY IN THE STATE AFORESAID DO HEREBY CERTIFY THAT

ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGE THAT THEY SIGNED AND DELIVERED THIS INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT, AS GIVEN UNDER MY HAND AND NOTARIAL SEAL.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

NOTARY PUBLIC MY COMMISSION EXPIRES \_\_\_\_\_

OWNER'S CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

DAVID H. BALDAUF (LOTS 1, 10, 11 AND 12) HEREBY CERTIFIES THAT HE IS THE OWNER OF THE ABOVE DESCRIBED PROPERTY AND HE HAS CAUSED THE SAME TO BE SURVEYED AND SUBDIVIDED AS SHOWN ON THE PLAT HEREON DRAWN.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

BY: \_\_\_\_\_

NOTARY CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

I, \_\_\_\_\_, A NOTARY PUBLIC IN AND FOR SAID COUNTY IN THE STATE AFORESAID DO HEREBY CERTIFY THAT

ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGE THAT THEY SIGNED AND DELIVERED THIS INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT, AS GIVEN UNDER MY HAND AND NOTARIAL SEAL.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

NOTARY PUBLIC MY COMMISSION EXPIRES \_\_\_\_\_

ENGINEER'S CERTIFICATE

STATE OF ILLINOIS )
SS
COUNTY OF DU PAGE )

I, \_\_\_\_\_, A REGISTERED PROFESSIONAL

ENGINEER IN ILLINOIS AND \_\_\_\_\_ IS THE OWNER OF THE LAND DEPICTED HEREON OR THEIR DULY AUTHORIZED ATTORNEY, DO HEREBY STATE, THAT TO THE BEST OF OUR KNOWLEDGE AND BELIEF, REASONABLE PROVISION HAS BEEN MADE FOR COLLECTION AND DIVERSION OF SUCH SURFACE WATER AND PUBLIC AREAS, OR DRAINS WHICH THE SUBDIVIDER HAS A RIGHT TO USE, AND THAT SUCH SURFACE WATERS WILL BE PLANNED FOR IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGE TO ADJOINING PROPERTY BECAUSE OF THE CONSTRUCTION OF THE SUBDIVISION. FURTHER, AS ENGINEER, I HEREBY CERTIFY THAT THE PROPERTY WHICH IS THE SUBJECT OF THIS SUBDIVISION OR ANY PART THEREOF IS NOT LOCATED WITHIN A SPECIAL FLOOD HAZARD AREA AS IDENTIFIED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, A.D., 20\_\_.

REGISTRATION EXPIRATION DATE
ILLINOIS REGISTERED PROFESSIONAL ENGINEER, STATE REGISTRATION NUMBER

PROPERTY OWNER: \_\_\_\_\_

INTECH CONSULTANTS, INC.
1989 UNIVERSITY LANE, SUITE D
LISLE, ILLINOIS 60532
PHONE: 630-964-5656
ENGINEERS - SURVEYORS
ILLINOIS REGISTRATION No. 184-001040
REVISD: 4-22-16
REVISD: 12-16-15
PREPARED: 1-21-2015
SHEET No. 2 of 2
JOB No.: 2002-028A

M:\CAD Projects\2002-028A\dwg\2002-028A FINAL PLAT OF SUB.dwg, 8:18:16 AM, \$P

FINAL PLAT OF SUBDIVISION



**VILLAGE OF DOWNERS GROVE  
REPORT FOR THE PLAN COMMISSION  
AUGUST 1, 2016 AGENDA**

<b>SUBJECT:</b>	<b>TYPE:</b>	<b>SUBMITTED BY:</b>
16-PLC-0032 1620 Ogden Avenue	Special Use for an Automobile Dealership	Scott Williams Planner

**REQUEST**

The petitioner is requesting approval of a Special Use for an Automobile Dealership.

**NOTICE**

The application has been filed in conformance with applicable procedural and public notice requirements.

**GENERAL INFORMATION**

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**OWNER/APPLICANT:** Local Community Signs, LLC  
400 E. Pinelake Circle  
Vernon Hills, IL 60061

**PROPERTY INFORMATION**

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**EXISTING ZONING:** B-3, General Services and Highway Business  
**EXISTING LAND USE:** Vacant McDonald's Restaurant  
**PROPERTY SIZE:** 29,300 square feet  
**PINS:** 09-06-302-006

**SURROUNDING ZONING AND LAND USES**

	<b>ZONING</b>	<b>FUTURE LAND USE</b>
<b>NORTH:</b>	R-2, Residential Detached House 2	Single Family Residential
<b>SOUTH:</b>	B-3, General Services and Highway Business	Commercial Corridor
<b>EAST:</b>	R-6, Residential Apartment/Condo 6	Commercial Corridor
<b>WEST:</b>	B-3, General Services and Highway Business	Commercial Corridor

**ANALYSIS**

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**SUBMITTALS**

This report is based on the following documents, which are on file with the Department of Community Development:

1. Application/Petition for Public Hearing
2. Project Narrative
3. Plat of Survey
4. Site Plan
5. Floor Plan
6. Landscape Plan

7. Landscape Plan
8. Photometric Plan
9. Plat of Easement
10. Owner Letter of Authorization

### **PROJECT DESCRIPTION**

The petitioner is seeking Special Use approval to open an automobile dealership at 1620 Ogden Avenue. The property is located on the north side of Ogden Avenue between Downers Drive and Lee Avenue. The property is zoned B-3, General Services and Highway Business. An automobile dealership is classified under Personal Vehicle Sales and Rentals per Section 5.050.Q.5 of the Zoning Ordinance as a Special Use in the B-3 zoning district.

The petitioner is proposing to use the existing 2,884 square foot former McDonald's building and repurpose it into an automobile dealership. The exterior of the building will not substantially change except for the addition of a garage door on the western elevation where previously there was a pedestrian vestibule and entryway into the McDonald's. This garage door would provide access to the 1,394 square foot showroom for the interior display of 2-3 cars. The rear portion of the building would become a detailing area for the vehicles. Other floor plan changes include a vestibule, unisex bathroom, and another entryway into the detail area. The drive-thru window would be removed with this area converted into an office.

No major changes are proposed to the layout of the exterior site plan. The number and location of curb cuts will remain the same. Although the number of parking spaces will be slightly reduced because of the addition of a landscaping end-cap, this will impact the outdoor display areas and not the parking available for the business. There will be adequate parking with a total of eight spaces including two handicap spaces and 30 spaces devoted to outdoor automobile display.

A landscaping plan shows significant amounts of landscaping will be added to the perimeter of the property. At the front, the concrete patio and two parking spaces will be replaced with landscaping. Shade trees will be added along the front and eastern sides of the property. The rest of the landscaping includes shrubs, ornamental trees, and ground cover.

Screening of adjacent residential properties will be improved at the northern and eastern property lines with the repair and conversion of the existing fence into a 6' tall privacy fence. An optional fence is also proposed for the western property line but is not a code requirement.

### **COMPLIANCE WITH THE COMPREHENSIVE PLAN**

The Comprehensive Plan designates the property for Corridor Commercial. Corridor Commercial uses are defined as automobile related uses that provide services and retail opportunities to the nearby neighborhoods and the surrounding region. Although the subject property is not located in the key focus areas of Ogden Avenue, the Comprehensive Plan does call for aesthetic and functional improvements. The petitioner is proposing improvements that include the beautification of Ogden Avenue through the addition of landscaping and by addressing property maintenance issues. Pedestrian connectivity will also be maintained.

The proposed automobile dealership at this property is consistent with the Comprehensive Plan.

### **COMPLIANCE WITH THE ZONING ORDINANCE**

The property is zoned B-3, General Services and Highway Business. Per Section 28.5.010.A of the Zoning Ordinance, the proposed Automobile Dealership use is an allowable Special Use in the B-3 zoning district. The petitioner is not proposing to make any changes to the building footprint or

substantially change the configuration of the site. Five parking spaces are required, and the petitioner is proposing eight spaces. Adequate screening is provided with particular attention paid to the property lines bordering residential districts. The property will continue to meet all requirements of the Zoning Ordinance.

### **ENGINEERING/PUBLIC IMPROVEMENTS**

The proposed use will not have a negative impact on the existing parking and traffic patterns in the area. A turning exhibit demonstrating that turning radii are clear of the automobile display areas was submitted. The automobile dealership will receive regular deliveries via a semi-truck trailer. Furthermore, vehicle-hauler trucks from nearby dealerships have been observed to navigate the site. The petitioner also understands and agrees that no unloading of automobiles will occur on Ogden Avenue.

A public sidewalk encroaches over the front property line. Per public work's request, the petitioner prepared a Plat of Easement granting a five foot easement across the frontage of the property. This provides the village access to repair and maintain the sidewalk. The petitioner also notes that the retaining walls near the sidewalk will be rebuilt if necessary.

### **NEIGHBORHOOD COMMENT**

Notice was provided to all property owners 250 feet or less from the property line in addition to posting the public hearing sign and publishing a legal notice in the *Downers Grove Suburban Life*. Staff received one call clarifying the nature of the petition and if the existing building was going to be used.

### **FINDINGS OF FACT**

The applicant is requesting a Special Use approval for an Automobile Dealership. Staff believes the proposed use meets the standards for granting a Special Use as outlined below:

#### **Section 28.12.050.H Approval Criteria – Special Uses**

*No special use may be recommended for approval or approved unless the respective review or decision-making body determines that the proposed special use is constituent with and in substantial compliance with all Village Council policies and plans and that the applicant has presented evidence to support each of the following conclusions:*

1. *That the proposed use is expressly authorized as a Special Use in the district in which it is to be located;*  
The zoning of this property is B-3, General Services and Highway Business. Under Section 5.010 of the Zoning Ordinance, Automobile Dealerships are listed as an allowable Special Use in the B-3 zoning district. This standard has been met.
2. *That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.*  
The proposed automobile dealership is a desirable service to the community and will contribute to the general welfare of the Village. The proposed development will repurpose a vacant building. The development will cater to both local and regional customers as desired in the Comprehensive Plan and will meet multiple goals and objectives outlined in the Comprehensive Plan. This standard has been met.
3. *That the proposed use will not, in the particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.*

The proposed use does not present a detriment to the health, safety or general welfare of the surrounding properties. Automobile unloading will occur on-site. A photometric plan demonstrates that lighting from the site will not negatively impact adjacent properties. This standard has been met.

## **RECOMMENDATIONS**

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The proposal is consistent and compatible with the Comprehensive Plan, the Zoning Ordinance, and surrounding zoning and land use classifications. Based on the findings listed above, staff recommends the Plan Commission make a positive recommendation to the Village Council regarding 16-PLC-0032 subject to the following condition:

1. The Special Use shall substantially conform to the staff report; architectural drawings prepared by 1212 Architects & Planners dated June 29, 2016 and landscape plans prepared by Planning Resources, Inc. date July 22, 2016; and documents attached to this report except as such plans may be modified to conform to the Village codes and ordinances.
2. Recording of a Plat of Easement granting a five-foot easement across the frontage of the property to cover the sidewalk and any future improvements.
3. All automobile deliveries shall take place on the subject site. No deliveries may take place on Ogden Avenue.
4. No test drives may take place on Lee Avenue, Lacey Avenue, and Downers Drive.
5. All lights shall be dimmed to security level intensity no later than 30 minutes after the close of business.

Staff Report Approved By:



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Stanley J. Popovich, AICP  
Director of Community Development

SP:sw  
-att



0 60 120 Feet

1620 Ogden Avenue - Location Map



## Project Summary / Narrative Letter

### Application Request

**Local Community Signs submits this application for 1620 Ogden Avenue to request the Special Use approval** to allow a used car and motorcycle sales business at this location.

### Special Use Criteria

The proposed use is expressly authorized as a Special Use in the B3 zoning classification:

- Use Category Commercial, Vehicle Sales and Service,
  - Subcategory **Personal Vehicle Sales and Rental**,  
(VoDG Municipal Code, Chapter 28, Article 5, Sec. 5.050.Q.5, and Table 5-1.)

This use is complimentary to similar auto dealerships along the Ogden Avenue corridor, and is a necessary and desirable service and facility that will serve the public convenience and contribute to the general welfare of the neighborhood and community. In no way will the proposed business or facility be detrimental to the health, safety, or general welfare of residential or commercial neighbors, or persons working in the vicinity. The proposed business and facility will in no way be injurious to the property values or development in the vicinity. No rezoning of the property is required or requested.

### History

The property at **1620 Ogden Avenue**, (PIN 0906302006) was a McDonald's restaurant for several decades, but has been vacant for some time. **Local Community Signs LLC purchased the property in April of 2016**. As part of the purchase from McDonald's, Local Community Signs agreed to a deed restriction for any foodservice business to be established at the property. Local Community Signs has no intention of offering a food service business, and meets the deed restriction. Given the deed restriction from McDonald's, we feel this auto dealership to be the best use of the property.

### Business Operations

The new used auto dealership will operate 6 days a week, Monday – Saturday, with regular business hours, varied to a maximum of 9am to 8pm. These operating hours are considerably less than the previous McDonald's usage.

### Building – Existing and Proposed

The existing structure is a 1-story brick building, with partial basement. It will be renovated as described below.

The exterior of the existing building will remain essentially the same. One large window will be replaced with double doors at the window location. The drive-through window will be removed, at the request of Village staff. Several rooftop units will be removed. Much of the roof is leaking, and it will be repaired and replaced with a new roofing permit as soon as possible. There will be no other exterior material work to the building – existing brick, windows, and finishes will remain. Local Community Signs may opt to paint the exterior at a later time.

The interior of the building will be renovated. The entire McDonald's kitchen and all equipment (which remain, at present) will be removed, as well as the service counters, tables and chairs, counters, and all booth seating. Only hand wash sinks will remain.

A structural engineer has inspected the existing concrete floor slab and determined that it is of adequate strength to support multiple cars being parked in the new Showroom. See attached calculations, analysis, and review.

The new open space will house a large open Showroom (1,394sf), as well as a Detail Area (776sf) for preparing vehicles prior to sale. Access to the existing Basement via the existing Stair will remain. All existing exit doors will remain, with a new exterior door added to allow vehicles to drive into the Showroom. Two existing restrooms that do not meet accessibility codes, will be replaced with one large accessible restroom to meet all current standards.

Most of the interior finishes, including floor and ceiling tile, FRP wall panels in the kitchen area, windows, and painted brick, will be repaired or replaced as necessary, but essentially remain.

The existing Basement will remain, with all existing interior walls, closets, and equipment removed. The Basement will only be used for file storage.

#### Fire Prevention

Due to the size of the Showroom and remaining building, and percent of building modification, the Fire Inspector has stated that no fire suppression systems (sprinklers) are required. The fire alarm system will be put back in service, and replaced, if necessary, with the preferred wireless connection to dispatch. All vehicles stored in the building will have fuel tanks filled to less than ¼ of a tank, or 5 gallons, whichever is less.

#### Parking – Existing and Proposed

The existing site has a total of 38 parking spaces, with 2 additional handicap spaces. VoDG Municipal Code requires 2 spaces per 1000sf of Showroom area, plus .4 spaces per 1000sf of outdoor display space, plus 2 per service bay (Article 7, Sec 7.040).

Based on those requirements, the Showroom (1,394sf) requires 3 spaces. The 4,980sf of outdoor display space requires 2 spaces. There is no service Bay, so no additional spaces are required.

The site will have a total of 6 spaces, plus 2 handicap spaces. The site therefore exceeds parking requirements for this Special Use category of 5 spaces, by having a total of 8 spaces.

All existing curbs, concrete, and asphalt paving will remain, other than the 2 spaces removed for additional landscaping area (see below). All access into the building will remain through existing accessible curb cuts.

#### Delivery Truck Traffic – Existing and Proposed

McDonald's previously developed the site to receive semi-truck trailers for regular deliveries. That drive-in, and drive-out ability will remain the same (see attached pictures). Since purchasing the property, we have seen vehicle-hauler trucks (for dealerships across the street) pull in and out of this

site without problem. See the attached truck-turning diagram as additional confirmation that there is space to do so. In addition no vehicle on display will be parked outside of the marked spaces so to avoid any obstruction of turning movement

#### Landscape and Screening – Existing and Proposed

The B3 zoning designation has a maximum allowed lot coverage of 90%. The existing lot is 29,300sf, and has a coverage area of 24,660sf, equaling 84%.

The B3 zoning designation also requires landscaping and screening areas to total 10% of the lot coverage, with half of that at the street front. The existing site has a total of 4,628sf, equaling 15.8%. However, only 882sf (3%) is at the street front.

To provide new landscaping to meet the requirement, we will add 695sf (2.4%) of landscaping at the street front by removing the existing concrete patio (with outdoor seating) and 2 adjacent parking spaces, immediately north of the public sidewalk. This area is immediately adjacent to the existing landscaping, and directly in front of the building. The public access sidewalk and concrete ramp will remain. This new landscaping area will bring the street front landscaping total to 1,577sf (5.4%), which exceeds the requirement. All new landscaping will meet the specifications as laid out in the Municipal Code.

With the addition of this new landscaped area, the total lot coverage will decrease to 82%. See the attached drawings highlighting the existing and proposed landscaped areas.

The landscape area at the west property line, between parking and adjacent building, will be upgraded/replaced with grass and landscaping. Per Municipal Code, 2 shade or ornamental trees will be provided, which also satisfy Public Work's request for 2 additional parkway trees. See Landscape Plan for more information.

#### Perimeter Fencing – Existing and Proposed

The existing site is adjacent to commercial property on the west, single family residential to the north, and multi-family residential to the east. There is an existing 6' high wood shadow-box style fence at the north and east property lines. All existing 6' wood fence will essentially remain, to be repaired and/or replaced with a 6' high solid fence (in-fill of shadow-box style has been approved by Village staff). In the 3' between the existing fence and the north property line, there is substantial plant growth - bushes that are approximately 15'-20' tall – that will remain.

The fence at the east is missing sections, which will be in-filled with new 6' solid wood fence to match the adjacent fence. The fence at the east will be extended with new matching fence to the south property line at the public sidewalk. No fence exists at the west.

#### Site Lighting – Existing and Proposed

All existing site lighting, on the building, as well as in parking lot fixtures, will remain. See attached photometric analysis and summary. No new fixtures will be provided.

### Site Signage – Existing and Proposed

McDonald's had a street front sign, as well as "entrance" and "exit" directional signs at the access points to and from Ogden Avenue. Those signs have been removed, but will be replaced with new, similar signs, at the same locations, to meet all requirements of the Municipal Code. Local Community Signs will apply for all required permits for permanent and temporary signage, and comply with all portions of the Municipal Code Article 9.

### Summary

Local Community Signs is confident that this development aligns with the comprehensive plan and intended use for the Ogden Avenue Corridor, meets and/or exceeds the planning, zoning and building requirements, and will be an improvement for the adjacent neighbors and Ogden Avenue. Local Community Signs welcomes the opportunity to become a member of the Downers Grove community.

Thank you for your consideration.



**Front (South) Elevation**

*Patio tables and chairs to be removed; new landscaping to consist of overlapping arrangement of groundcover and ornamental grasses*



**Left (West) Elevation**

*Drive through window to be removed. Large center window to be replaced with glass doors.*



**Back (North) Elevation**

*Landscaping to remain and be pruned back to prevent over growth. Concrete, traffic bollards to remain.*



**Right (East) Elevation**

*Parking spaces, traffic bollards to remain. 6' tall solid wood fence to be installed along east perimeter (or existing fence to be repaired) for screening with mix of low deciduous and evergreen buffer plantings.*



Center landscaping to be removed and replaced with an overlapping mix of low groundcover plantings and ornamental grasses; (2) spaces nearest sidewalk to be converted to planting area.



Sidewalk to building, ramp, patio with fixed seating, and center landscaping section all to be removed. Decomposed granite pad to be installed closest to building with new plant material as well.



Landscaping and lawn to be removed and replaced with (1) canopy shade tree and low groundcover; New sign to be installed at existing sign post. Parking spaces to be replaced with auto display.



West exit to remain with landscaping to be removed; New plant material and optional 6' tall solid wood fence at west perimeter. Parking space to be replaced with auto display



Landscaping, light pole and 6' wood shadow-box style fence at north property line.

Light pole to remain (typical).

6' tall solid wood fence along north and east perimeters (or repair existing fence) with maintained plantings consisting of deciduous trees, deciduous and evergreen shrubs, and low groundcover

Parking spaces to be replaced with auto display.



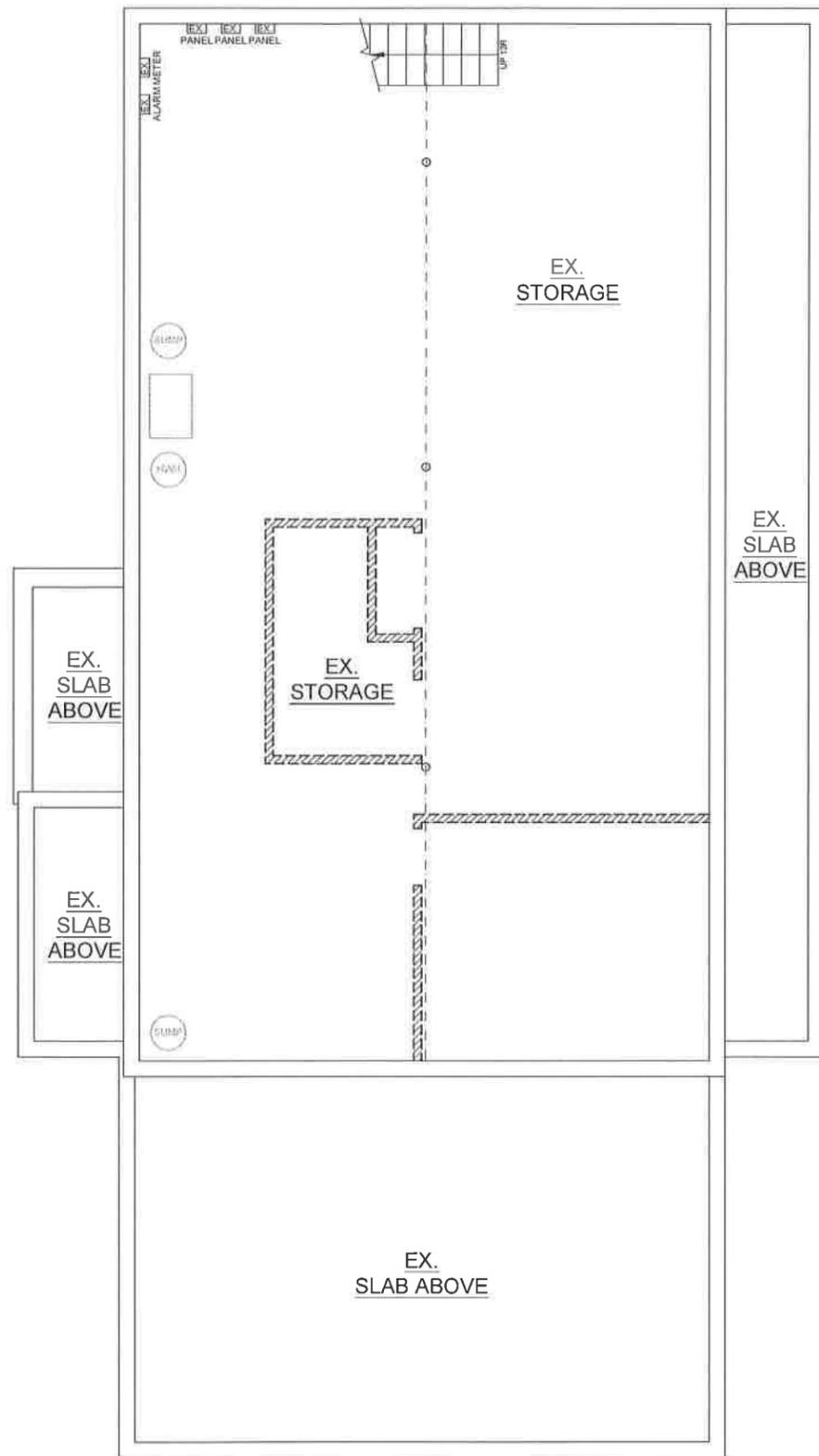
**Back (North) property line**

Garbage enclosure to remain. Light poles to remain (typical). 6' tall solid wood fence to be installed (or repair existing fence) with deciduous shrubs as buffer plantings. Parking spaces to be replaced with auto display.



**Right (East) property line**

Light poles to remain (typical). New 6' tall solid wood fence to be installed (or repair existing fence) with (2) canopy shade trees, deciduous and evergreen shrubs, and low groundcover as buffer plantings. Parking spaces to be replaced with auto display.

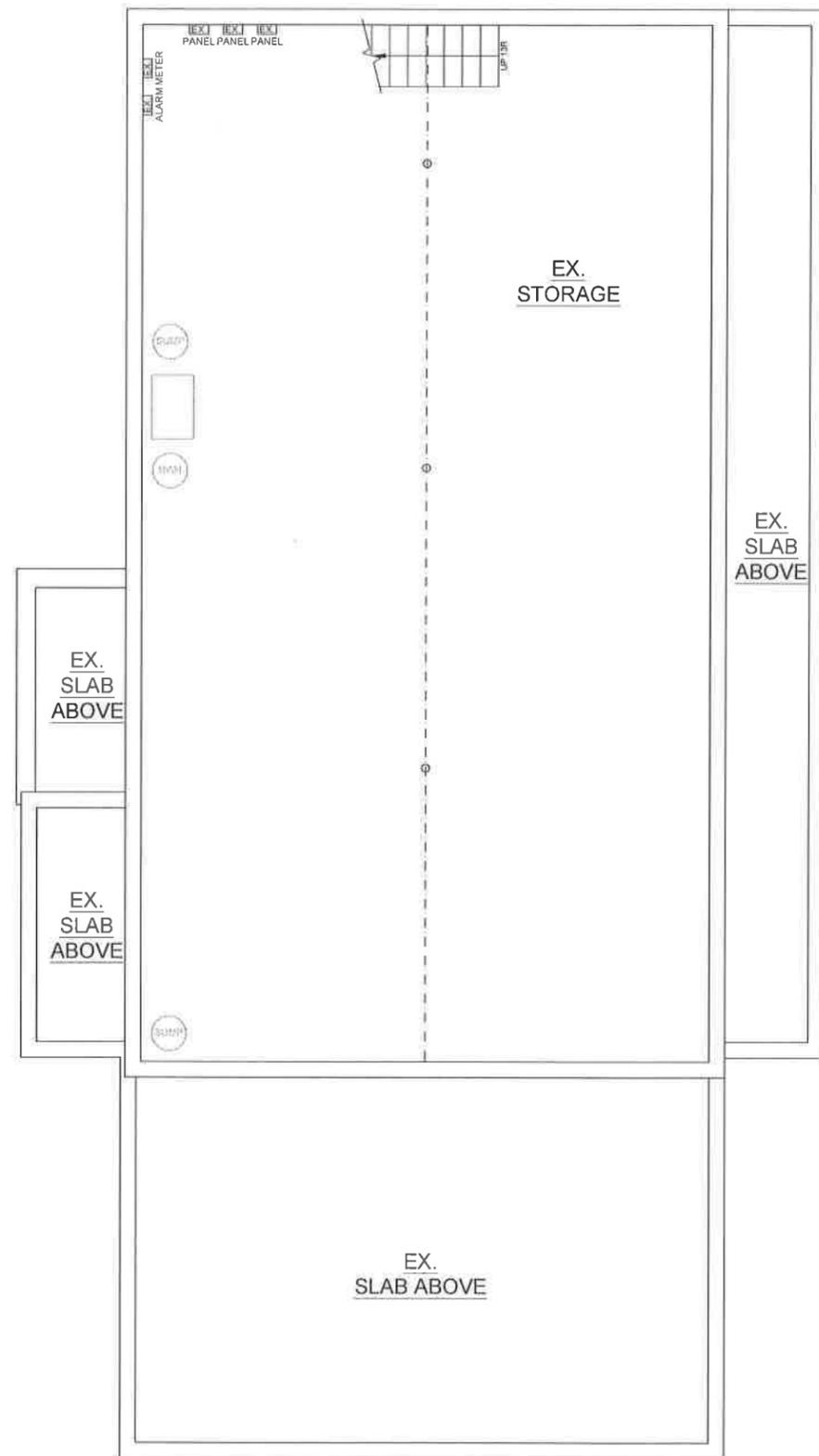


BASEMENT FLOOR PLAN - EXISTING / DEMOLITION



16

BASEMENT FLOOR PLAN - NEW



4

Local Community Signs  
Interior Renovation  
1620 Ogden Avenue  
Downers Grove, Illinois 60515

Number	2016-030
Date	29 June 2016
Drawn	CKL
Checked	CKL
Revised	

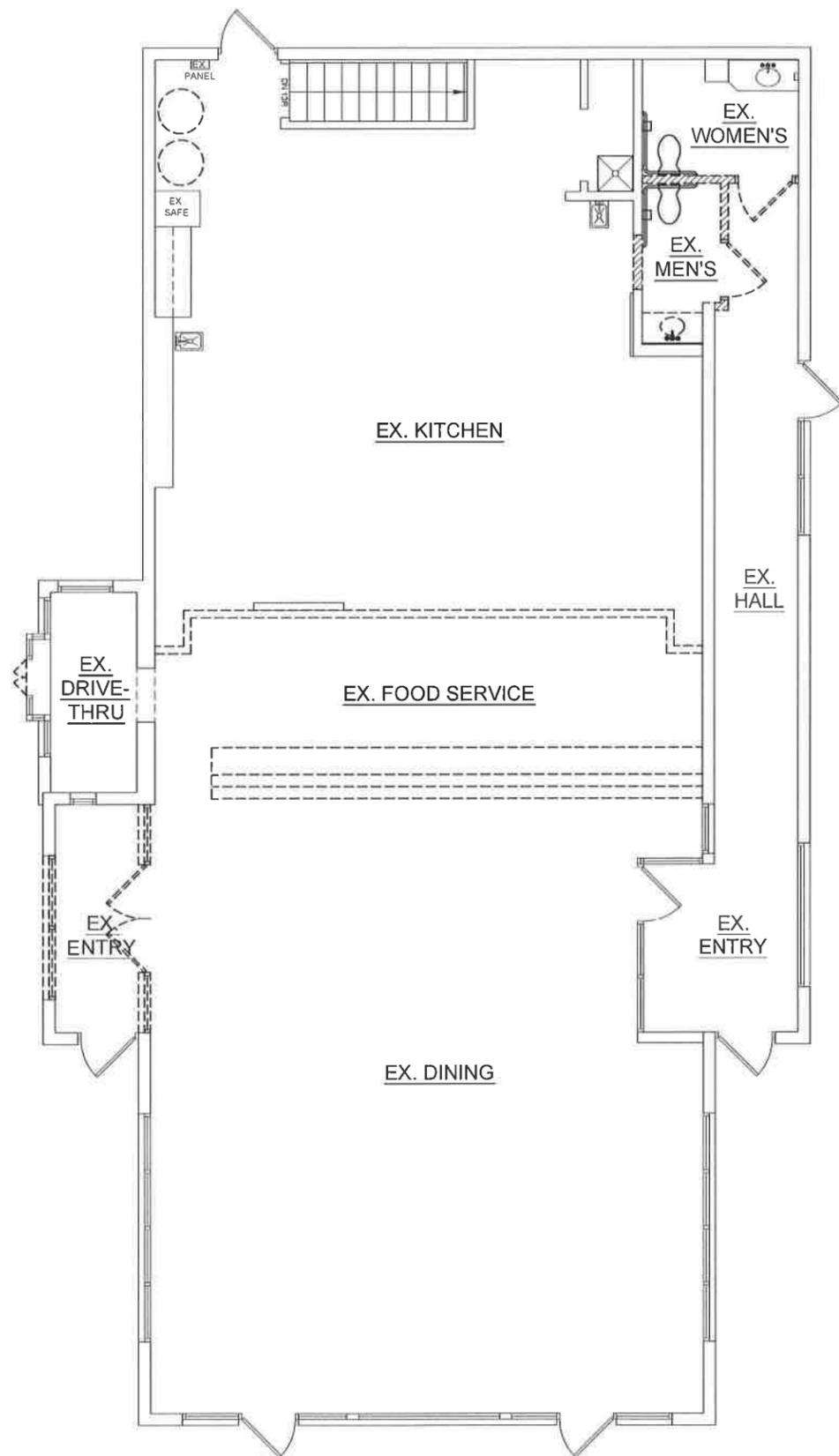


GRAPHIC NORTH

12/12  
architects  
& planners

644 67th Street  
Downers Grove, Illinois 60516  
p. 630.346.1620  
f. 630.493.1817  
w: www.1212architects.com  
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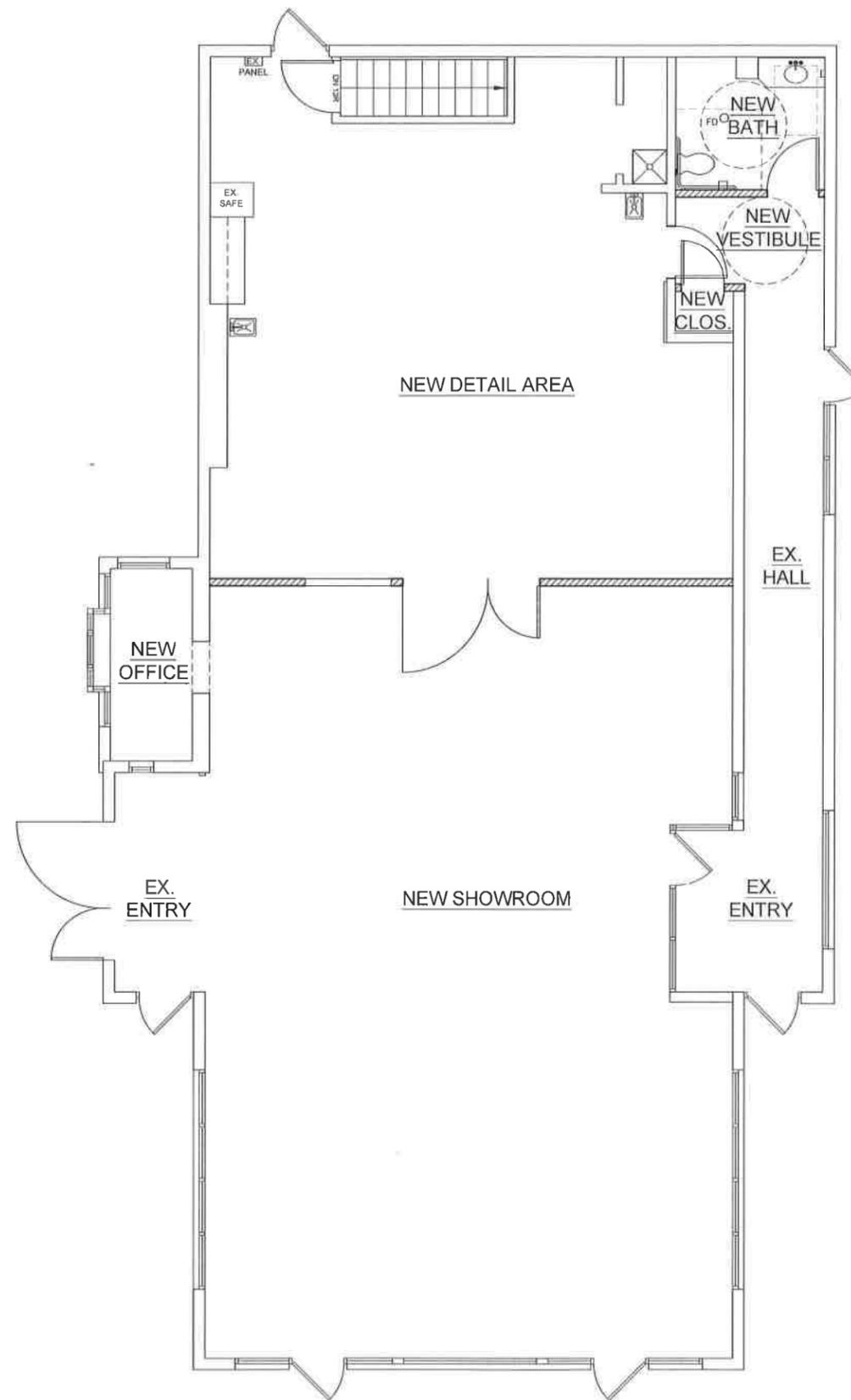
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BASEMENT FLOOR PLAN - EXISTING / DEMO (2,868 SF GROSS)



16



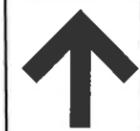
BASEMENT FLOOR PLAN - NEW (2,868 SF GROSS)



4

Local Community Signs  
Interior Renovation  
1620 Ogden Avenue  
Downers Grove, Illinois 60515

Number	2016-030
Date	29 June 2016
Drawn	CKL
Checked	CKL
Revised	



GRAPHIC NORTH

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A1.1



GRAPHIC NORTH



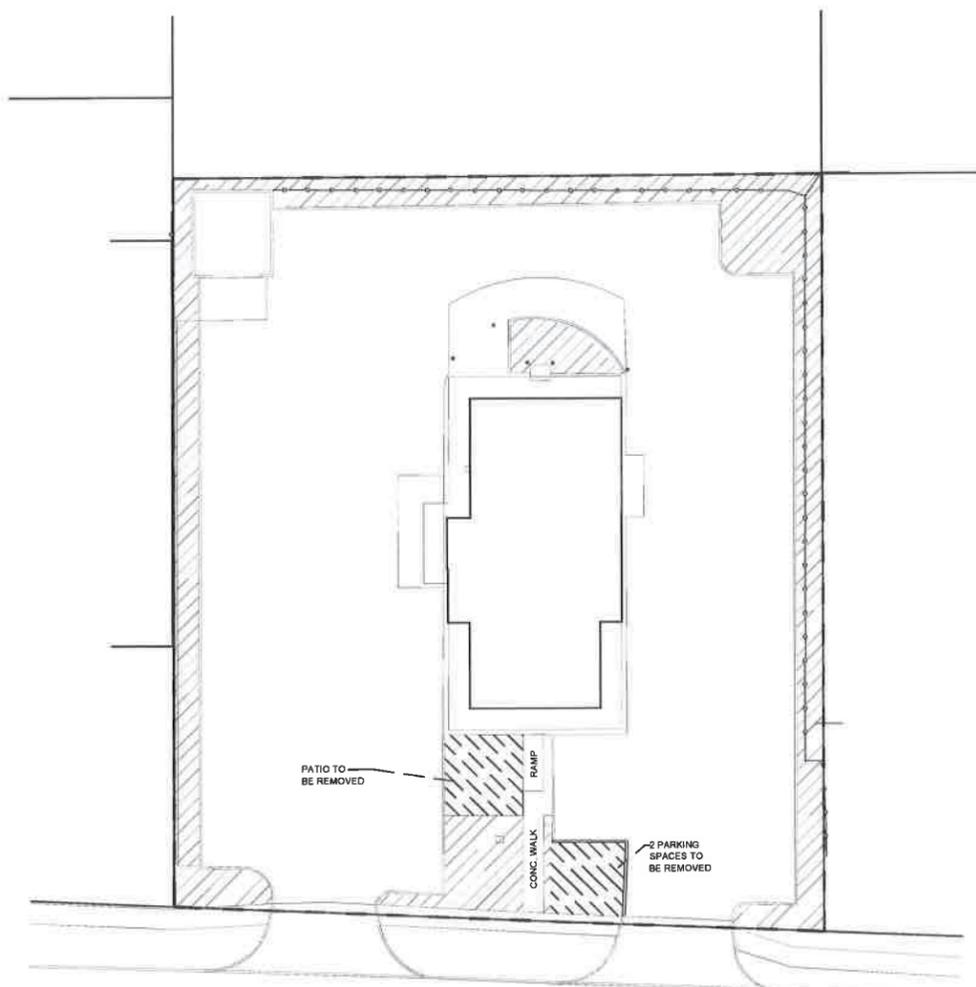
EXISTING 6' WOOD FENCE AT NORTH



EXISTING 6' WOOD FENCE AT EAST

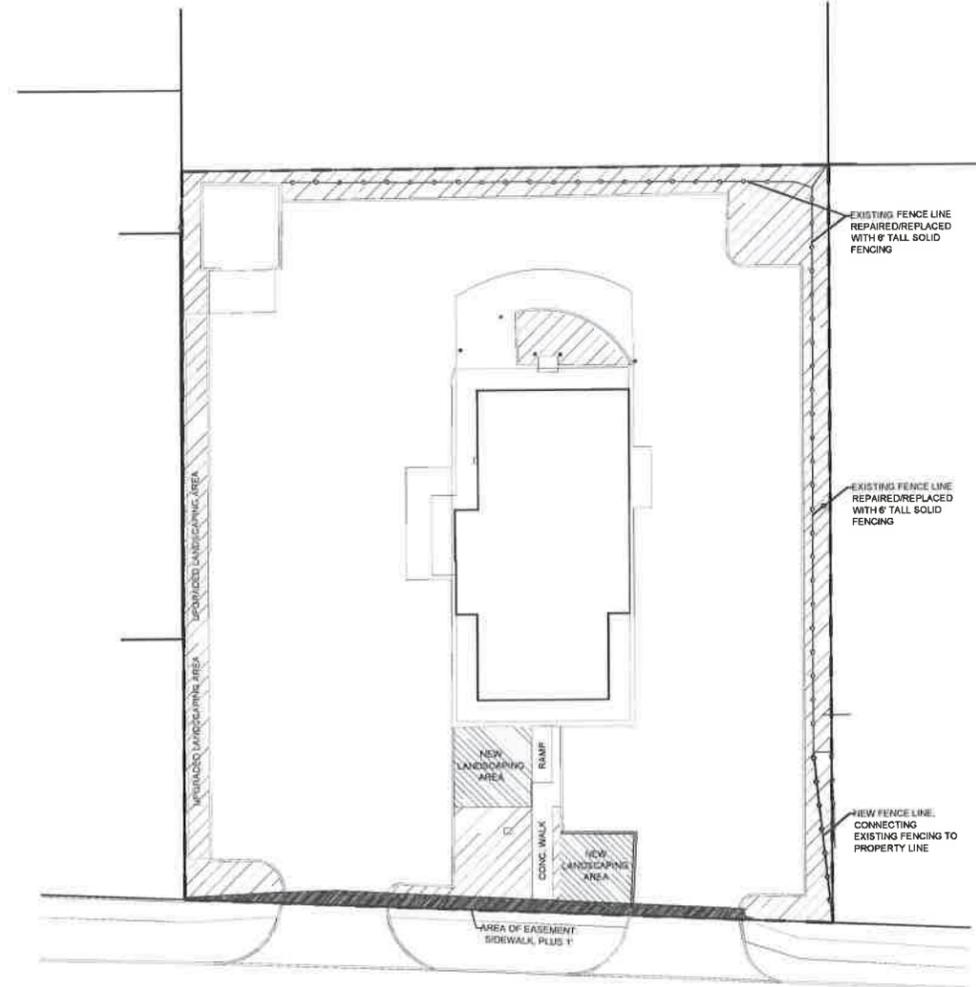


EXISTING LANDSCAPING (PATIO TO BE REMOVED)



MAXIMUM ALLOWED LOT COVERAGE %	=	90%
TOTAL LOT AREA	=	29,300 SF
TOTAL LOT COVERAGE	=	24,660 SF
TOTAL LOT COVERAGE	=	84%

REQUIRED LANDSCAPING % AT FRONT	=	1465 SF (5%)
EXISTING LANDSCAPING COVERAGE AT FRONT		
WEST EXIT	=	209 SF
EAST ENTRANCE	=	135 SF
FRONT SIGN AREA	=	494 SF
EAST OF SIDEWALK NEAR SIGN	=	44 SF
TOTAL LANDSCAPING AT FRONT	=	882 SF (3%)



MAXIMUM ALLOWED LOT COVERAGE %	=	90%
TOTAL LOT AREA	=	29,300 SF
NEW TOTAL LOT COVERAGE	=	24,025 SF
NEW TOTAL LOT COVERAGE %	=	82%

REQUIRED LANDSCAPING % AT FRONT	=	1465 SF (5%)
ACTUAL LANDSCAPING COVERAGE AT FRONT		
WEST EXIT	=	209 SF
EAST ENTRANCE	=	135 SF
FRONT SIGN AREA	=	494 SF
EAST OF SIDEWALK NEAR SIGN	=	44 SF
NEW LANDSCAPING AREA	=	695 SF
TOTAL LANDSCAPING AT FRONT	=	1577 SF (5.4%)

SITE PLAN - EXISTING LANDSCAPING AREA



SCALE: 1" = 20'-0"

16

SITE PLAN - NEW LANDSCAPING AREA



SCALE: 1" = 20'-0"

4



EXISTING LANDSCAPING AT BACK OF BUILDING

13



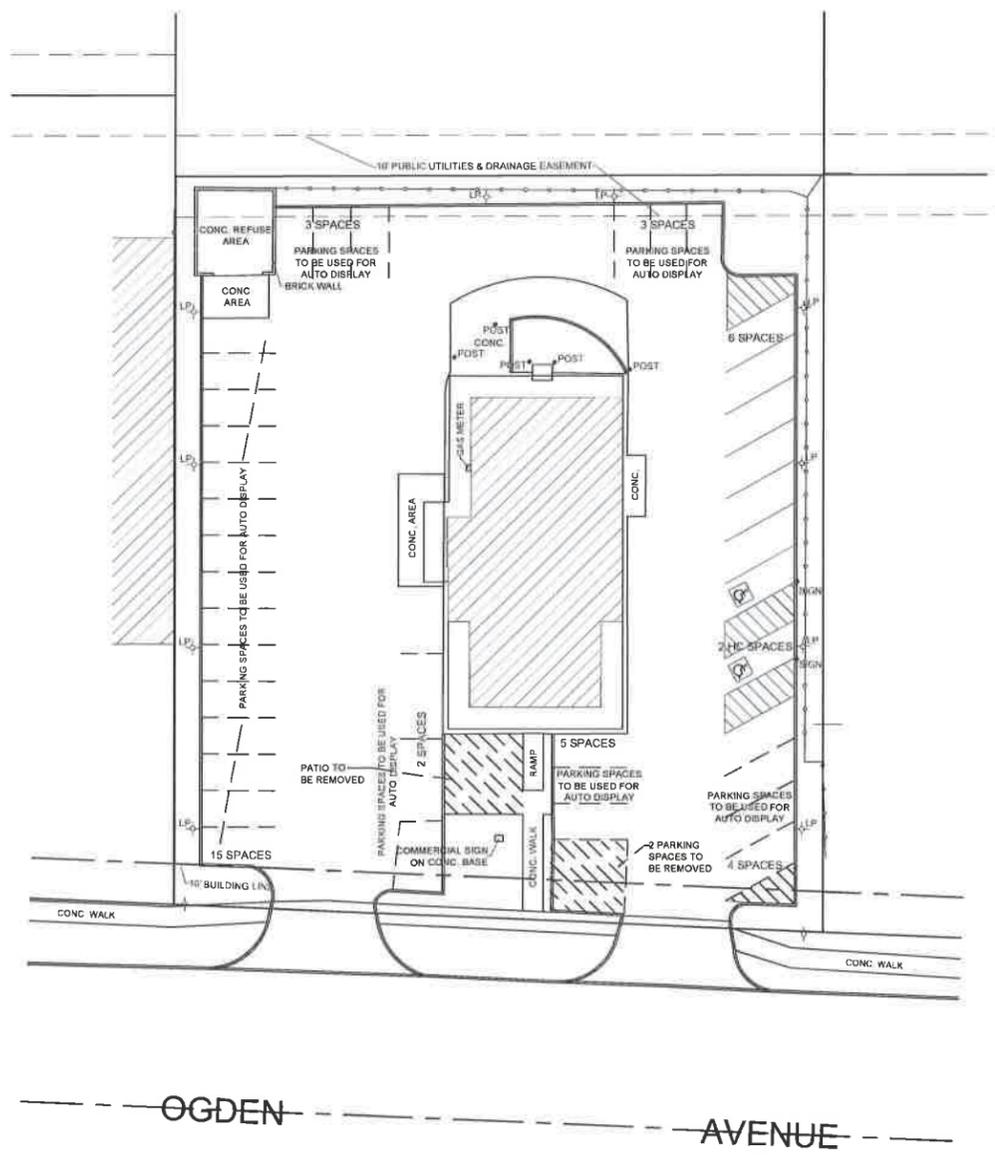
EXISTING LANDSCAPING (PATIO TO BE REMOVED)

5

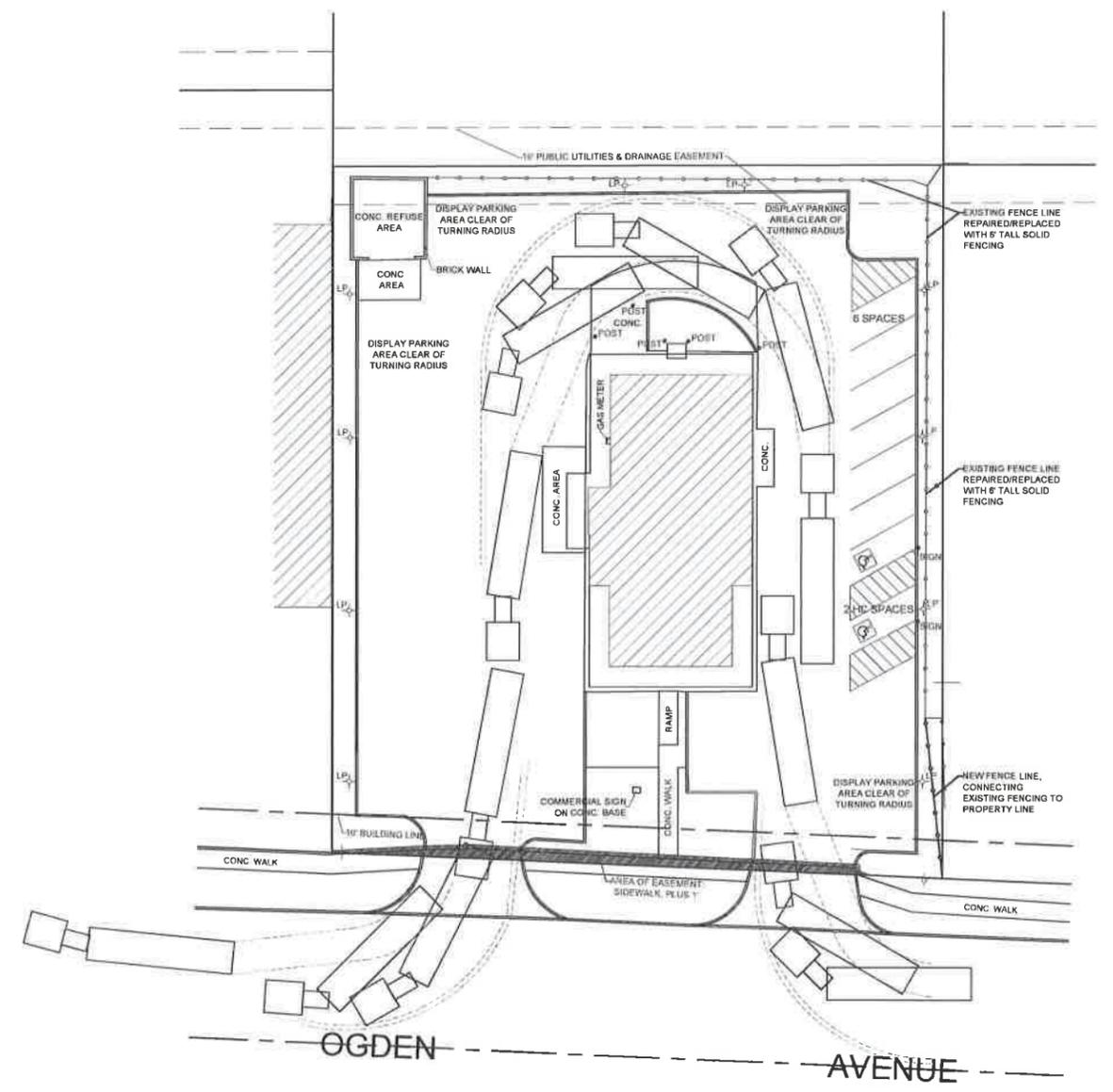


EXISTING LANDSCAPING AT ENTRY DRIVE

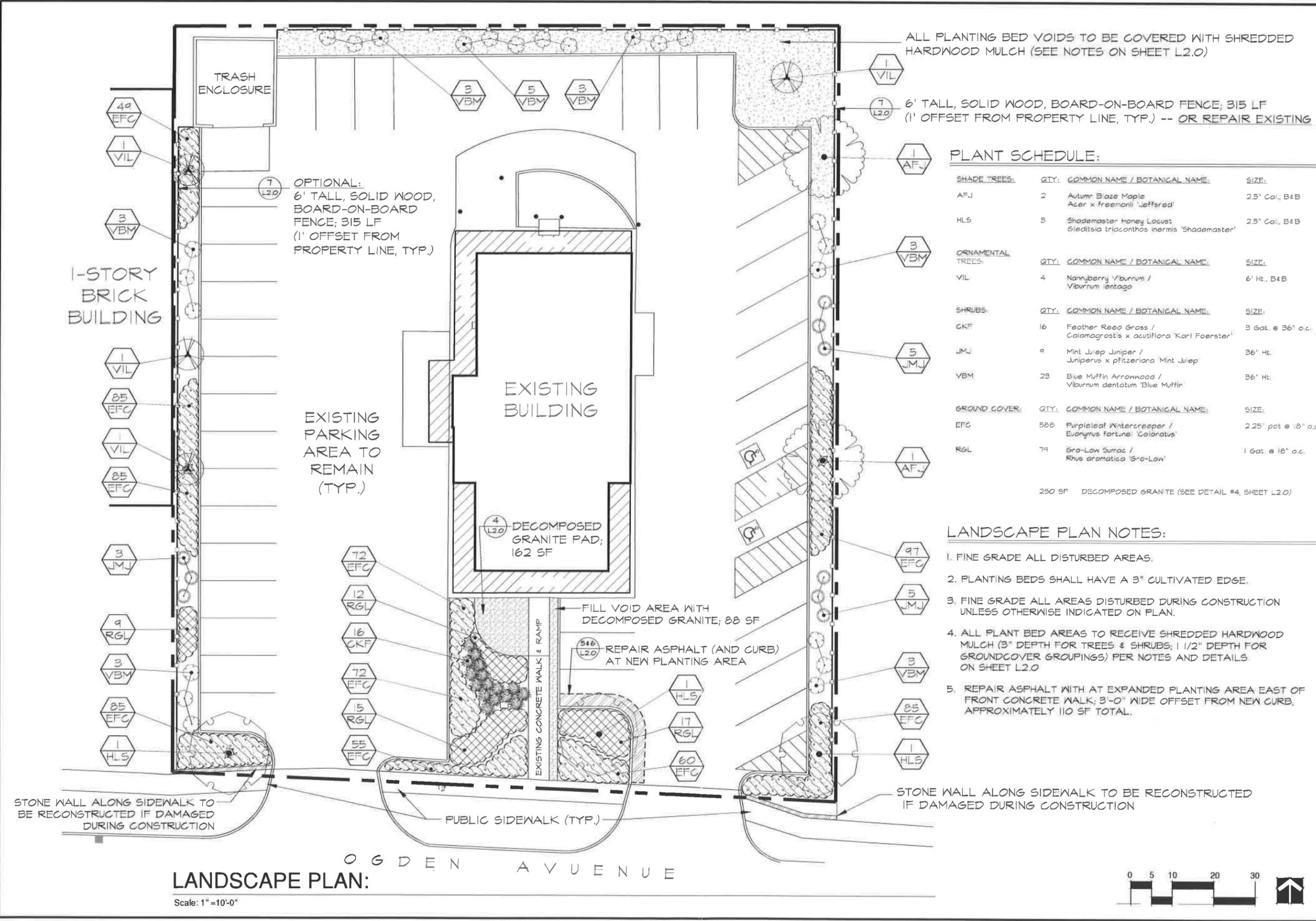
1



SITE PLAN - EXISTING / DEMOLITION (38 SPACES + 2 HC SPACES) SCALE: 1" = 20'-0" 16



SITE PLAN - NEW (6 SPACES + 2 HC SPACES), TRUCK TURNING SCALE: 1" = 20'-0" 4



ALL PLANTING BED VOIDS TO BE COVERED WITH SHREDDED HARDWOOD MULCH (SEE NOTES ON SHEET L2.0)

6' TALL, SOLID WOOD, BOARD-ON-BOARD FENCE; 315 LF (1' OFFSET FROM PROPERTY LINE, TYP.) -- OR REPAIR EXISTING

OPTIONAL:  
6' TALL, SOLID WOOD, BOARD-ON-BOARD FENCE; 315 LF (1' OFFSET FROM PROPERTY LINE, TYP.)

**PLANT SCHEDULE:**

SHADE TREES:	QTY:	COMMON NAME / BOTANICAL NAME:	SIZE:
AFJ	2	Autumn Blaze Maple <i>Acer x freemanii 'Jeffersred'</i>	2.5" Cal., B&B
HLS	3	Shademaster Honey Locust <i>Gleditsia triacanthos inermis 'Shademaster'</i>	2.5" Cal., B&B
ORNAMENTAL TREES:	QTY:	COMMON NAME / BOTANICAL NAME:	SIZE:
VIL	4	Nannyberry Viburnum / <i>Viburnum lentago</i>	6' Ht., B4B
SHRUBS:	QTY:	COMMON NAME / BOTANICAL NAME:	SIZE:
GKF	16	Feather Reed Grass / <i>Calamagrostis x acutiflora 'Karl Foerster'</i>	3 Gat. @ 36" o.c.
JMJ	9	Mint Julep Juniper / <i>Juniperus x pfitzeriana 'Mint Julep'</i>	36" Ht.
VBM	23	Blue Muffin Arrowwood / <i>Viburnum dentatum 'Blue Muffin'</i>	36" Ht.
GROUND COVER:	QTY:	COMMON NAME / BOTANICAL NAME:	SIZE:
EFG	500	Purpleleaf Wintercreeper / <i>Euonymus fortunei 'Coloratus'</i>	2.25" pot @ 18" o.c.
RGL	79	Grass-Low Sumac / <i>Rhus aromatica 'Grass-Low'</i>	1 Gat. @ 18" o.c.

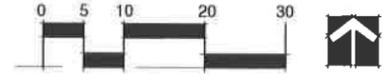
250 SF DECOMPOSED GRANITE (SEE DETAIL #4, SHEET L2.0)

**LANDSCAPE PLAN NOTES:**

1. FINE GRADE ALL DISTURBED AREAS.
2. PLANTING BEDS SHALL HAVE A 3" CULTIVATED EDGE.
3. FINE GRADE ALL AREAS DISTURBED DURING CONSTRUCTION UNLESS OTHERWISE INDICATED ON PLAN.
4. ALL PLANT BED AREAS TO RECEIVE SHREDDED HARDWOOD MULCH (3" DEPTH FOR TREES & SHRUBS; 1 1/2" DEPTH FOR GROUND COVER GROUPINGS) PER NOTES AND DETAILS ON SHEET L2.0
5. REPAIR ASPHALT WITH AT EXPANDED PLANTING AREA EAST OF FRONT CONCRETE WALK; 3'-0" WIDE OFFSET FROM NEW CURB, APPROXIMATELY 110 SF TOTAL.

**LANDSCAPE PLAN:**

Scale: 1" = 10'-0"



**LANDSCAPE PLAN**

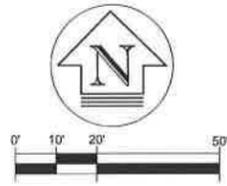
DATE:	08-01-16
SUBMITTAL	
PLAN COMMISSION	

**LOCAL COMMUNITY SIGNS**  
1620 OGDEN AVENUE  
DOWNERS GROVE, ILLINOIS

REVIEWED BY:	DEG
APPROVED BY:	SMH
DRAWN/REVISIONED BY:	DNF
PROJECT NO.:	PP16033-00
DATE:	07.22.16
SCALE:	1" = 10'-0"

**L1.0**





**UNITED SURVEY SERVICE, LLC**

CONSTRUCTION AND LAND SURVEYORS  
 2100 N. 15th AVENUE, SUITE C, MELROSE PARK, IL 60160  
 TEL.: (847) 299 - 1010 FAX : (847) 299 - 5887  
 E-MAIL: USURVEY@USANDCS.COM

**PLAT OF EASEMENT**

OF

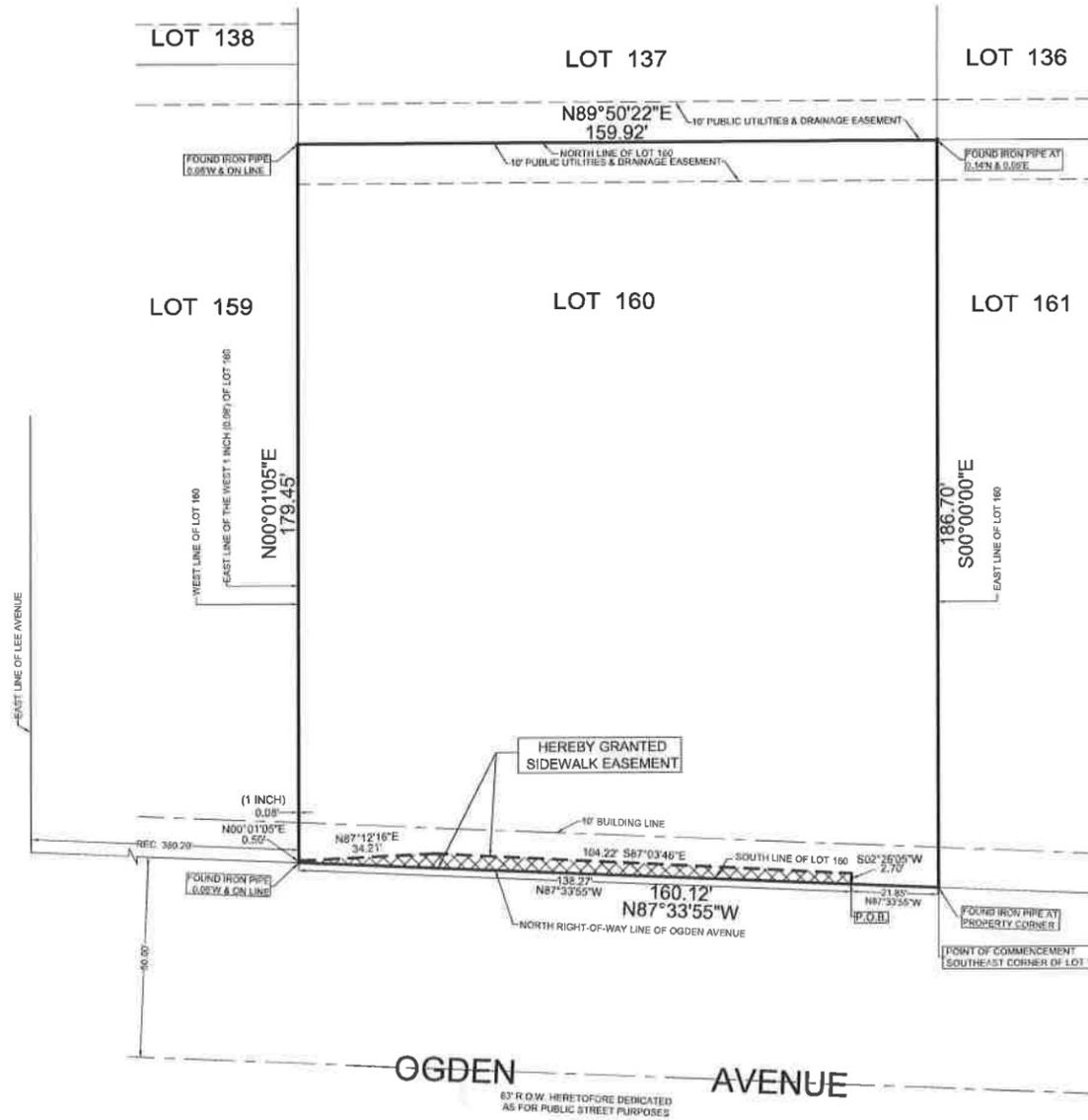
THAT PART OF LOT 160 (EXCEPT THE WEST 1 INCH) IN BURLINGTON HIGHLANDS, BEING A SUBDIVISION OF PART OF SECTION 6, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED AUGUST 26, 1953 AS DOCUMENT 693133, IN DUPAGE COUNTY, ILLINOIS, DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHEAST CORNER OF SAID LOT 160; THENCE NORTH 87 DEGREES 33 MINUTES 55 SECONDS WEST, ALONG THE SOUTH LINE OF SAID LOT 160, ALSO BEING THE NORTH RIGHT-WAY LINE OF OGDEN AVENUE, A DISTANCE OF 21.85 FEET TO THE POINT OF BEGINNING;

THENCE CONTINUING ALONG THE SAID LINE, NORTH 87 DEGREES 33 MINUTES 55 SECONDS WEST, A DISTANCE OF 136.27 FEET; THENCE NORTH 00 DEGREES 01 MINUTE 05 SECONDS EAST, A DISTANCE OF 0.50 FEET; THENCE NORTH 87 DEGREES 12 MINUTES 16 SECONDS EAST, A DISTANCE OF 34.21 FEET; THENCE SOUTH 87 DEGREES 03 MINUTES 46 SECONDS EAST, A DISTANCE OF 104.22 FEET; THENCE SOUTH 02 DEGREES 20 MINUTES 05 SECONDS WEST, A DISTANCE OF 2.70 FEET TO THE POINT OF BEGINNING, CONTAINING 399 SQUARE FEET OR 0.0091 ACRE, MORE OR LESS.

KNOWN AS: **1620 OGDEN AVENUE, DOWNERS GROVE, ILLINOIS**

PERMANENT INDEX NUMBER: 09 - 06 - 302 - 006 - 0000



STATE OF ILLINOIS )  
 ) S.S.  
 COUNTY OF COOK )

I, ROY G. LAWNICZAK, DO HEREBY CERTIFY THAT I HAVE SURVEYED THE ABOVE DESCRIBED PROPERTY AND THAT THE PLAT HEREON DRAWN IS A CORRECT REPRESENTATION OF PLAT OF EASEMENT FOR THE PURPOSE SHOWN HEREON.

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

DIMENSIONS ARE SHOWN IN FEET AND DECIMALS AND ARE CORRECTED TO A TEMPERATURE OF 68° FAHRENHEIT.

MELROSE PARK, ILLINOIS, JULY 21, A.D. 2016

BY: Roy G. Lawniczak  
 ROY G. LAWNICZAK, REGISTERED ILLINOIS LAND SURVEYOR NO. 35-2290  
 LICENSE EXPIRES: NOVEMBER 30, 2016  
 PROFESSIONAL DESIGN FIRM LICENSE NO.: 184-004576  
 LICENSE EXPIRES: APRIL 30, 2017



ORDERED BY: GW PROPERTIES		
SCALE : 1" = 20'		
DATE : JULY 21, 2016		
FILE No.:		
2016-23072-1	DATE	REVISION



## **ANALYSIS**

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### **SUBMITTALS**

This report is based on the following documents, which are on file with the Department of Community Development:

1. Application/Petition for Public Hearing
2. Project Narrative
3. Plat of Survey
4. Site Plan
5. Engineering Plans
6. Parking Study
7. Ordinance 5265

### **PROJECT DESCRIPTION**

The petitioner is requesting an amendment to a Special Use to remove use restrictions set forth in Village Ordinance 5265. The requested amendment would allow all uses, either permitted as of right uses or special uses, expressly identified in the M-1, Light Manufacturing Zoning District, which is the underlying zoning for the property. The site is located on Wisconsin Avenue, 480 feet west of Belmont Road in the Ellsworth Business Park. The approximately five acre property is improved with three multi-tenant buildings, constructed after a Special Use approval in 2012.

As per the Zoning Ordinance in effect in 2012, a Special Use approval was required for multiple buildings on a single lot. When approving the Special Use, Village Council established multiple conditions, one of which restricted the types of uses permitted on the property based on their concerns regarding the high density of the buildings and limited parking on-site. At the public hearing, discussion included a review of allowed uses in the M-1 District and Village Council determined that a condition restricting the types of uses was necessary to limit tenants to less parking intensive uses. The petitioner in 2012 agreed to the conditions of approval for the Special Use.

The petitioner is requesting that the Plan Commission and Village Council reconsider the original use restrictions set forth in Ordinance 5265 because of difficulties leasing the remaining units, as described in the petitioner's project narrative. There are 55 tenant units in the three buildings, with 29 units occupied by a variety of uses including a research and development automotive facility and a craft brewing facility. All current tenants fall into the 2012 Zoning Ordinance use categories of light manufacturing, warehouse, office, research and development. As described in the analysis below, the use categories and specific types of business activities were slightly altered through the 2014 Zoning Ordinance revision.

Ordinance 5265 limits the potential tenants to the following use categories, per the Zoning Ordinance in effect in 2012:

- a) Professional and Business Offices
- b) Research and Development Facilities
- c) Light Manufacturing including production, processing, cleaning, testing and repair limited to uses and products described in 28.902(h)
- d) Wholesale and warehousing, storage and distribution facilities, excluding motor freight terminals or self-storage facilities (mini-warehouses); however, no single tenant shall contain more than 10,000 square feet
- e) Uses accessory to the above-listed permitted uses

Staff supports reconciling the specific use types in the current Zoning Ordinance with the approved use

categories as selected by Village Council through Ordinance 5265. This would expand the permitted use types to include all activities that fell under the 2012 Zoning Ordinance umbrella for the five categories Council approved, regardless of what category the business type correlates to in the 2014 ordinance revision. Through the revision of the Zoning Ordinance, culminating in 2014, some specific use types or business activities were shifted from one category to another, and as a result, are no longer allowed for this development. For example, repair of household or office machinery or equipment and printing and publishing establishments previously fell under item “c” which was classified as “Industrial” in 2012. In the current Zoning Ordinance, these uses fall under the umbrella of “Commercial Service”, not “Industrial”, and thus Staff would no longer consider the activity a legally conforming, permitted use since the use category “Commercial Service” was not approved by Council. Staff is supportive of guaranteeing all uses allowed at the time of Ordinance 5265 remain permitted, regardless of shifting priorities and categories in the Zoning Ordinance.

Staff supports expanding the allowed uses to include the following use types from Table 5-1 in the Zoning Ordinance and described further in Section 5:

- Building service
- Business support service
- Consumer maintenance and repair
- Research service
- Business and professional office
- Medical, dental and health practitioner
- Warehouse
- Wholesale sales and distribution
- Artisan industrial
- Limited industrial
- General industrial

Per the current Zoning Ordinance, the uses allowed in the M-1 District range from public, civic and institutional, commercial, industrial, agriculture to other types of uses. Many of these permitted uses are traffic intensive and generate substantial foot-traffic on the property such as “medical, dental and health practitioner”, “day care center”, and “financial service.” The broader use category, “Vehicle Sales and Service” could also generate more onsite traffic, create circulation and stacking issues, and lead to the policing of outdoor storage of inoperable vehicles.

The petitioner provided a parking study that showed parking as observed on a Wednesday and Friday. According to the parking study, the peak parking demand was observed in the evening between 6:00 – 9:00 PM, influenced by the demand at Alter Brewing Company. Ninety-five out of the 147 parking spaces are outdoor parking spaces. Of the 95 outdoor spaces, on Friday evening at 9:00 PM, 50 spaces were occupied. As stated in the parking study, shared parking is working because of the varied peak parking characteristics. However, if the property was opened up to more parking intensive uses with evening competing hours or onsite storage and stacking of vehicles, Staff believes that overall parking could be significantly impacted as one use occupies one third of the parking lot at peak time.

#### **COMPLIANCE WITH THE COMPREHENSIVE PLAN**

This property is designated as Light Industrial/Business Park in the Future Land Use Plan. Light Industrial/Business Park uses include manufacturing activities such as design, assembly, processing, packaging, storage and transportation of products. The property at 2300 Wisconsin Avenue continues to be in broad compliance with the Comprehensive Plan and the proposed amendment to the Special Use does not affect the overall land use or character. However, the Comprehensive Plan recommends the uses to be restricted to light industrial and office, thus protecting from the non-compatible commercial, institutional, membership, or recreational uses.

The Comprehensive Plan identifies the property as a catalyst site within the Belmont/Ellsworth Business Park, and since the construction of the three buildings, remains a flexible tenant space for contemporary light industrial and business uses. The development is in close proximity to I-355 and I-88 and complements the existing uses in the Ellsworth Industrial Park.

#### **COMPLIANCE WITH THE ZONING ORDINANCE**

The subject property is zoned M-1, Light Manufacturing, as are the properties to the north, south and west. Properties to the east of the subject property are zoned M-1, Light Manufacturing and O-R-M, Office-Research-Manufacturing. The development was approved through a Special Use process. No changes are being proposed that would affect the site or the buildings on the property. The allowed uses on the property fall under the categories permitted in Ordinance 5265. These uses are a restricted subgroup of uses allowed in the underlying M-1 Zoning District.

Required parking is calculated based on the parking factor in the zoning ordinance for the proposed use and the square footage of the tenant space. Staff tracks the available parking and tenant space for the project site. The current total number of required parking spaces from a zoning perspective is 66 of the total 147 parking spaces on the property.

#### **ENGINEERING/PUBLIC IMPROVEMENTS**

The petitioner is not proposing any changes to the site that would result in public improvements. All required infrastructure exists and adequately services the property.

#### **NEIGHBORHOOD COMMENT**

Notice was provided to all property owners within 250 feet of the subject property in addition to posting a public hearing sign on the subject property and publishing a notice in *Downers Grove Suburban Life*. Staff has received one inquiry about the petition expressing concerns related to parking and the potential spillover of parking onto the neighboring property.

#### **FINDINGS OF FACT**

The petitioner is requesting an amendment to an approved Special Use to allow all uses expressly permitted in the M-1 Zoning District at 2300 Wisconsin Avenue. Staff finds that the proposal does not meet all of the standards for granting a Special Use as outlined below:

##### ***Section 28.12.050.H Approval Criteria***

*No special use may be recommended for approval or approved unless the respective review or decision-making body determines that the proposed special use is constituent with and in substantial compliance with all Village Council policies and plans and that the applicant has presented evidence to support each of the following conclusions:*

- 1. That the proposed use is expressly authorized as a Special Use in the district in which it is to be located;***  
The property is located in the M-1, Light Manufacturing zoning district. The development was approved through a Special Use process under a previous version of the Zoning Ordinance. The current uses on the property are consistent with Ordinance 5265 and are permitted in the underlying zoning district and the current Zoning Ordinance. The proposed amendment to the Special Use is to provide the option to consider all uses allowed in the M-1, Light Manufacturing zoning district. This standard has been met.
- 2. That the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.***

While the uses permitted under the M-1 zoning district are compatible with the surrounding properties similarly zoned, the high density of this development creates the unique challenge of adequately allocating

available parking across various uses and tenant spaces. The Special Use approving three buildings with 55 units on the property in 2012 identified some traffic, circulation and parking challenges and therefore conditions were placed by the Village Council to limit the types of uses to uses that would be less parking intensive and commensurate with the development plan. By expanding the allowed uses to all uses permitted in the M-1 District, parking overspill and parking in access drive aisles may result. There are no site changes proposed that affect the original determination by Village Council. Therefore, the proposed amendment is not in the interest of public convenience and does not contribute to the general welfare of the neighborhood or community. This standard is not met.

3. *That the proposed use will not, in the particular case, be detrimental to the health, safety or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.*

The proposal to allow all uses in the M-1 zoning district includes uses that generate a greater traffic and parking demand as discussed above. It includes the potential for auto-oriented uses, such as personal vehicle repair and maintenance, to generate heavier traffic and parking needs. Given the tight circulation and site constraints, some typical M-1 allowed uses may not be compatible with the site. Also, at the time of the original development, the site plan did not take into account different elements that would have allowed for greater flexibility with uses such as pedestrian access, circulation, stacking and other issues. Therefore, the proposed use will be detrimental to the health, safety or general welfare of persons residing in or working in the vicinity. This standard has not been met.

## RECOMMENDATIONS

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The request to amend the Special Use Ordinance for 2300 Wisconsin Avenue, to remove all use restrictions and allow all uses expressly permitted in the M-1 Zoning District, is not consistent with Ordinance 5625 as adopted by Village Council in 2012. No change in the conditions or development have occurred since Council first placed the use restrictions. Based on the findings listed in this report, Staff recommends that the petitioner's request be **denied**; however Staff recommends amending the approved uses from 2012 to the categories in the current Zoning Ordinance shown below in the conditions:

1. The Special Use shall substantially conform to the staff report, except as such plans may be modified to conform to the Village codes and ordinances.
2. The property may be occupied by the following use types from Table 5-1 in the Zoning Ordinance:
  - a. Building service
  - b. Business support service
  - c. Consumer maintenance and repair
  - d. Research service
  - e. Business and professional office
  - f. Medical, dental and health practitioner
  - g. Warehouse
  - h. Wholesale sales and distribution
  - i. Artisan industrial
  - j. Limited industrial
  - k. General industrial
3. The petitioner shall provide the Village with updated parking vacancy numbers for the entire site with every new tenant use.
4. If future tenant uses are allowable Special Uses per the results of this petition, an application to Plan Commission for approval of the Special Use must still be sought.

Staff Report Approved By:



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Stanley J. Popovich, AICP  
Director of Community Development

SP:sp  
-att

P:\P&CD\PROJECTS\PLAN COMMISSION\2016 PC Petition Files\16-PLC-0033 2300 Wisconsin Ave - Special Use\Staff Report 16-PLC-0033.docx

## PROJECT NARRATIVE

MacNeil Real Estate Holdings, LLC (“Owner”) is the current owner of the property located at 2300 Wisconsin Avenue (“Property”) in Downers Grove. On May 15, 2012, the Village Council approved a special use to allow construction on the Property of three principal buildings on a single lot within the M-1 Light Industrial District (“Ordinance.”) The ordinance approving the Special Use limited the uses allowed on the Property to the following:

- a. Professional and Business Offices.
- b. Research and Development Facilities.
- c. Light Manufacturing including production, processing, cleaning, testing and repair limited to uses and products described in 28.902(h).
- d. Wholesale and warehousing, storage and distribution facilities, excluding motor freight terminals or self-storage facilities (mini-warehouses); however, no single tenant shall contain more than 10,000 square feet.
- e. Uses accessory to the above-listed permitted uses.

The project was completed and opened on October 1, 2014. Since that time, it has encountered challenges leasing the units due to the limits placed on the allowable uses. Currently, only 29 of 55 total units are leased (as are depicted on the attached site plan and list of existing tenants.) The Owner has actively marketed the property over the past 21 months, to no avail. Advertisements for leasing opportunities have been published in the Naperville Sun and the Doings Newspaper, which serves Hinsdale, Clarendon Hills, La Grange (Park), Western Springs, Oak Brook, Elmhurst, Burr Ridge, Indian Head Park, Oakbrook Terrace and Westchester. In addition, advertisements have been placed on various commercial real estate websites including LoopNet, City Feet, and CoStar. Failure to fully lease the Property for such an extended period of time is making the financial success of the development difficult to achieve.

There have been a minimum of seven potential uses that would have been an authorized permitted or special use in the underlying M-1 District that have been rejected by the Village of Downers Grove due to the use limitations imposed by the Ordinance. These users included a wholesale facility, personal trainer, and various motor vehicle uses. Almost all of the rejected uses would have been allowed by the M-1 District regulations as a permitted or special use. This option was not available to the Owner due to the Ordinance. Minutes of the Plan Commission and Village Board are silent as to why the additional restrictions were imposed by the Ordinance and current Village staff is unable to clarify what happened due to staff turnover.

The Owner is filing an application seeking to amend the existing Ordinance to remove the limitation on uses and permit the full range of authorized permitted and special uses allowed in the underlying M-1 District with the goal of fully leasing the development. The Owner simply seeks to be treated the same as any other owner of property located in the M-1 District and have

the right to make a case that an authorized use meets the Code requirements. Many of the rejected uses are deemed a “special use” under the M-1 regulations. The special use process would require a hearing where evidence can be introduced that parking is available to serve the use and conditions can be placed on that use if deemed required. The Owner believes that this would be a more fair approach and allow more flexibility to lease the vacant spaces.

Owner is seeking consistent application of the Downers Grove Zoning Code. There are a number of uses such as “Animal Service” that has a similar parking requirement to the “Professional and Business Office” which is permitted by the Ordinance. It is unclear why “Animal Service” is not allowed on the Property while it has a similar parking requirement as a use allowed by the Ordinance and is allowed on other properties zoned M-1. Another example is “Personal Improvement Service” which has a similar parking requirement. This use is allowed down the street at 2474 Wisconsin Avenue, which is also located in the M-1 District, but is not allowed on the Property.

As the Parking Study prepared by KLOA concluded, existing uses are generating parking at a ratio less than anticipated allowing more parking to be utilized by future tenants. In addition, the development is an example where shared parking is working. Existing tenants with different peak parking periods have resulted in less occupied parking during the business day. This data supports the request for an amendment to the Ordinance to allow a greater variety of uses on site.

Full occupancy of the building is in the interest of the Downers Grove due to the jobs created and enhanced tax revenues paid to the Village. As an owner of three properties located within the Ellsworth Industrial Park, Owner has a history of being a responsible landlord and has strict Tenant Rules in place to ensure that this remains a high quality development from an aesthetic and operational perspective. Owner believes that the flexibility provided by the ability to lease to a larger group of potential tenants will result in the success of this project and an enhanced manufacturing environment for Downers Grove.

Compliance with Special Use Standards

The Owner believes that the proposed amendment to the special use complies with the standards contained in the Downers Grove Zoning Ordinance which govern approval of a special use:

**“1. that the proposed use is expressly authorized as a special use in the district in which it is to be located;”**

This standard is not applicable since the purpose of the original special use (i.e. to allow three principal buildings on one zoning lot) has been effectuated by successfully constructing the development in 2014.

**“2. that the proposed use at the proposed location is necessary or desirable to provide a service or a facility that is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community;”**

The Owner is simply seeking equity by having the Village apply the same rules to the subject property as are applied to other properties located within M-1 zoning districts located throughout Downers Grove. If a use is permitted in a M-1 District, it should be allowed as of right. If the proposed use is an authorized special use, the Owner should have the right to make its case to the Village that the proposed use is consistent with the special use standards.

It is in the interest of the public convenience to fully lease the development. This will make a broader range of uses available to meet the needs of the citizens of Downers Grove and result in increased tax revenues to the Village. Failure to remove the lease restrictions will continue to hinder leasing efforts and prevent the development from becoming a successful, productive property benefitting the Village.

**“3. that the proposed use will not, in the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the vicinity or be injurious to property values or improvements in the vicinity.”**

The Owner is seeking to remove the use restrictions from applying to the subject property and be treated like any other property owner in a M-1 District. Any use to be located within the buildings will comply with the underlying zoning and, thus, be consistent with surrounding uses and not be detrimental to the health, safety, or general welfare of persons working in the vicinity. In addition, this consistency with the underlying plan for the area will ensure that there are no adverse effects on property values or improvements in the vicinity. The Owner is concerned that a failure to fully lease the building and have a

Mr. Stan Popovich  
July 1, 2016

successful development will ultimately adversely impact property values and the general welfare of nearby workers.

We look forward to working with you to answer any questions that you may have regarding the proposed amendment to the special use. Please let us know if any further information or documentation is required for your review.

Thank you in advance for your assistance with this matter.

Sincerely,

A handwritten signature in black ink that reads "Bridget O'Keefe" followed by a stylized flourish.

Bridget O'Keefe

Attachments

cc: David MacNeil (via e-mail w/out attachments)  
Richard Curran (via e-mail w/out attachments)  
Kevin Greyhill (via e-mail w/out attachments)  
Robert Aument (via e-mail w/out attachments)

WISCONSIN AVENUE

UNIT 105	UNIT 104	UNIT 103	UNIT 102	UNIT 101
1,610 Square Feet	1,610 Square Feet	1,610 Square Feet	1,610 Square Feet	1,560 Square Feet

UNIT 213	UNIT 212	UNIT 211	UNIT 210	UNIT 209	UNIT 208	EMPTY				UNIT 207	UNIT 206	UNIT 205	UNIT 204	UNIT 203	UNIT 202	UNIT 201
1,650 Square Feet	1,600 Square Feet															

UNIT 214	UNIT 215	UNIT 216	UNIT 217	UNIT 218	UNIT 219	UNIT 220	UNIT 221	UNIT 222	UNIT 223	UNIT 224	UNIT 225	UNIT 226
1,650 Square Feet												

UNIT 312	UNIT 311	UNIT 310	UNIT 309	UNIT 308	UNIT 307	UNIT 306	UNIT 305	UNIT 304	UNIT 303	UNIT 302	UNIT 301
1,750 Square Feet	1,600 Square Feet	1,700 Square Feet									

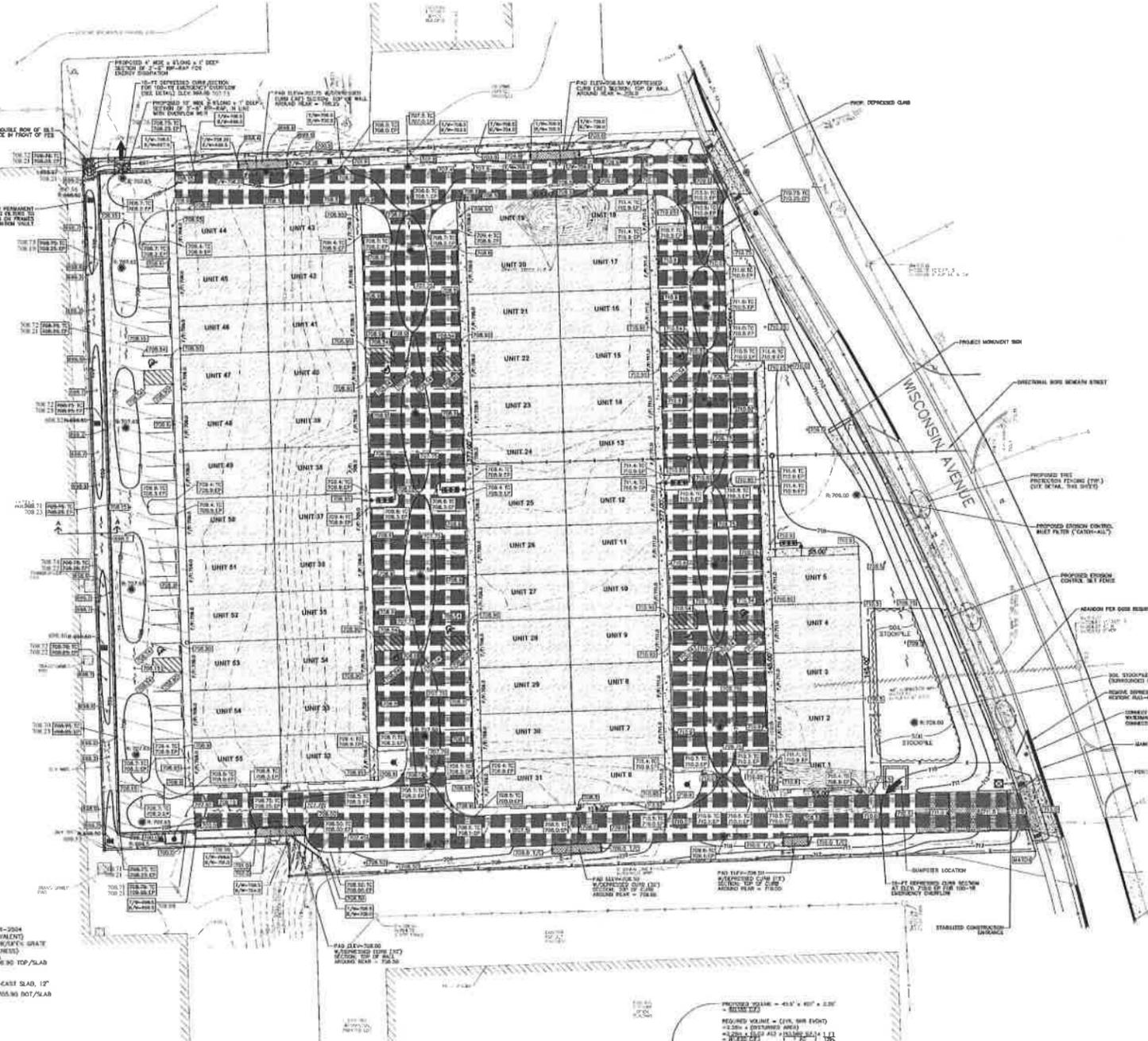
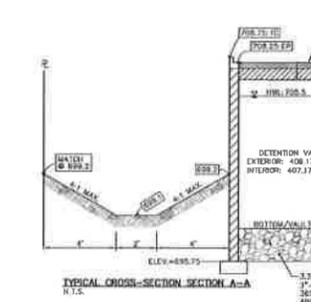
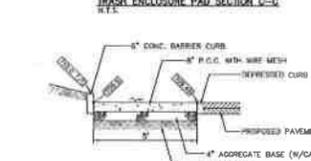
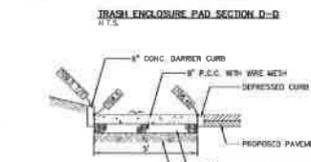
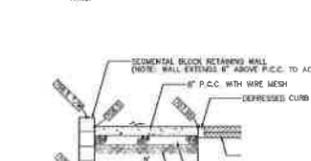
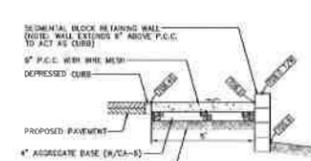
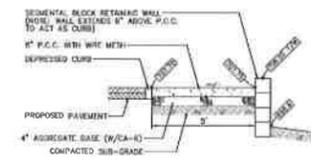
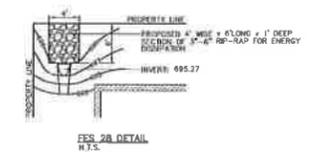
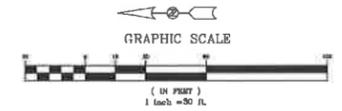
UNIT 313	UNIT 314	UNIT 315	UNIT 316	UNIT 317	UNIT 318	UNIT 319	UNIT 320	UNIT 321	UNIT 322	UNIT 323	UNIT 324
1,750 Square Feet	1,600 Square Feet	1,750 Square Feet									

ADD 44 SQ. FT. PER DEMISING WALL THAT IS NOT INSTALLED

## 2300 WISCONSIN AVENUE

2300 Wisconsin Avenue | Downers Grove, IL 60515  
 Phone: 800-943-9250 | 630-769-1500 | Fax: 630-769-0300

# RECORD/AS BUILTS 9-15-2014



**STORMWATER DETENTION TABLE**

DETONATION REQUIRED = 104,849 CF = 2.407 AC-FT  
 DETENTION PROVIDED = 120,417 CF = 2.784 AC-FT  
 - 11% = 705.5  
 - BOTTOM OF VAULT = 899.00  
 - RESTRICTOR = 2.375' AT INV 899.00 LOCATED IN STORM STRUCTURE #23

**STAGE STORAGE, DISCHARGE RELATIONSHIP**

Stage (ft)	Storage (cu ft)	Discharge (cfs)
899.00	0.00	0.00
899.50	1000.00	10.00
900.00	4000.00	40.00
900.50	9000.00	90.00
901.00	16000.00	160.00
901.50	25000.00	250.00
902.00	36000.00	360.00
902.50	49000.00	490.00
903.00	64000.00	640.00
903.50	81000.00	810.00
904.00	100000.00	1000.00
904.50	121000.00	1210.00
905.00	144000.00	1440.00
905.50	169000.00	1690.00
906.00	196000.00	1960.00
906.50	225000.00	2250.00
907.00	256000.00	2560.00
907.50	289000.00	2890.00
908.00	324000.00	3240.00
908.50	361000.00	3610.00
909.00	400000.00	4000.00
909.50	441000.00	4410.00
910.00	484000.00	4840.00
910.50	529000.00	5290.00
911.00	576000.00	5760.00
911.50	625000.00	6250.00
912.00	676000.00	6760.00
912.50	729000.00	7290.00
913.00	784000.00	7840.00
913.50	841000.00	8410.00
914.00	900000.00	9000.00
914.50	961000.00	9610.00
915.00	1024000.00	10240.00
915.50	1089000.00	10890.00
916.00	1156000.00	11560.00
916.50	1225000.00	12250.00
917.00	1296000.00	12960.00
917.50	1369000.00	13690.00
918.00	1444000.00	14440.00
918.50	1521000.00	15210.00
919.00	1600000.00	16000.00
919.50	1681000.00	16810.00
920.00	1764000.00	17640.00
920.50	1849000.00	18490.00
921.00	1936000.00	19360.00
921.50	2025000.00	20250.00
922.00	2116000.00	21160.00
922.50	2209000.00	22090.00
923.00	2304000.00	23040.00
923.50	2401000.00	24010.00
924.00	2500000.00	25000.00
924.50	2601000.00	26010.00
925.00	2704000.00	27040.00
925.50	2809000.00	28090.00
926.00	2916000.00	29160.00
926.50	3025000.00	30250.00
927.00	3136000.00	31360.00
927.50	3249000.00	32490.00
928.00	3364000.00	33640.00
928.50	3481000.00	34810.00
929.00	3600000.00	36000.00
929.50	3721000.00	37210.00
930.00	3844000.00	38440.00
930.50	3969000.00	39690.00
931.00	4096000.00	40960.00
931.50	4225000.00	42250.00
932.00	4356000.00	43560.00
932.50	4489000.00	44890.00
933.00	4624000.00	46240.00
933.50	4761000.00	47610.00
934.00	4900000.00	49000.00
934.50	5041000.00	50410.00
935.00	5184000.00	51840.00
935.50	5329000.00	53290.00
936.00	5476000.00	54760.00
936.50	5625000.00	56250.00
937.00	5776000.00	57760.00
937.50	5929000.00	59290.00
938.00	6084000.00	60840.00
938.50	6241000.00	62410.00
939.00	6400000.00	64000.00
939.50	6561000.00	65610.00
940.00	6724000.00	67240.00
940.50	6889000.00	68890.00
941.00	7056000.00	70560.00
941.50	7225000.00	72250.00
942.00	7396000.00	73960.00
942.50	7569000.00	75690.00
943.00	7744000.00	77440.00
943.50	7921000.00	79210.00
944.00	8100000.00	81000.00
944.50	8281000.00	82810.00
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945.50	8649000.00	86490.00
946.00	8836000.00	88360.00
946.50	9025000.00	90250.00
947.00	9216000.00	92160.00
947.50	9409000.00	94090.00
948.00	9604000.00	96040.00
948.50	9801000.00	98010.00
949.00	10000000.00	100000.00

**STORMWATER DETENTION TABLE**

DETONATION REQUIRED = 104,849 CF = 2.407 AC-FT  
 DETENTION PROVIDED = 120,417 CF = 2.784 AC-FT  
 - 11% = 705.5  
 - BOTTOM OF VAULT = 899.00  
 - RESTRICTOR = 2.375' AT INV 899.00 LOCATED IN STORM STRUCTURE #23

**STAGE STORAGE, DISCHARGE RELATIONSHIP**

Stage (ft)	Storage (cu ft)	Discharge (cfs)
899.00	0.00	0.00
899.50	1000.00	10.00
900.00	4000.00	40.00
900.50	9000.00	90.00
901.00	16000.00	160.00
901.50	25000.00	250.00
902.00	36000.00	360.00
902.50	49000.00	490.00
903.00	64000.00	640.00
903.50	81000.00	810.00
904.00	100000.00	1000.00
904.50	121000.00	1210.00
905.00	144000.00	1440.00
905.50	169000.00	1690.00
906.00	196000.00	1960.00
906.50	225000.00	2250.00
907.00	256000.00	2560.00
907.50	289000.00	2890.00
908.00	324000.00	3240.00
908.50	361000.00	3610.00
909.00	400000.00	4000.00
909.50	441000.00	4410.00
910.00	484000.00	4840.00
910.50	529000.00	5290.00
911.00	576000.00	5760.00
911.50	625000.00	6250.00
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947.00	9216000.00	92160.00
947.50	9409000.00	94090.00
948.00	9604000.00	96040.00
948.50	9801000.00	98010.00
949.00	10000000.00	100000.00

**REVISIONS:**

DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION
8/27/12	RT	VILLAGE COMMENTS			
12/7/12	AJ	VILLAGE COMMENTS			
7/16/13	DK	VILLAGE COMMENTS			

**ENGINEERING RESOURCE ASSOCIATES, INC.**  
 35701 WEST AVENUE, SUITE 150  
 WARRENVILLE, ILLINOIS 60555  
 PHONE (630) 393-3300  
 FAX (630) 393-2152

**PREPARED FOR:**  
**MACNEIL AUTOMOTIVE PRODUCTS, LTD.**  
 1 MACNEIL COURT  
 BOLINGBROOK, ILLINOIS 60440

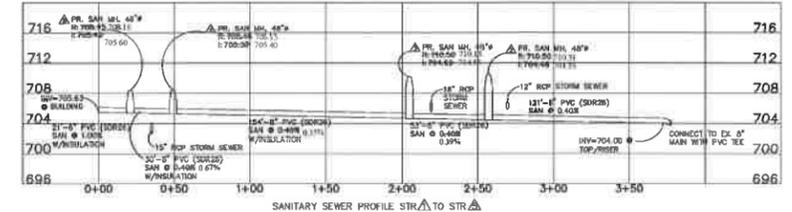
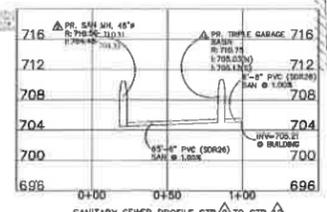
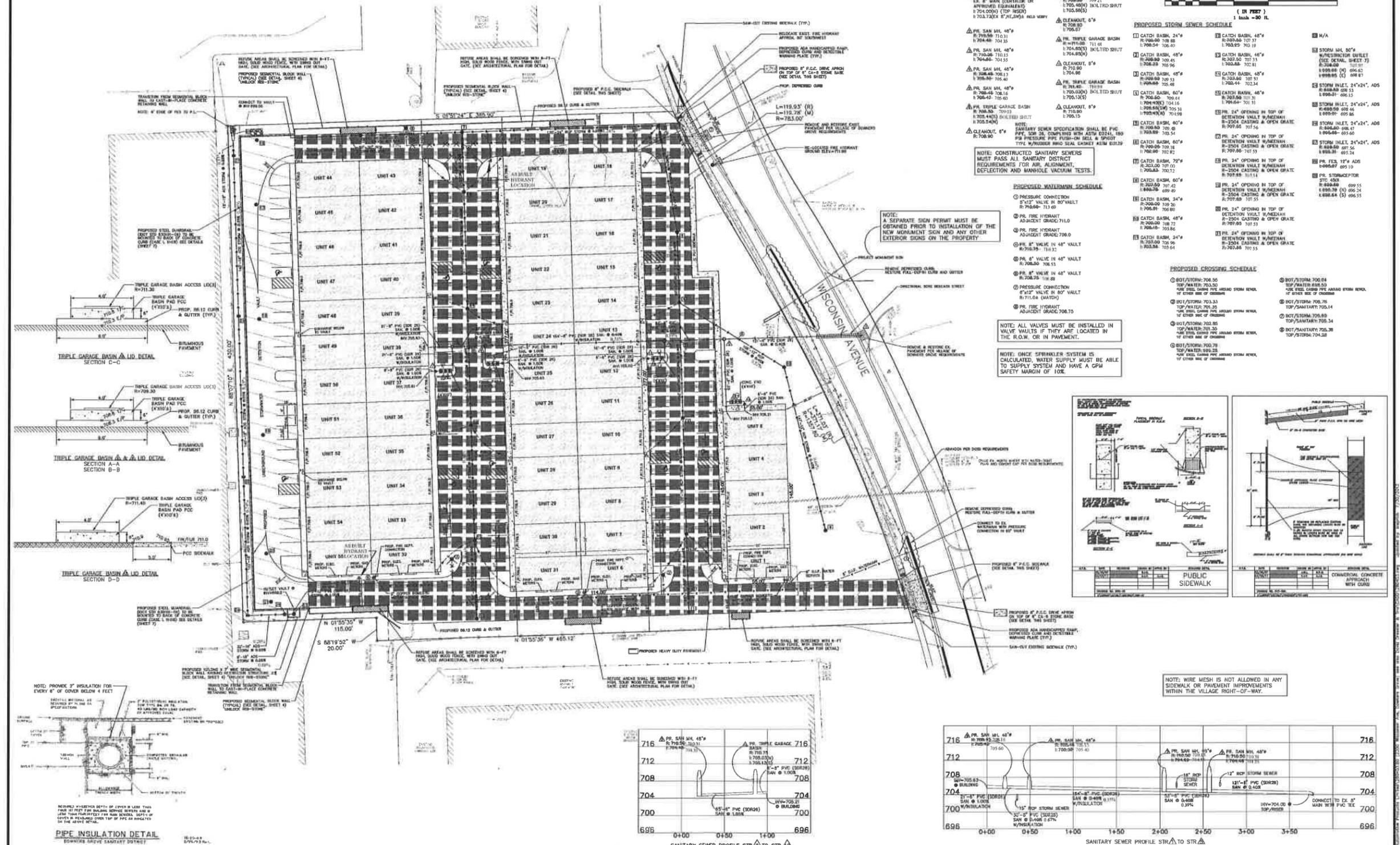
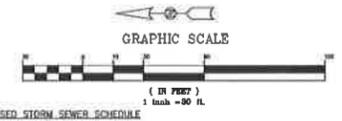
**GRADING AND SOIL EROSION CONTROL PLAN**

SCALE: 1" = 30'  
 DATE: JUNE 8, 2012  
 JOB NO: 2804039

**Sheet 4 of 7**

# RECORD/AS BUILTS 9-15-2014

NOTE: AT ALL DRIVEWAY CROSSINGS & IN FRONT OF UNIT GARAGE DOORS, PCC SIDEWALK SHALL BE 8" DEPTH. SEE DETAIL ON THIS SHEET FOR SIDEWALK AT DRIVEWAY CROSSINGS.  
PCC SIDEWALK SHALL BE 5" IN DEPTH IN ALL OTHER PLACES.



DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION
8/27/12	RT	VILLAGE COMMENTS	7/19/13	BL	VILLAGE COMMENTS
10/01/12	RT	ODD COMMENTS			
12/7/12	AJ	VILLAGE COMMENTS			

**ENGINEERING RESOURCE ASSOCIATES, INC.**  
CONSULTING ENGINEERS, SCIENTISTS & SURVEYORS

35701 WEST AVENUE, SUITE 150  
WARRENVILLE, ILLINOIS 60555  
PHONE: (630) 393-3660  
FAX: (630) 393-2157

501 W. STATE STREET, SUITE 203  
GENEA, ILLINOIS 60141  
PHONE: (630) 262-8559  
FAX: (630) 267-8699

10 S. RIVERSIDE IN A7A,  
SUITE 100  
CHICAGO, ILLINOIS 60606  
PHONE: (312) 693-0110

PREPARED FOR:  
**MACNEIL AUTOMOTIVE PRODUCTS, LTD.**  
1 MACNEIL COURT  
BOLINGBROOK, ILLINOIS 60440

TITLE:  
**GEOMETRY & UTILITY PLAN**

SCALE: 1" = 30'  
DATE: JUNE 8, 2012  
JOB NO: 20040339  
SHEET 3 OF 7



MEMORANDUM TO: Richard Curran  
MacNeal Real Estate Holdings, LLC

FROM: Luay R. Aboona, PE  
Principal

DATE: July 22, 2016

SUBJECT: Parking Study  
2300 Wisconsin Avenue  
Downers Grove, Illinois

This memorandum presents the findings of a parking study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for the 2300 Wisconsin Avenue office development in Downers Grove, Illinois. The development consists of three buildings with 55 units and has a total of 89,800 square feet of gross floor area with 95 outdoor parking spaces and 55 indoor (individual garages) parking spaces for a total of 150 spaces. The development is currently 53 percent occupied (29 occupied units) with Building 1 vacant, Building 2 35 percent occupied, and Building 3 83 percent occupied. **Figure 1** is an aerial view of the development.

The purpose of this parking study is to determine the adequacy of the parking supply in meeting the parking needs of the existing uses, establish the parking ratio for the uses, and confirm the adequacy of the parking supply for future tenants of the development.

### **Parking Occupancy Surveys**

Parking occupancy surveys of the existing parking were conducted on Wednesday, July 13, 2016 and Friday, July 15, 2016 at various times of the day to determine current occupancy levels and peak demand. **Table 1** summarizes the results of the surveys for each day by the following parking areas:

- Area 1: Between Buildings 1 and 2 with 42 outdoor spaces (three handicap) and 18 indoor spaces
- Area 2: Between Buildings 2 and 3 with 36 outdoor spaces (four handicap) and 25 indoor spaces
- Area 3: North of Building 3 with 17 outdoor spaces (two handicap) and 12 indoor spaces



Aerial View of Site Location

Figure 1

Table 1  
 PARKING OCCUPANCY SURVEYS

Time	Parking Occupancy			Total	Available Parking
	1	2	3		
Wednesday, July 13, 2016					
10:00 A.M.	9	6	6	24	129
12:00 P.M.	10	6	8	24	126
2:00 P.M.	16	9	6	31	119
4:00 P.M.	20	7	6	33	117
6:00 P.M.	31	4	7	42	108
8:00 P.M.	32	5	3	40	110
Friday, July 15, 2016					
11:00 A.M.	15	6	11	32	118
1:00 P.M.	15	7	8	30	120
3:00 P.M.	18	8	6	32	118
5:00 P.M.	16	4	7	27	123
7:00 P.M.	40	3	2	45	105
9:00 P.M.	42	6	2	50	100

As can be seen, the peak parking demand on Wednesday occurred at 6:00 P.M. with 42 occupied spaces or 28 percent of available spaces. On Friday, the peak demand occurred at 9:00 P.M. with 50 occupied spaces or 33 percent of available spaces.

### **Parking Evaluation**

The results of the parking occupancy surveys show that parking is readily available during the day. The peak demand occurs after regular business hours and is influenced by the demand of Alter Brewing Company. The peak demand during regular business hours is 33 spaces occurring on Wednesday at 4:00 P.M. which is 22 percent of available parking spaces. These results indicate the following:

- Shared parking is currently working well due to the different peak parking characteristics of the uses including Alter Brewing Company.
- The overall parking ratio of occupied spaces is 1.72 spaces per occupied unit which occurs at 9:00 P.M.
- During regular business hours, the parking demand ratio is 1.14 spaces per occupied unit occurring at 4:00 P.M.
- The available parking spaces (117 spaces), when compared to the vacant units (26 units), will translate into a ratio of 4.5 spaces per unit, which far exceeds the observed demand.

Tenant 2300 Wisconsin Ave.	Units	Contact : Person and Number	Email
<b>Building 1</b>			
Empty	101		
Empty	102		
Empty	103		
Empty	104		
Empty	105		
<b>Building 2 South</b>			
Fastenal	201-203	Kirk Decker 630-795-1081	<a href="mailto:kdecker@fastenal.com">kdecker@fastenal.com</a>
Empty	204		
Empty	205		
Empty	206		
Empty	207		
Custom Cabinet Connections	208	Pete Guardi 630-688-2448	<a href="mailto:pguardi@comcast.net">pguardi@comcast.net</a>
Alter Brewery	209-210-211- 212-213	David Yob 773-203-9434	<a href="mailto:davidyob@gmail.com">davidyob@gmail.com</a>
<b>Building 2 North Side</b>			
Empty	214		
Empty	215		
Empty	216		
Empty	217		
Empty	218		
Empty	219		
Empty	220		
Empty	221		
Empty	223		
Empty	224		
Empty	225		
Empty	226		
<b>Building 3 South Side</b>			
Olsen Motor Sports	301-302	Tim Olsen 312 810-5353	<a href="mailto:Olsenmotorsports@me.com">Olsenmotorsports@me.com</a>
Boo Coo Enterprises	303	ED Brackett ( 630-484-6779	<a href="mailto:Edbrackett4@gmail.com">Edbrackett4@gmail.com</a>
All Around Vending	304	Dennis Neisteam 630-674-7	<a href="mailto:Snacktime@hotmail.com">Snacktime@hotmail.com</a>
Apple and Sons Vending	305	Anthony Appleyard 708-837	<a href="mailto:apples1992@sbcglobal.com">apples1992@sbcglobal.com</a>
Tim Hendricks	308	Tim Hendricks 312-513-2471	

John Kucera	309	John Kucera 630-887-7510 <a href="mailto:jdkucera@comcast.net">jdkucera@comcast.net</a>
Saratore Resource Group	310	David Iverson 773- 399- 959 <a href="mailto:DSLverson@weathertech.com">DSLverson@weathertech.com</a>
The Car Stall	311-312	Patrick Wolmak <a href="mailto:pwomack20@aol.com">pwomack20@aol.com</a>
<b>Building 3 northside</b>		
Naperville property's	313	Joel Weinberger 630-388-54 <a href="mailto:jkw@continentalmotors.com">jkw@continentalmotors.com</a>
Sievers & Stevens Const	314	Mike Stevens <a href="mailto:Michael.Stevens.&lt;mike@sieversan">Michael Stevens. &lt;mike@sieversan</a>
Vulcan Motot Sports	315	Anthony 331-588-9083 <a href="mailto:Tonyd8620@gmail.com">Tonyd8620@gmail.com</a>
Empty	316	
Empty	317	
McBath Construction	318	Mike McBath 708-654-8204
Empty	319	
Epoc Construction	320	Dan Deem 630-808-7962 <a href="mailto:ddeem@epochconstrction.com">ddeem@epochconstrction.com</a>
Empty	312	
1st Class Garage Door	322	Branden Suva 630-532-8937 <a href="mailto:fcgdoor@gmail.com">fcgdoor@gmail.com</a>
empty	323	
Todd Hensley	324	Todd Hensley 417-830-2990 <a href="mailto:Todd@toddsdomain.com">Todd@toddsdomain.com</a>

**ORDINANCE NO. 5265**

**AN ORDINANCE AUTHORIZING A SPECIAL USE  
TO PERMIT THE CONSTRUCTION OF THREE PRINCIPAL BUILDINGS  
ON A SINGLE LOT AT 2300 WISCONSIN AVENUE**

WHEREAS, the following described property, to wit:

Lot 1 in Frank Lopata Resubdivision of Lots 10, 11 and 12 in the Resubdivision of Lots 8 to 13 inclusive in Ellsworth Park Unit 3, and Lot 24 in Ellsworth Park Unit 5 in the east half of the southwest quarter of Section 12, and the north half of the southeast quarter of Section 12, Township 38 North, Range 10 East of the third principal meridian, according to the plat of said Frank Lopata Resubdivision recorded August 13, 1965 as Document R65-30445, in DuPage County, Illinois,

Commonly known as 2300 Wisconsin Avenue, Downers Grove, IL 60515 (PIN 08-12-407-006)

(hereinafter referred to as the "Property") is presently zoned "*M-1 -Light Manufacturing District*" under the Comprehensive Zoning Ordinance of the Village of Downers Grove; and

WHEREAS, the owner of the Property has filed with the Plan Commission, a written petition conforming to the requirements of the Zoning Ordinance, requesting that a Special Use per Section 28.1300SEC (a) of the Zoning Ordinance be granted to permit construction of three principal buildings on a single lot; and,

WHEREAS, such petition was referred to the Plan Commission of the Village of Downers Grove on April 2, 2012, and said Plan Commission has given the required public notice, has conducted a public hearing respecting said petition and has made its findings and recommendations, all in accordance with the statutes of the State of Illinois and the ordinances of the Village of Downers Grove; and,

WHEREAS, the Plan Commission has recommended approval of the requested Special Use, subject to certain conditions; and,

WHEREAS, the Village Council finds that the evidence presented in support of said petition, as stated in the aforesaid findings and recommendations of the Plan Commission, is such as to establish the following:

1. The proposed use at that particular location requested is necessary or desirable to provide a service or a facility which is in the interest of public convenience and will contribute to the general welfare of the neighborhood or community.
2. The proposed use will not, under the circumstances of the particular case, be detrimental to the health, safety, morals, or general welfare of persons residing or working in the vicinity or injurious to property values or improvements in the vicinity.
3. The proposed use will comply with the regulations specified in this Zoning Ordinance for the district in which the proposed use is to be located.
4. The proposed use is one of the special uses specifically listed for the district in which it is to be located and, if approved with restrictions as set forth in this ordinance, will comply with the provisions of the Downers Grove Zoning Ordinance regulating this Special Use.

NOW, THEREFORE, BE IT ORDAINED by the Council of the Village of Downers Grove, in DuPage County, Illinois, as follows:

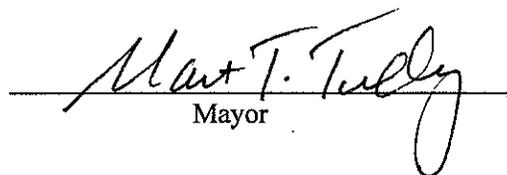
SECTION 1. That a Special Use of the Property is hereby granted to permit construction of three principal buildings on a single lot within the M-1 zoning district.

SECTION 2. This approval is subject to the following conditions:

1. The Special Use shall substantially conform to the staff report, engineering plans prepared by Engineering Resource Associates, Inc. dated February 24, 2012 and architectural plans prepared by Fergon Architects, LLC., dated February 21, 2012, except as such plans may be modified to conform to the Village codes and ordinances.
2. The applicant shall obtain an extension of the Special Use from Village Council if the construction of Phase II does not commence on or before May 1, 2014.
3. The Special Use shall be limited to the following uses:
  - a. Professional and Business Offices
  - b. Research and Development Facilities
  - c. Light Manufacturing including production, processing, cleaning, testing and repair limited to uses and products described in 28.902(h).
  - d. Wholesale and warehousing, storage and distribution facilities, excluding motor freight terminals or self-storage facilities (mini-warehouses); however, no single tenant shall contain more than 10,000 square feet.
  - e. Uses accessory to the above-listed permitted uses.
4. The proposed sidewalk on the private property shall be connected to the existing public sidewalk along Wisconsin Avenue.
5. All new signage on the property shall conform to the Sign Ordinance. A separate sign permit is required prior to installation of any new signage on the property.
6. Private fire hydrants shall be provided within 100 feet of the Fire Department connection and no greater than 300 feet apart.
7. The buildings shall have fire suppression and detection systems in a manner suitable to the Fire Prevention Bureau Chief.

SECTION 3. The above conditions are hereby made part of the terms under which the Special Use is granted. Violation of any or all of such conditions shall be deemed a violation of the Village of Downers Grove Zoning Ordinance, the penalty for which may include, but is not limited to, a fine and or revocation of the Special Use granted herein.

SECTION 4. That all ordinances or parts of ordinances in conflict with the provisions of this ordinance are hereby repealed.

  
Mayor

Passed: May 15, 2012  
Published: May 16, 2012

Attest: April K. Mc  
Village Clerk

Downers Grove Municipal Code

enclosed by solid wall or fence eight (8) feet high, including solid doors or gates thereto. However, open off-street loading facilities and open off-street parking facilities for the storage of motor vehicles may be unenclosed throughout the district except for such screening of parking and loading facilities as may be required under the provisions of Article XIV.

(4069, Enacted, 10/05/1998)

**Section 28.902. M-1 Districts - Permitted uses.**

The following uses are allowed in the M-1 Light Manufacturing District as permitted uses:

(a) Uses permitted in the B-3 General Services and Highway Business District, which are not permitted in other business districts.

(b) Animal hospitals, pounds, and shelters

(c) Banks and financial institutions

(d) Day care center. (See Section 28-1017.)

(e) Fuel and ice sales

(f) Gymnasiums

(g) Offices, business and professional.

(h) Production, processing, cleaning, testing and repair limited to the following uses and products:

(1) Advertising displays

(2) Art and needlework and hand weaving

(3) Artificial limb manufacture

(4) Awnings, venetian blinds

(5) Bakeries

(6) Batteries

(7) Beverages, non-alcoholic

(8) Bicycles

(9) Blacksmith shops

(10) Blueprinting and photostating establishments

(11) Boat building and repair of pleasure craft

(12) Books - hand binding and tooling

(13) Bottling works - beverage or creamery

(14) Brushes and brooms

(15) Camera and other photographic equipment, except film

(16) Candy manufacture

(17) Canvas and canvas products

(18) Carpentry and woodworking shops

(19) Carpet and rug cleaning

(20) Caskets and casket supplies

(21) Ceramic products - such as pottery and small glazed tile

(22) Clothing manufacture

(23) Cold storage plants

(24) Cosmetics and toiletries

(25) Dentures

(26) Drugs

(27) Dry cleaning establishments

(28) Dwellings for watchmen, located on the premises where employed.

(29) Electric appliances, such as lighting fixtures, irons, fans, toasters, and electric toys.

(30) Food products, processing and combining of baking, boiling, canning, cooking, dehydrating, freezing, frying, grinding, mixing and pressing.

(31) Frozen food lockers

Downers Grove Municipal Code

- (32) Fur goods, not including tanning or dyeing
- (33) Glass products, from previously manufactured glass
- (34) Hair, felt and feather products (except washing, curling and dyeing)
- (35) Hat bodies of fur and wool felt
- (36) House trailers or mobile homes
- (37) Ice, dry and natural
- (38) Ink mixing and packaging and inked ribbons
- (39) Insecticides
- (40) Jewelry
- (41) Laboratories, medical, dental, research, experimental and testing
- (42) Laundries
- (43) Leather products, including shoes and machine belting
- (44) Luggage
- (45) Machine shops for tool, die and pattern making
- (46) Machinery, farm - sales, repairing and overhauling
- (47) Medical and dental clinics.
- (48) Metal finishing, plating, grinding, sharpening, polishing, cleaning, rust-proofing and heat treatment.
- (49) Metal stamping and extrusion of small products, such as costume jewelry, pins and needles, razor blades, bottle caps, buttons and kitchen utensils.
- (50) Monument works
- (51) Musical instruments
- 52) Orthopedic and medical appliances, such as artificial limbs, braces, supports and stretchers.
- (53) Paper products, small, such as envelopes and stationery, bags, boxes, tubes, and wallpaper printing.
- (54) Perfumes and perfumed soaps, compounding and packaging only.
- (55) Pharmaceutical products, compounding and packaging
- (56) Plastic manufacture and processing
- (57) Precision instruments, such as optical, medical and drafting
- (58) Printing, lithographing, or publishing establishments for letter press, business cards, mimeographing and other similar custom services, newspaper publishing, including engraving and photo-engraving.
- (59) Products from finished materials - plastic, bone, cork, feathers, felt, fibre, fur, glass, hair, horn, leather, paper, precious and semi-precious stones, rubber, shell, or yarn.
- (60) Repair of household or office machinery or equipment
- (61) Rubber products, small and synthetic treated fabrics, (excluding all rubber and synthetic processing), such as washers, gloves, footwear, bathing caps and atomizers.
- (62) Sheet metal shops
- (63) Silverware, plate and sterling
- (64) Sign painting
- (65) Soap and detergents, packaging only
- (66) Soldering and welding
- (67) Sporting and athletic equipment, such as balls, baskets, cues, gloves, bats, racquets and rods.
- (68) Statuary, mannequins, figurines, and religious and church art goods, excluding foundry operations.
- (69) Textiles - spinning, weaving, manufacturing, dyeing, printing, knit goods, yarn, thread, and cordage, but not including textile bleaching.
- (70) Truck repair garages
- (71) Tools and hardware, such as bolts, nuts and screws, doorknobs, drills, hand tools and cutlery, hinges, house hardware, locks, non-ferrous metal castings, and plumbing appliances.

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- (72) Toys
  - (73) Umbrellas
  - (74) Upholstering (bulk), including mattress manufacturing, rebuilding and renovating.
  - (75) Vehicles, children's - such as bicycles, scooters, wagons, and baby carriages
  - (76) Watches
  - (77) Wood products such as furniture, boxes, crates, baskets, and pencils, and cooperage corks.
- (i) Wholesale and warehousing, storage and distribution Facilities, excluding motor freight terminals or self-storage facilities (mini-warehouses)
  - (j) Public and community service uses, including electric distribution substations, fire and police stations, telephone exchange and similar uses.
  - (k) Adult Entertainment Establishments. (See Section 28-1011.)
  - (l) Telecommunications Towers. (See Section 28-1307.)
  - (m) Uses incidental to permitted uses as follows:
    - (1) Accessory structures.
    - (2) Signs.
    - (3) Incidental retail sales of products, parts or service directly related to a permitted use.
    - (4) Temporary buildings for construction purposes for a period not to exceed the duration of such construction.
- (Ord. 4914, Amended, 10/02/2007; Ord. 4628, Amended, 12/07/2004; Ord. 4598, Amended, 07/06/2004; 4164, Amended, 12/07/1999; 4150, Amended, 08/16/1999; 4069, Enacted, 10/05/1998)

### **Section 28.903. M-1 Districts - Special Uses.**

The following uses are allowed in the M-1 Light Manufacturing District as special uses:

- (a) Aircraft landing areas.
  - (b) Automobile Dealerships.
  - (c) Automobile Laundries.
  - (d) Automobile Repair.
  - (e) Automobile Service Stations. (See Section 28-1018.)
  - (f) Banks and financial institution drive-in type facilities.
  - (g) Planned developments. (See Article XVI.)
  - (h) Railroad and motor freight terminal, railroad switching facilities.
  - (i) Recycling collection facilities. (See Section 28-1007.)
  - (j) Self-storage facilities, including mini-warehouses. (See Section 28-1010.)
- (Ord. 4628, Amended, 12/07/2004; 4150, Amended, 08/16/1999; 4069, Enacted, 10/05/1998)

### **Section 28.904. M-2 District - General Provisions.**

In the M-2 Restricted Manufacturing District all uses are subject to the following conditions:

(a) The M-2 District is intended generally to accommodate nuisance-free operations, such as headquarters offices, research laboratories, and restricted industrial activities. The standards prescribed for this district are intended to ensure the creation of an environment in which each use is a credit to the others and the investment in well-designed and maintained facilities and grounds is secured by the maintenance of the highest reasonable standards throughout the district. All uses shall be in keeping with the character of the district and shall not be detrimental to the orderly and harmonious development and maintenance of the Village as a whole and nearby districts.

(b) Except as otherwise provided, and except for authorized temporary uses, all permitted uses shall be conducted wholly within enclosed buildings. Within five hundred (500) feet of the nearest point of any residence district, all storage shall be in completely enclosed structures, and